

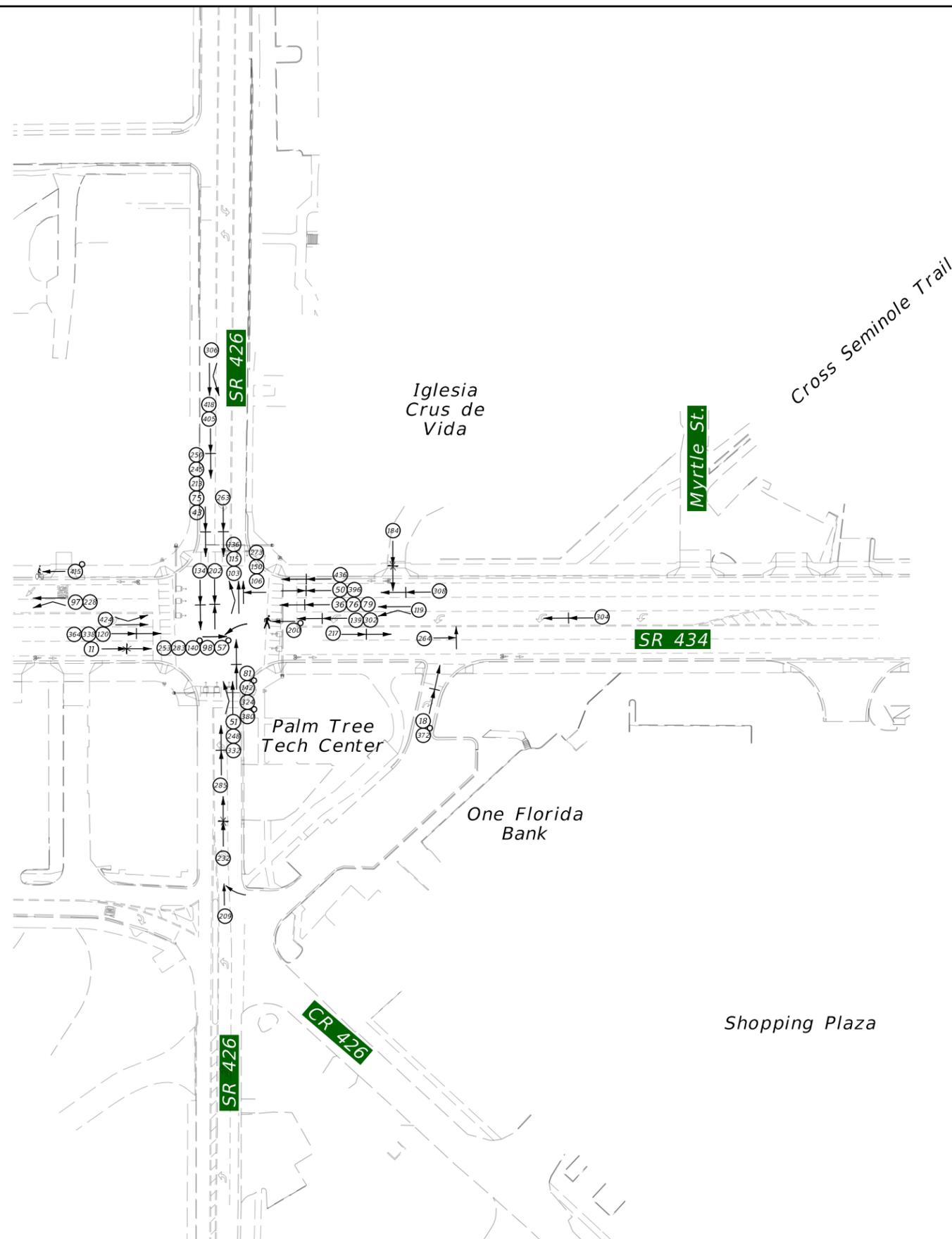
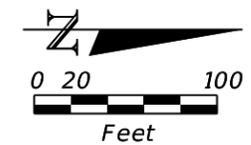
1/31/2024 12:17:20 PM a.johnson T:\Worksets\FDOT\4467561201\Concepts\PLANCD01.dgn

SYMBOLS			
	INJURY CRASH		LEFT TURN COLLISION
	OFF ROAD COLLISION		REAR-END COLLISION
	SIDESWIPE COLLISION		ANGLE COLLISION

PROTEAN DESIGN GROUP
 100 EAST PINE STREET, SUITE 600
 ORLANDO, FL 32801
 PH: 407-246-0044

FDOT
 STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 DISTRICT 5

Figure 13
COLLISION DIAGRAM (4)



SYMBOLS		
	INJURY CRASH	
	RIGHT TURN COLLISION	
	ANGLE COLLISION	
	PEDESTRIAN COLLISION	

PROTEAN DESIGN GROUP
 100 EAST PINE STREET, SUITE 600
 ORLANDO, FL 32801
 PH: 407-246-0044

FDOT
 STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 DISTRICT 5

Figure 13
COLLISION DIAGRAM (5)

1/31/2024 12:17:21 PM a.johnson
 T:\Worksets\FDOT\4467561\201\Concerns\PLANCD01.dgn

COLLISION DATA

Section: 77170-000
 State Road: 434
 Intersection Roadway: Mitchell Hammock Rd to SR 426 - CR 419
 Source Data: Signal Four Analytics

MP: 3.038 - 4.058
 County: Seminole
 City: Oviedo

Study Period		From	1/11/2019	to	12/27/2023	59	Months									
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause	
1	88801250	1/11/19	Fri	17:28	29	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,600	Careless Driving	
2	88801275	1/30/19	Wed	15:17	23	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving	
3	88801279	2/2/19	Sat	14:39	22	0	Daylight	Dry	Cloudy	0	0	0	Left Turn	\$4,500	Careless Driving	
4	88801280	2/3/19	Sun	17:13	22	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Careless Driving	
5	88801290	2/9/19	Sat	17:43	20	0	Daylight	Dry	Clear	0	0	0	Off Road	\$10,000	Careless Driving	
6	88801331	3/3/19	Sun	21:54	38	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving	
7	88801333	3/5/19	Tue	14:03	21	0	Daylight	Dry	Cloudy	0	0	0	Left Turn	\$8,000	Failed To Report Crash	
8	88801336	3/8/19	Fri	15:25	17	0	Daylight	Dry	Clear	0	0	0	Right Turn	\$2,000	Other	
9	88801339	3/9/19	Sat	8:25	34	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$8,000	Careless Driving	
10	88801346	3/12/19	Tue	20:00	22	0	Dark (No SL)	Dry	Clear	0	0	0	Rear End	\$15,000	Careless Driving	
11	88801349	3/15/19	Fri	13:17	31	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$2,500	Careless Driving	
12	88801352	3/19/19	Tue	14:07	22	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$2,000	Careless Driving	
13	88801372	3/28/19	Thu	17:49	49	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$5,000	N/A	
14	88801376	3/29/19	Fri	16:45	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving	
15	87366020	3/29/19	Fri	15:47	43	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,300	N/A	
16	88801374	3/29/19	Fri	9:29	43	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	Improper License	
17	88801401	4/8/19	Mon	18:40	32	0	Dusk	Dry	Cloudy	0	1	0	Pedestrian	\$1,000	License Suspended	
18	88801408	4/10/19	Wed	17:44	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving	
19	88801410	4/11/19	Thu	13:07	86	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$8,000	Careless Driving	
20	88801413	4/12/19	Fri	14:14	37	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving	
21	88801421	4/16/19	Tue	18:24	50	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$200	N/A	
22	88801422	4/17/19	Wed	13:43	45	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Improper U-Turn	
23	88801427	4/18/19	Thu	21:15	18	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$4,000	License Suspended	
24	88801428	4/19/19	Fri	9:50	41	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Improper License	
25	88801442	4/30/19	Tue	7:58	17	0	Daylight	Dry	Clear	0	0	0	Rear End	\$8,000	Careless Driving	
26	88801454	5/3/19	Fri	14:55	57	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving	
27	88801461	5/7/19	Tue	10:50	51	0	Daylight	Dry	Clear	0	1	0	Rear End	\$4,000	Careless Driving	
28	88801460	5/7/19	Tue	9:06	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,501	Careless Driving	
29	88801480	5/13/19	Mon	17:55	29	0	Daylight	Dry	Clear	0	1	0	Rear End	\$3,500	Careless Driving	
30	88801501	5/22/19	Wed	17:48	72	0	Daylight	Dry	Clear	0	1	0	Right Turn	\$10,000	FTY at Intersection	
31	88801515	5/29/19	Wed	6:52	38	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving	
32	88801533	6/5/19	Wed	20:00	18	0	Dark (SL)	Dry	Cloudy	0	0	0	Sideswipe	\$1,500	Careless Driving	
33	88801549	6/13/19	Thu	19:38	17	0	Dawn	Wet	Cloudy	0	0	0	Rear End	\$10,000	Careless Driving	
34	88801550	6/13/19	Thu	15:53	17	0	Daylight	Wet	Rain	0	0	0	Sideswipe	\$4,000	Careless Driving	
35	88801554	6/14/19	Fri	19:53	18	0	Daylight	Wet	Rain	0	2	0	Left Turn	\$10,000	Improper Lane Change	
36	88801564	6/18/19	Tue	15:38	118	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$500	N/A	
37	88801571	6/22/19	Sat	11:23	16	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$4,000	Careless Driving	
38	88801578	6/26/19	Wed	7:57	79	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving	
39	88801579	6/26/19	Wed	9:15	21	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving	
40	89235213	7/10/19	Wed	16:07	17	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$12,000	Careless Driving	
41	89235215	7/11/19	Thu	14:37	43	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$5,000	Careless Driving	
42	89235220	7/14/19	Sun	11:04	60	0	Daylight	Dry	Clear	0	5	0	Left Turn	\$50,000	N/A	
43	89235228	7/17/19	Wed	9:00	42	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	No Vehicle Registration	
44	89235238	7/24/19	Wed	16:00	49	0	Daylight	Dry	Clear	0	1	0	Rear End	\$3,000	Careless Driving	
45	89235244	7/26/19	Fri	10:43	48	0	Daylight	Dry	Clear	0	0	0	Rear End	\$8,000	FTY at Intersection	
46	89235249	7/29/19	Mon	15:25	32	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	Careless Driving	
47	89235259	8/5/19	Mon	15:32	48	0	Daylight	Wet	Rain	0	1	0	Rear End	\$2,000	Careless Driving	
48	89235280	8/16/19	Fri	12:43	36	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$2,500	Careless Driving	
49	89235293	8/21/19	Wed	18:20	21	0	Daylight	Dry	Clear	0	0	0	Rear End	\$10,000	Careless Driving	
50	89235292	8/22/19	Thu	12:15	57	0	Daylight	Dry	Clear	0	0	0	Head On	\$500	Careless Driving	
51	89235291	8/22/19	Thu	12:42	22	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$6,000	Careless Driving	
52	89235307	8/29/19	Thu	23:56	58	1	Dark (SL)	Wet	Rain	0	0	0	Off Road	\$10,000	Careless Driving	
53	89235315	9/4/19	Wed	15:35	17	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$2,000	Careless Driving	
54	89235323	9/8/19	Sun	20:40	18	0	Dark (SL)	Dry	Clear	0	4	0	Left Turn	\$20,000	Careless Driving	
55	89235329	9/10/19	Tue	13:15	33	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$6,000	FTYRW	
56	89235334	9/11/19	Wed	8:37	23	0	Daylight	Dry	Clear	0	0	0	Off Road	\$2,500	Careless Driving	
57	89235353	9/16/19	Mon	19:41	17	0	Dusk	Dry	Clear	0	2	0	Left Turn	\$4,000	FTYRW	

COLLISION DATA

Section: 77170-000
 State Road: 434
 Intersection Roadway: Mitchell Hammock Rd to SR 426 - CR 419
 Source Data: Signal Four Analytics

MP: 3.038 - 4.058
 County: Seminole
 City: Oviedo

Study Period		From	1/11/2019		to	12/27/2023		59 Months							
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause
58	89235358	9/18/19	Wed	11:38	69	0	Daylight	Dry	Cloudy	0	1	0	Pedestrian	\$500	Careless Driving
59	89235362	9/19/19	Thu	8:14	33	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$9,000	N/A
60	89235368	9/22/19	Sun	14:45	86	0	Daylight	Dry	Clear	0	0	0	Angle	\$1,000	N/A
61	89235375	9/26/19	Thu	14:07	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$600	Careless Driving
62	89235379	9/28/19	Sat	22:36	53	0	Dark (SL)	Dry	Clear	0	1	0	Rear End	\$10,000	FTYRW
63	89235384	10/1/19	Tue	20:40	41	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$2,500	Careless Driving
64	89235393	10/5/19	Sat	13:20	29	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,500	FTY at Intersection
65	89235407	10/8/19	Tue	18:00	20	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$20,000	N/A
66	89235406	10/8/19	Tue	18:45	23	0	Dawn	Wet	Cloudy	0	0	0	Rear End	\$10,000	Careless Driving
67	89235434	10/21/19	Mon	18:10	69	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	FTY at Intersection
68	89235444	10/30/19	Wed	13:11	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving
69	89235449	10/31/19	Thu	16:02	60	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Improper Backing
70	89235454	11/4/19	Mon	12:51	33	0	Daylight	Dry	Cloudy	0	1	0	Rear End	\$10,000	Careless Driving
71	89235460	11/7/19	Thu	12:04	25	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving
72	89235471	11/10/19	Sun	19:13	69	0	Dark (No SL)	Dry	Clear	0	0	0	Sideswipe	\$5,000	Careless Driving
73	89235479	11/13/19	Wed	8:03	62	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$10,000	Leaving Scene
74	89235493	11/18/19	Mon	14:39	28	0	Daylight	Dry	Clear	0	1	0	Rear End	\$25,500	Careless Driving
75	89235498	11/20/19	Wed	17:02	17	0	Dusk	Dry	Clear	0	0	0	Rear End	\$1,000	FTYT Oncoming Traffic
76	89235506	11/25/19	Mon	9:50	47	0	Daylight	Dry	Clear	0	0	0	Rear End	\$7,000	Failed To Obey TCD
77	89235515	12/3/19	Tue	9:05	32	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Careless Driving
78	89235522	12/6/19	Fri	15:57	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	FTY at Intersection
79	89235545	12/13/19	Fri	13:43	42	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$3,000	Improper License
80	89235555	12/17/19	Tue	10:49	20	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$1,001	Careless Driving
81	89235559	12/18/19	Wed	9:26	25	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$6,000	Careless Driving
82	89235563	12/19/19	Thu	16:11	24	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$600	N/A
83	89235564	12/19/19	Thu	16:49	63	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$5,900	Careless Driving
84	89235574	12/20/19	Fri	15:30	37	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving
85	89235570	12/21/19	Sat	15:02	46	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$14,500	Careless Driving
86	89235586	12/28/19	Sat	20:13	28	1	Dark (SL)	Wet	Rain	0	2	0	Rear End	\$49,000	Careless Driving
87	89235598	1/8/20	Wed	15:08	46	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving
88	89235597	1/8/20	Wed	13:58	19	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$30,000	Careless Driving
89	89235646	2/2/20	Sun	18:46	18	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$10,000	Careless Driving
90	89235649	2/5/20	Wed	18:50	21	0	Dark (SL)	Dry	Clear	0	1	0	Left Turn	\$30,000	Careless Driving
91	89235651	2/7/20	Fri	18:40	25	0	Dark (SL)	Dry	Clear	0	1	0	Rear End	\$4,000	Careless Driving
92	12433951	2/9/20	Sun	11:04	43	0	Daylight	Dry	Clear	0	1	0	Rear End	\$2,000	Careless Driving
93	89235670	2/19/20	Wed	18:20	21	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$0	Improper Backing
94	89235681	2/24/20	Mon	17:43	38	0	Daylight	Dry	Clear	0	1	0	Rear End	\$25,100	FTY at Intersection
95	89235690	2/28/20	Fri	17:35	33	0	Daylight	Dry	pg. Smog, Smo	0	0	0	Angle	\$5,000	License Suspended
96	89235689	2/29/20	Sat	7:52	46	0	Daylight	Wet	Clear	0	0	0	Rear End	\$2,500	Careless Driving
97	89452085	3/11/20	Wed	17:44	53	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$2,000	Careless Driving
98	89452088	3/11/20	Wed	19:45	47	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$10,000	Careless Driving
99	89452094	3/17/20	Tue	13:08	66	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$20,000	Careless Driving
100	89452099	3/26/20	Thu	11:40	0	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$6,000	N/A
101	89452106	4/2/20	Thu	8:23	54	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Careless Driving
102	89452110	4/6/20	Mon	10:10	21	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$3,500	FTY at Intersection
103	89452114	4/15/20	Wed	16:45	33	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$1,500	N/A
104	89452126	5/2/20	Sat	19:58	24	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$300	Careless Driving
105	89452127	5/3/20	Sun	10:04	25	0	Daylight	Dry	Clear	0	0	0	Rear End	\$500	Careless Driving
106	89452130	5/7/20	Thu	10:17	67	0	Daylight	Dry	Clear	0	0	0	Angle	\$1,000	Careless Driving
107	89452134	5/15/20	Fri	18:25	51	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$700	N/A
108	89452141	5/24/20	Sun	17:46	22	0	Dusk	Dry	Clear	0	0	0	Rear End	\$8,000	Careless Driving
109	89452143	5/26/20	Tue	18:50	62	1	Daylight	Dry	Clear	0	1	0	Rear End	\$11,500	Careless Driving
110	89452144	5/27/20	Wed	14:34	47	0	Daylight	Wet	Rain	0	1	0	Rear End	\$2,500	Careless Driving
111	89452168	6/12/20	Fri	18:21	50	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	Improper Backing
112	89452174	6/14/20	Sun	22:10	18	0	Dark (SL)	Dry	Clear	0	1	0	Left Turn	\$8,000	Improper Right Turn
113	89452178	6/17/20	Wed	20:56	22	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$1,500	Careless Driving
114	89452180	6/20/20	Sat	17:57	42	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,500	Careless Driving

COLLISION DATA

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 Source Data: Signal Four Analytics

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Study Period		From	1/11/2019	to	12/27/2023	59	Months									
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause	
115	89452182	6/22/20	Mon	10:45	81	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving	
116	89452185	6/23/20	Tue	17:39	57	0	Daylight	Dry	Clear	0	1	0	Angle	\$10,000	Careless Driving	
117	89452190	6/28/20	Sun	16:54	44	0	Daylight	Dry	Clear	0	0	0	Bicycle	\$0	Careless Driving	
118	89452191	6/28/20	Sun	18:30	22	0	Daylight	Dry	Clear	0	2	0	Rear End	\$30,000	Careless Driving	
119	89452192	6/29/20	Mon	16:41	50	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving	
120	89452214	7/9/20	Thu	17:19	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$12,000	Careless Driving	
121	89452216	7/10/20	Fri	8:56	27	0	Daylight	Wet	Rain	0	0	0	Rear End	\$200	Careless Driving	
122	89452220	7/12/20	Sun	17:54	31	0	Daylight	Dry	Clear	0	1	0	Rear End	\$1,000	Careless Driving	
123	89452228	7/17/20	Fri	8:23	28	0	Daylight	Dry	Clear	0	1	0	Angle	\$40,000	Careless Driving	
124	89452231	7/20/20	Mon	11:56	0	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$500	N/A	
125	89452248	7/29/20	Wed	17:23	39	0	Daylight	Wet	Clear	0	0	0	Rear End	\$8,000	Careless Driving	
126	89452255	8/3/20	Mon	11:08	77	0	Daylight	Dry	Clear	0	0	0	Right Turn	\$3,000	Careless Driving	
127	89452268	8/7/20	Fri	13:08	37	0	Daylight	Dry	Clear	0	2	0	Left Turn	\$50,000	Careless Driving	
128	89452269	8/7/20	Fri	17:58	23	0	Daylight	Wet	Rain	0	0	0	Rear End	\$200	N/A	
129	89855264	8/9/20	Sun	8:17	54	0	Daylight	Dry	Clear	0	2	0	Left Turn	\$3,000	N/A	
130	89452274	8/15/20	Sat	16:06	77	0	Daylight	Wet	Cloudy	0	0	0	Left Turn	\$30,000	N/A	
131	89452294	8/23/20	Sun	21:14	42	0	Dark (SL)	Dry	Clear	0	0	0	Angle	\$15,000	FTY at Intersection	
132	89452296	8/24/20	Mon	9:32	21	0	Daylight	Dry	Cloudy	0	1	0	Left Turn	\$17,000	Careless Driving	
133	89452295	8/24/20	Mon	8:16	53	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$3,000	Careless Driving	
134	89452324	9/8/20	Tue	15:39	54	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	Improper Lane Change	
135	89452333	9/15/20	Tue	14:55	0	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,000	N/A	
136	89452334	9/16/20	Wed	8:53	31	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$3,000	Careless Driving	
137	89452351	9/28/20	Mon	14:49	31	0	Daylight	Wet	Rain	0	0	0	Rear End	\$2,000	Careless Driving	
138	89452357	10/1/20	Thu	16:00	0	0	Daylight	Dry	Clear	0	1	0	Rear End	\$2,000	N/A	
139	89452364	10/5/20	Mon	13:17	76	0	Daylight	Dry	Clear	0	0	0	Rear End	\$500	License Suspended	
140	89452365	10/5/20	Mon	15:25	44	0	Daylight	Dry	Clear	0	1	0	Left Turn	\$50,000	Careless Driving	
141	89452375	10/10/20	Sat	20:27	59	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$900	N/A	
142	89452378	10/12/20	Mon	7:27	57	0	Daylight	Dry	Clear	0	1	0	Rear End	\$3,000	Careless Driving	
143	89452380	10/12/20	Mon	11:56	22	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving	
144	89452419	11/3/20	Tue	14:34	16	0	Daylight	Dry	Clear	0	0	0	Rear End	\$7,000	Careless Driving	
145	89452426	11/9/20	Mon	14:14	69	0	Daylight	Wet	Rain	0	1	0	Sideswipe	\$6,000	Careless Driving	
146	89452427	11/9/20	Mon	16:05	20	0	Daylight	Wet	Rain	0	0	0	Rear End	\$12,000	Careless Driving	
147	89452460	11/27/20	Fri	22:40	23	0	Dark (SL)	Dry	Clear	0	1	0	Left Turn	\$10,000	Careless Driving	
148	89452462	11/29/20	Sun	16:21	51	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$8,000	Careless Driving	
149	89452465	11/30/20	Mon	12:55	19	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$100	Careless Driving	
150	89452468	12/2/20	Wed	14:01	20	0	Daylight	Dry	Clear	0	0	0	Angle	\$6,000	Careless Driving	
151	89452472	12/5/20	Sat	16:15	21	0	Daylight	Dry	Clear	0	1	0	Rear End	\$2,500	Careless Driving	
152	24260279	12/10/20	Thu	18:28	38	0	Dusk	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving	
153	24260278	12/10/20	Thu	15:38	51	0	Daylight	Dry	Clear	0	0	0	Rear End	\$23,250	Careless Driving	
154	24260288	12/11/20	Fri	14:32	25	0	Daylight	Dry	Clear	0	0	0	Rear End	\$15,000	Improper Backing	
155	24260290	12/14/20	Mon	18:56	29	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$20,000	Careless Driving	
156	24260293	12/16/20	Wed	17:16	38	0	Dark (No SL)	Wet	Rain	0	0	0	Rear End	\$15,000	Careless Driving	
157	89856520	12/20/20	Sun	19:50	21	0	Dark (SL)	Wet	Rain	0	0	0	Rear End	\$1,500	N/A	
158	24260309	12/24/20	Thu	12:54	54	0	Daylight	Dry	Clear	0	0	0	Rear End	\$10,000	FTYRW	
159	24260318	1/3/21	Sun	12:06	17	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$6,000	Failed To Obey TCD	
160	24260324	1/4/21	Mon	19:26	57	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$4,000	Careless Driving	
161	24260341	1/13/21	Wed	11:35	22	0	Daylight	Dry	Clear	0	1	0	Rear End	\$20,000	Careless Driving	
162	24260339	1/13/21	Wed	20:41	65	1	Dark (SL)	Dry	Clear	0	0	0	Off Road	\$3,000	Careless Driving	
163	24260343	1/14/21	Thu	15:00	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Careless Driving	
164	24260349	1/19/21	Tue	8:28	28	0	Daylight	Dry	Clear	0	0	0	Angle	\$4,000	Failed To Show License	
165	24260359	1/26/21	Tue	18:25	28	0	Dark (SL)	Dry	Clear	0	2	0	Rear End	\$10,500	Careless Driving	
166	24260358	1/26/21	Tue	17:17	24	0	Dusk	Dry	Clear	0	0	0	Rear End	\$15,000	FTYRW	
167	24260360	1/27/21	Wed	13:43	64	0	Daylight	Dry	Clear	0	0	0	Angle	\$1,000	Careless Driving	
168	24260363	1/28/21	Thu	18:55	19	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving	
169	89856836	1/29/21	Fri	17:00	25	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	N/A	
170	24260366	1/30/21	Sat	11:20	58	0	Daylight	Dry	Clear	0	1	0	Rear End	\$5,000	FTYRW	
171	24260372	2/5/21	Fri	14:36	90	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving	

COLLISION DATA

Section: 77170-000
 State Road: 434
 Intersection Roadway: Mitchell Hammock Rd to SR 426 - CR 419
 Source Data: Signal Four Analytics

MP: 3.038 - 4.058
 County: Seminole
 City: Oviedo

Study Period		From	1/11/2019	to	12/27/2023	59	Months									
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause	
172	24260383	2/8/21	Mon	16:55	20	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$2,000	Careless Driving	
173	24260389	2/10/21	Wed	18:52	31	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$2,000	FTYRW	
174	24260388	2/10/21	Wed	13:30	61	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,200	FTYT Oncoming Traffic	
175	24260395	2/12/21	Fri	13:59	42	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$8,000	Careless Driving	
176	24260396	2/12/21	Fri	14:45	17	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,500	FTYRW	
177	24260400	2/15/21	Mon	18:28	34	0	Dark (No SL)	Dry	Clear	0	0	0	Rear End	\$10,050	N/A	
178	24260403	2/17/21	Wed	13:11	18	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$2,000	Careless Driving	
179	24260402	2/17/21	Wed	13:11	28	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$1,500	FTY at Intersection	
180	24260422	2/26/21	Fri	11:52	59	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,001	FTYRW	
181	24260424	2/28/21	Sun	13:40	65	0	Daylight	Dry	Clear	0	0	0	Rear End	\$100	FTYRW	
182	24260426	3/1/21	Mon	13:05	31	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	License Suspended	
183	24260436	3/6/21	Sat	16:04	20	0	Daylight	Wet	Cloudy	0	0	0	Sideswipe	\$100	N/A	
184	24260440	3/8/21	Mon	7:56	62	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$6,000	Followed Too Closely	
185	24260450	3/11/21	Thu	8:26	0	0	Daylight	Dry	Clear	0	0	0	Rear End	\$500	N/A	
186	24260453	3/12/21	Fri	22:38	19	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$2,000	Leaving Scene	
187	24260466	3/21/21	Sun	1:33	24	0	Dark (SL)	Wet	Rain	0	0	0	Angle	\$10,000	Careless Driving	
188	24260477	3/28/21	Sun	1:20	38	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$6,000	Improper Passing	
189	24260478	3/28/21	Sun	10:22	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving	
190	24260486	3/31/21	Wed	12:00	23	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,500	Careless Driving	
191	24260484	3/31/21	Wed	18:29	73	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving	
192	24260491	4/3/21	Sat	13:42	120	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,000	N/A	
193	24260496	4/5/21	Mon	16:05	0	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$500	N/A	
194	24260497	4/6/21	Tue	2:25	60	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$2,500	N/A	
195	24260518	4/14/21	Wed	17:10	19	0	Daylight	Dry	Clear	0	1	0	Rear End	\$10,000	Careless Driving	
196	24260514	4/14/21	Wed	7:39	22	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	Careless Driving	
197	24260522	4/16/21	Fri	11:02	36	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$20,500	Careless Driving	
198	24260524	4/16/21	Fri	18:00	29	0	Daylight	Wet	Rain	0	0	0	Rear End	\$15,000	Careless Driving	
199	24260526	4/20/21	Tue	11:17	30	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$8,000	Careless Driving	
200	24260536	4/24/21	Sat	15:32	0	0	Daylight	Dry	Cloudy	0	1	0	Pedestrian	\$0	N/A	
201	24260551	4/30/21	Fri	8:51	20	0	Daylight	Dry	Clear	0	1	0	Rollover	\$2,000	N/A	
202	24260565	5/6/21	Thu	16:43	33	1	Daylight	Dry	Clear	0	0	0	Head On	\$5,000	Careless Driving	
203	24260570	5/8/21	Sat	16:32	23	0	Daylight	Dry	Clear	0	1	0	Left Turn	\$100,000	Careless Driving	
204	24260580	5/15/21	Sat	16:09	22	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$2,000	Careless Driving	
205	24245945	5/18/21	Tue	9:14	17	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,500	N/A	
206	24260603	5/26/21	Wed	17:16	29	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,500	Careless Driving	
207	24260625	6/7/21	Mon	1:58	24	1	Dark (SL)	Dry	Clear	0	1	0	Off Road	\$5,500	Careless Driving	
208	24260624	6/9/21	Wed	18:11	17	0	Daylight	Dry	Clear	0	1	0	Rear End	\$3,000	Improper Backing	
209	24260640	6/21/21	Mon	17:05	55	0	Daylight	Dry	Clear	0	0	0	Right Turn	\$15,000	Careless Driving	
210	24260651	7/1/21	Thu	10:30	39	0	Daylight	Wet	Rain	0	0	0	Sideswipe	\$1,200	Careless Driving	
211	24260663	7/10/21	Sat	15:55	51	0	Daylight	Dry	Clear	0	3	0	Left Turn	\$15,500	FTY at Intersection	
212	24260665	7/13/21	Tue	16:09	18	0	Daylight	Dry	Cloudy	0	1	0	Rear End	\$10,000	Followed Too Closely	
213	24260669	7/15/21	Thu	12:14	17	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving	
214	24260670	7/15/21	Thu	5:57	84	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$3,500	Followed Too Closely	
215	24260688	7/28/21	Wed	16:42	82	0	Dusk	Dry	Rain	0	0	0	Sideswipe	\$1,000	Careless Driving	
216	24260692	7/31/21	Sat	11:20	51	0	Daylight	Dry	Clear	0	0	0	Angle	\$3,000	N/A	
217	24260695	8/4/21	Wed	16:02	23	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$200	N/A	
218	24260703	8/7/21	Sat	16:30	18	0	Daylight	Dry	Clear	0	0	0	Right Turn	\$2,000	Careless Driving	
219	24260733	8/22/21	Sun	0:17	31	1	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$20,000	License Suspended	
220	24260734	8/22/21	Sun	10:39	24	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,500	Careless Driving	
221	24260735	8/22/21	Sun	10:39	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,500	No Vehicle Registration	
222	24260738	8/23/21	Mon	14:23	37	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$2,500	Careless Driving	
223	24260742	8/24/21	Tue	12:20	20	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$20,000	Careless Driving	
224	24260748	8/25/21	Wed	22:12	22	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$4,000	Careless Driving	
225	24260755	8/27/21	Fri	16:30	20	0	Daylight	Wet	Rain	0	0	0	Rear End	\$1,500	Careless Driving	
226	24260757	8/29/21	Sun	20:50	44	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving	
227	24260762	9/1/21	Wed	8:50	51	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$2,500	Careless Driving	
228	24716732	9/10/21	Fri	17:52	0	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,000	N/A	

COLLISION DATA

Section: 77170-000
 State Road: 434
 Intersection Roadway: Mitchell Hammock Rd to SR 426 - CR 419
 Source Data: Signal Four Analytics

MP: 3.038 - 4.058
 County: Seminole
 City: Oviedo

Study Period		From	1/11/2019		to	12/27/2023		59 Months							
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause
229	24716739	9/13/21	Mon	12:49	25	0	Daylight	Dry	Clear	0	1	0	U-Turn	\$11,000	Careless Driving
230	24716752	9/18/21	Sat	14:21	21	0	Daylight	Dry	Clear	0	2	0	Left Turn	\$20,000	Careless Driving
231	24716756	9/20/21	Mon	15:12	23	0	Daylight	Wet	Cloudy	0	2	0	Rear End	\$16,000	Careless Driving
232	24716759	9/22/21	Wed	15:23	0	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$500	N/A
233	24716768	9/25/21	Sat	11:42	45	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	N/A
234	24716783	9/29/21	Wed	18:20	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$15,000	Careless Driving
235	24716786	10/1/21	Fri	0:56	24	0	Dark (SL)	Dry	Clear	0	0	0	Angle	\$28,000	Improper Backing
236	24716796	10/6/21	Wed	14:30	18	0	Daylight	Dry	Clear	0	0	0	Angle	\$3,000	Careless Driving
237	24716829	10/20/21	Wed	17:25	16	0	Daylight	Dry	Clear	0	0	0	Angle	\$5,000	Careless Driving
238	24716827	10/20/21	Wed	13:48	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$15,000	Careless Driving
239	24716828	10/20/21	Wed	15:40	17	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$8,000	Careless Driving
240	24716836	10/22/21	Fri	20:58	17	0	Dark (SL)	Dry	Clear	0	1	0	Left Turn	\$10,000	Careless Driving
241	24716835	10/22/21	Fri	14:45	56	0	Daylight	Dry	Clear	0	0	0	Rear End	\$900	N/A
242	24716837	10/23/21	Sat	17:51	55	0	Daylight	Dry	Clear	0	0	0	Angle	\$1,100	License Suspended
243	24716839	10/25/21	Mon	14:57	20	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$2,000	Careless Driving
244	24716844	10/27/21	Wed	9:09	28	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$20,000	Careless Driving
245	24716846	10/28/21	Thu	8:41	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$100	FTYT Oncoming Traffic
246	24716853	10/29/21	Fri	16:10	70	0	Daylight	Wet	Cloudy	0	2	0	Angle	\$25,000	Careless Driving
247	24716856	11/1/21	Mon	2:00	0	0	Dark (SL)	Dry	Clear	0	0	0	Off Road	\$1,200	N/A
248	24716857	11/2/21	Tue	6:59	16	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$5,000	Careless Driving
249	24716865	11/5/21	Fri	20:24	36	0	Dark (SL)	Wet	Rain	0	0	0	Right Turn	\$500	DUI
250	24716875	11/9/21	Tue	15:49	25	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving
251	24716879	11/10/21	Wed	15:18	40	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$8,000	Careless Driving
252	24716883	11/12/21	Fri	7:53	26	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$5,000	Careless Driving
253	24716884	11/12/21	Fri	14:45	17	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$10,000	Careless Driving
254	24716894	11/19/21	Fri	19:24	22	0	Dark (SL)	Wet	Rain	0	0	0	Rear End	\$100	Careless Driving
255	24716896	11/21/21	Sun	16:07	20	0	Daylight	Dry	Clear	0	0	0	Angle	\$30,000	Careless Driving
256	24716910	12/3/21	Fri	12:28	21	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving
257	24716911	12/3/21	Fri	14:15	37	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$2,000	Careless Driving
258	24716909	12/3/21	Fri	8:34	51	0	Daylight	Dry	Clear	0	0	0	Head On	\$4,000	Careless Driving
259	24716915	12/4/21	Sat	19:06	20	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving
260	24716921	12/7/21	Tue	9:01	39	0	Daylight	Dry	pg, Smog, Smog	0	0	0	Rear End	\$1,000	Careless Driving
261	24716928	12/9/21	Thu	12:42	21	0	Daylight	Dry	Clear	0	1	0	Rear End	\$7,000	Careless Driving
262	24716927	12/9/21	Thu	12:41	22	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving
263	24716947	12/16/21	Thu	19:30	42	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving
264	24716952	12/20/21	Mon	18:23	0	0	Dark (SL)	Wet	Rain	0	0	0	Angle	\$800	N/A
265	24716954	12/21/21	Tue	15:15	19	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$2,000	License Suspended
266	24716959	12/25/21	Sat	23:27	17	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$11,000	Careless Driving
267	24716968	1/1/22	Sat	14:04	19	0	Daylight	Dry	Clear	0	0	0	Right Turn	\$1,500	Improper Passing
268	24716976	1/4/22	Tue	18:57	64	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$2,500	Careless Driving
269	24716974	1/4/22	Tue	12:31	62	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$500	Careless Driving
270	24716979	1/8/22	Sat	13:00	20	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,000	Careless Driving
271	24716982	1/10/22	Mon	15:30	23	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$2,000	Careless Driving
272	24716986	1/11/22	Tue	17:17	16	0	Daylight	Dry	Clear	0	0	0	Rear End	\$10,000	Careless Driving
273	24716989	1/12/22	Wed	15:10	32	0	Daylight	Dry	Clear	0	0	0	Angle	\$1,000	Careless Driving
274	24716998	1/15/22	Sat	16:34	18	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving
275	24717001	1/18/22	Tue	6:03	46	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$16,000	Careless Driving
276	24717011	1/19/22	Wed	15:05	19	0	Daylight	Dry	Clear	0	0	0	Rear End	\$10,000	Careless Driving
277	24717013	1/19/22	Wed	18:47	76	0	Dark (SL)	Dry	Clear	0	0	0	Angle	\$4,000	Careless Driving
278	24717015	1/20/22	Thu	18:24	18	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving
279	24717016	1/21/22	Fri	13:51	24	0	Daylight	Dry	Clear	0	2	0	Left Turn	\$40,000	Careless Driving
280	24717019	1/24/22	Mon	15:43	49	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Careless Driving
281	24717022	1/25/22	Tue	10:31	32	0	Daylight	Wet	Rain	0	0	0	Sideswipe	\$2,000	FTYRW
282	24717028	1/28/22	Fri	8:58	16	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$9,000	Careless Driving
283	24717034	1/31/22	Mon	16:10	18	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$20,000	Careless Driving
284	24717062	2/14/22	Mon	15:00	64	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$3,000	Careless Driving
285	24717082	2/24/22	Thu	16:28	16	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,500	Careless Driving

COLLISION DATA

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Study Period		From	1/11/2019		to	12/27/2023		59 Months							
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause
286	24717098	3/10/22	Thu	9:28	18	0	Daylight	Wet	Rain	0	0	0	Rear End	\$5,000	Careless Driving
287	24717099	3/10/22	Thu	7:25	46	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$10,000	Careless Driving
288	24717108	3/14/22	Mon	15:45	78	0	Daylight	Dry	Clear	0	0	0	Angle	\$30,000	Careless Driving
289	24717127	3/26/22	Sat	16:35	27	0	Daylight	Dry	Clear	0	0	0	Off Road	\$3,000	Careless Driving
290	24717133	3/29/22	Tue	10:37	17	0	Daylight	Dry	Clear	0	2	0	Rear End	\$15,000	Careless Driving
291	24717139	3/30/22	Wed	8:51	68	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving
292	24717143	4/2/22	Sat	13:24	54	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$1,000	N/A
293	24717153	4/4/22	Mon	17:37	18	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$1,100	Careless Driving
294	24717160	4/7/22	Thu	18:06	18	0	Daylight	Dry	Clear	0	0	0	Rear End	\$15,000	Careless Driving
295	24717161	4/8/22	Fri	7:58	56	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	N/A
296	24717181	4/20/22	Wed	1:00	18	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$10,000	FTY at Intersection
297	24717186	4/21/22	Thu	17:35	18	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$8,000	Careless Driving
298	24717194	4/26/22	Tue	7:01	23	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	N/A
299	24717202	4/27/22	Wed	16:10	29	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$5,000	Careless Driving
300	24717206	4/30/22	Sat	9:49	40	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$3,000	Careless Driving
301	24717207	5/1/22	Sun	12:00	56	0	Daylight	Dry	Clear	0	0	0	Rear End	\$50	FTYRW
302	24717212	5/2/22	Mon	7:59	43	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	Careless Driving
303	24717220	5/8/22	Sun	22:32	57	1	Dark (SL)	Dry	Clear	0	1	0	Rear End	\$500	Careless Driving
304	24717227	5/11/22	Wed	13:57	51	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving
305	89452482	5/16/22	Mon	11:55	60	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	FTY at Intersection
306	25354839	5/25/22	Wed	12:11	58	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,500	Careless Driving
307	25354838	5/25/22	Wed	8:35	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,500	Careless Driving
308	25354848	5/31/22	Tue	9:20	24	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving
309	25354853	6/2/22	Thu	14:45	66	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	Careless Driving
310	25354855	6/7/22	Tue	11:14	23	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	License Suspended
311	25354861	6/9/22	Thu	14:28	36	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$6,000	Careless Driving
312	25354864	6/10/22	Fri	13:53	61	0	Daylight	Wet	Cloudy	0	1	0	Left Turn	\$20,000	N/A
313	25354872	6/15/22	Wed	16:21	84	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Careless Driving
314	25354875	6/16/22	Thu	13:23	40	0	Daylight	Dry	Clear	0	0	0	Off Road	\$500	DUI
315	25354874	6/16/22	Thu	10:46	39	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving
316	25354891	6/23/22	Thu	22:45	62	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$8,000	FTYT Oncoming Traffic
317	25354900	6/28/22	Tue	16:40	16	0	Daylight	Dry	Clear	0	5	0	Left Turn	\$30,000	License Suspended
318	25354914	7/7/22	Thu	16:31	18	0	Daylight	Wet	Rain	0	0	0	Angle	\$2,000	No Insurance
319	25354934	7/18/22	Mon	19:52	66	0	Daylight	Dry	Clear	0	0	0	Rear End	\$100	N/A
320	25354942	7/20/22	Wed	9:12	26	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	Careless Driving
321	25354939	7/21/22	Thu	11:07	50	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,000	FTYRW
322	25354951	7/27/22	Wed	18:10	16	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$10,000	Careless Driving
323	25354954	7/28/22	Thu	5:58	55	0	Dark (SL)	Dry	Clear	0	0	0	Angle	\$10,000	Careless Driving
324	25354956	7/28/22	Thu	17:50	0	0	Daylight	Dry	Clear	0	0	0	Rear End	\$500	N/A
325	25354957	7/29/22	Fri	22:45	0	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$500	N/A
326	25354963	8/2/22	Tue	18:06	26	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving
327	25354964	8/3/22	Wed	7:39	50	0	Daylight	Dry	Clear	0	0	0	Rear End	\$2,000	Careless Driving
328	25354972	8/5/22	Fri	18:58	18	0	Daylight	Dry	Clear	0	1	0	Rear End	\$20,000	Careless Driving
329	25354986	8/17/22	Wed	16:25	42	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$1,500	Insecure Load
330	25355011	8/27/22	Sat	0:29	15	0	Dark (SL)	Dry	Clear	0	0	0	Left Turn	\$50,000	N/A
331	25355016	8/29/22	Mon	18:41	16	0	Daylight	Wet	Rain	0	0	0	Rear End	\$8,100	Improper Lane Change
332	25355020	8/31/22	Wed	15:45	0	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$1,001	N/A
333	25355022	9/1/22	Thu	21:33	19	0	Dark (SL)	Dry	Clear	0	1	0	U-Turn	\$2,000	Careless Driving
334	25355025	9/2/22	Fri	18:30	22	0	Daylight	Dry	Clear	0	0	0	Off Road	\$8,001	N/A
335	25355033	9/6/22	Tue	20:24	34	0	Dark (No SL)	Dry	Clear	0	1	0	Off Road	\$140,000	Careless Driving
336	25355031	9/6/22	Tue	15:38	57	0	Daylight	Dry	Clear	0	1	0	Backed Into	\$1,100	N/A
337	25355039	9/8/22	Thu	16:15	31	0	Daylight	Wet	Rain	0	0	0	Rear End	\$2,000	N/A
338	25355065	9/15/22	Thu	18:47	29	0	Daylight	Wet	Rain	0	0	0	Rear End	\$20,000	Careless Driving
339	25355063	9/15/22	Thu	12:17	66	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,500	License Suspended
340	25355068	9/16/22	Fri	19:54	59	0	Dark (SL)	Wet	Rain	0	0	0	Off Road	\$2,000	N/A
341	25355079	9/19/22	Mon	16:15	22	0	Daylight	Dry	Clear	0	1	0	Rear End	\$1,400	Careless Driving
342	25355083	9/21/22	Wed	12:53	21	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$4,000	Careless Driving

COLLISION DATA

Section: 77170-000
 State Road: 434
 Intersection Roadway: Mitchell Hammock Rd to SR 426 - CR 419
 Source Data: Signal Four Analytics

MP: 3.038 - 4.058
 County: Seminole
 City: Oviedo

Study Period		From	1/11/2019		to	12/27/2023		59 Months							
No.	HSMV No.	Date	Day	Time	Driver 1 Age	Alcohol/Drugs Involved	Lighting Condition	Roadway Surface	Weather	Number of Fatalities	Number of Injuries	Most Severe Injury	Harmful Event	Property Damage	Contributing Cause
343	25355084	9/21/22	Wed	8:30	46	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$10,000	Careless Driving
344	25355086	9/21/22	Wed	14:56	22	0	Daylight	Wet	Rain	0	0	0	Rear End	\$4,000	Careless Driving
345	25355090	9/22/22	Thu	20:10	16	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$4,100	Careless Driving
346	25355092	9/22/22	Thu	8:50	38	0	Daylight	Dry	Clear	0	0	0	Angle	\$30,000	Leaving Scene
347	25355104	9/29/22	Thu	14:47	39	0	Daylight	Dry	Rain	0	0	0	Rear End	\$3,000	License Suspended
348	25355105	9/30/22	Fri	9:56	43	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$200	N/A
349	25355122	10/5/22	Wed	8:52	88	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	License Suspended
350	25355126	10/8/22	Sat	23:31	17	0	Dark (SL)	Dry	Clear	0	0	0	Angle	\$32,500	Careless Driving
351	25355131	10/11/22	Tue	19:40	0	0	Dusk	Dry	Clear	0	0	0	Rear End	\$0	N/A
352	25355132	10/12/22	Wed	7:34	50	0	Dawn	Dry	Cloudy	0	0	0	Sideswipe	\$1,000	License Suspended
353	25355140	10/15/22	Sat	20:30	0	0	Dark (SL)	Dry	Clear	0	0	0	Sideswipe	\$500	N/A
354	25355155	10/26/22	Wed	18:06	63	0	Daylight	Dry	Clear	0	2	0	Rear End	\$4,000	N/A
355	25355170	11/3/22	Thu	13:21	44	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	FTY at Intersection
356	25355171	11/3/22	Thu	19:02	21	0	Dark (SL)	Dry	Clear	0	2	0	Rear End	\$18,000	Careless Driving
357	25355195	11/18/22	Fri	10:15	31	0	Daylight	Dry	Clear	0	0	0	Off Road	\$2,000	Careless Driving
358	25355200	11/22/22	Tue	14:52	20	0	Daylight	Wet	Rain	0	0	0	Left Turn	\$10,000	No Insurance
359	25355214	12/1/22	Thu	16:50	36	0	Daylight	Dry	Clear	0	0	0	Backed Into	\$500	N/A
360	25355213	12/1/22	Thu	14:09	54	0	Daylight	Dry	Clear	0	3	1	Left Turn	\$30,000	Careless Driving
361	25355218	12/2/22	Fri	23:15	19	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$2,501	N/A
362	25355220	12/4/22	Sun	19:15	28	0	Dusk	Dry	Clear	0	1	0	Left Turn	\$50,000	Improper Backing
363	25355226	12/6/22	Tue	19:37	23	0	Dark (SL)	Dry	Clear	0	0	0	Angle	\$5,500	N/A
364	25355230	12/11/22	Sun	15:18	38	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$1,000	Careless Driving
365	25355232	12/12/22	Mon	18:10	16	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$30,000	Careless Driving
366	25355240	12/14/22	Wed	7:08	52	0	Daylight	Wet	Cloudy	0	0	0	Rear End	\$10,000	Careless Driving
367	25355244	12/16/22	Fri	23:00	0	0	Dark (SL)	Dry	Clear	0	0	0	Off Road	\$5,000	N/A
368	25355251	12/20/22	Tue	18:00	70	0	Dusk	Wet	Clear	0	0	0	Rear End	\$500	N/A
369	25355263	12/30/22	Fri	12:43	58	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$8,000	Careless Driving
370	25355271	1/4/23	Wed	19:25	18	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$0	Careless Driving
371	25355272	1/5/23	Thu	8:37	48	0	Daylight	Dry	Clear	0	0	0	Left Turn	\$10,000	Careless Driving
372	25355297	1/17/23	Tue	7:00	16	0	Dawn	Dry	Clear	0	1	0	Rear End	\$6,000	Careless Driving
373	25355313	1/29/23	Sun	20:29	18	0	Dark (SL)	Dry	Clear	0	1	0	Left Turn	\$5,000	Careless Driving
374	25355315	1/31/23	Tue	14:40	53	0	Daylight	Dry	Clear	0	0	0	Rear End	\$6,000	Careless Driving
375	25355324	2/7/23	Tue	11:43	25	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,000	Careless Driving
376	25355343	2/19/23	Sun	10:05	77	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$2,000	FTY at Intersection
377	25355344	2/20/23	Mon	7:05	20	0	Daylight	Dry	Clear	0	0	0	Right Turn	\$2,000	Careless Driving
378	25355346	2/21/23	Tue	6:27	24	0	Dawn	Dry	Clear	0	0	0	Rear End	\$1,500	Careless Driving
379	25355357	2/26/23	Sun	13:55	45	0	Daylight	Dry	Clear	0	0	0	Angle	\$20,000	N/A
380	25355361	2/27/23	Mon	10:10	0	0	Daylight	Dry	Clear	0	1	0	Rear End	\$1,001	N/A
381	25355375	3/2/23	Thu	14:28	20	0	Daylight	Dry	Clear	0	0	0	Rear End	\$7,000	Careless Driving
382	25355379	3/7/23	Tue	11:38	29	0	Daylight	Dry	Clear	0	0	0	Sideswipe	\$4,000	Careless Driving
383	25355412	3/24/23	Fri	13:44	18	0	Daylight	Dry	Clear	0	0	0	Rear End	\$4,200	FTYRW
384	25355443	4/10/23	Mon	20:43	53	0	Dark (SL)	Wet	Clear	0	0	0	Left Turn	\$10,000	FTY at Intersection
385	25355453	4/13/23	Thu	20:32	61	0	Dark (SL)	Dry	Clear	0	0	0	Rear End	\$2,000	FTYRW
386	25355476	4/25/23	Tue	16:29	47	0	Daylight	Dry	Clear	0	0	0	Rear End	\$15,000	Careless Driving
387	25355488	5/4/23	Thu	18:39	22	0	Daylight	Dry	Clear	0	0	0	Rear End	\$10,000	Careless Driving
388	25355492	5/5/23	Fri	15:27	52	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$3,000	Careless Driving
389	25355495	5/6/23	Sat	12:07	22	0	Daylight	Dry	Cloudy	0	0	0	Sideswipe	\$3,000	Careless Driving
390	25355503	5/10/23	Wed	17:50	42	0	Daylight	Dry	Clear	0	0	0	Rear End	\$1,500	Careless Driving
391	25355513	5/16/23	Tue	8:45	44	0	Daylight	Dry	Clear	0	0	0	Angle	\$4,000	FTYT Oncoming Traffic
392	25355536	5/31/23	Wed	15:59	22	0	Daylight	Dry	Cloudy	0	0	0	Rear End	\$10,000	Careless Driving
393	25355538	6/1/23	Thu	9:22	18	0	Daylight	Dry	Clear	0	1	0	Left Turn	\$20,000	Careless Driving
394	25355544	6/2/23	Fri	18:28	18	0	Daylight	Dry	Clear	0	0	0	Rear End	\$3,000	N/A
395	25355542	6/3/23	Sat	15:33	69	0	Daylight	Wet	Rain	0	0	0	Sideswipe	\$1,000	N/A
396	25355557	6/6/23	Tue	21:10	17	0	Dark (SL)	Dry	Clear	0	0	0	Head On	\$12,000	FTY at Intersection
397	25355547	6/6/23	Tue	11:33	32	0	Daylight	Dry	Clear	0	0	0	Rear End	\$5,000	Improper Right Turn
398	25355562	6/10/23	Sat	23:00	0	0	Dark (SL)	Wet	Clear	0	0	0	Off Road	\$5,000	N/A
399	25355564	6/13/23	Tue	20:33	29	0	Dusk	Dry	Clear	0	2	0	Rear End	\$500	FTY at Intersection

COLLISION DATA

Section: 77170-000
 State Road: 434
 Intersection Roadway: Mitchell Hammock Rd to SR 426 - CR 419
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MP: 3.038 - 4.058
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Study Period		From	1/11/2019 to 12/27/2023			59 Months										
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0%	9%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%				
CONTRIBUTING CAUSE																
Alcohol Under Influence	Drugs Under Influence	Careless Driving	FTYRW	No Insurance	Leaving Scene	Followed Too Closely	Ran Off Road	DUI	Disregarded Stop Sign	Improper Lane Change	Improper Passing	Failed To Report Crash	Insecure Load	Improper Backing	No Vehicle Registration	
8	1	268	24	2	3	3	0	2	0	3	2	2	1	7	2	
2%	0%	60%	5%	0%	1%	1%	0%	0%	0%	1%	0%	0%	0%	2%	0%	
Failure To Obey Traffic Signal	Other	Vehicle Modified	Driver License Suspended	FTYT Oncoming Traffic	Improper License	Failed To Show License	Improper Left Turn	Improper Right Turn	Improper U-Turn	Ran Stop Sign	Ran the Red Light	Failed To Yield/Stop at Intersection	Failed To Obey Traffic Control Device	Failed To Stay In Single Lane	N/A	
0	1	0	14	8	3	3	0	2	1	0	0	18	3	1	71	
0%	0%	0%	3%	2%	1%	1%	0%	0%	0%	0%	0%	4%	1%	0%	16%	

Appendix L

Sunshine One Call



UTILITY:	AFFILITATION:	STREET ADDRESS:	CITY:	STATE:	ZIP CODE:	CONTACT NAME:	CONTACT EMAIL:	CONTACT PHONE:
Telecom.	Zayo Group	130 N Main St	Butte	MT	59701	Henry Klobucar	n/a	406-496-6510
Utilities	City of Oviedo	400 Alexandria Blvd	Oviedo	FL	32765	Lawson Hodge	n/a	407-971-5695
Utilities	Seminole County	500 West Lake Mary Blvd	Sanford	FL	32773	Paul Zimmerman	n/a	407-665-2040
Traffic Sig.	Seminole County Traffic Engineering	140 Bush Loop	Sanford	FL	32773	Keith Brown	n/a	321-377-2405
Electric	Duke Energy	1150 Greenwood Blvd	Lake Mary	FL	32746	n/a	n/a	407-629-1010
Telecom.	Smart City	3100 Bonnett Creek Rd	Lake Buena Vista	FL	32830	Guy Bower	n/a	407-828-6744
Telecom.	Centurylink	1325 Bairstone Rd, Room 13	Tallahassee	FL	32301	Bill McCloud	n/a	850-599-1444
Telecom.	Lumen	1025 Eldorado Blvd	Broomfield	CO	80021	n/a	n/a	877-366-8344 ext. 2
Telecom.	MCI	400 International Pkwy	Richardson	TX	75081	n/a	n/a	800-624-9675 ext. 2
Utilities	TECO Peoples Gas	8416 Palm River Rd	Tampa	FL	33619	Joan Domning	n/a	813-275-3783
Telecom.	AT&T	6628 Lakeside Rd	West Palm Beach	FL	33411	Dino Farruggio	n/a	561-683-2729
Fiber	Uniti Fiber, LLC.	107 St Francis St, Suite 1800	Mobile	AL	36602	Charlie Croft	n/a	251-214-7059
Telecom.	Charter Communications	3767 All American Blvd	Orlando	FL	32810	John Smith	n/a	407-532-8520
Telecom.	Embarq Communications	952 1st St	Altamonte Springs	FL	32701	Robert Godek	n/a	407-830-3421

Todd Davis

From: Sunshine 811 Exactix <no-reply@exactix.sunshine811.com>
Sent: Tuesday, January 9, 2024 2:12 PM
To: Courtney Orlando
Subject: SSOCOF CONFRM 2024/01/09 #00000 009405629-000 NORM DSGN NEW

CONFRM 00000 CALL SUNSHINE 01/09/24 14:11:32ET 009405629-000 DESIGN GRID Ticket : 009405629 Rev:000 Taken: 01/09/24 14:11ET

State: FL Cnty: SEMINOLE GeoPlace: OVIEDO
CallerPlace: OVIEDO
Subdivision:

Address :
Street : E MITCHELL HAMMOCK RD

Locat: LOCATE 500' RIGHT OF WAY TO RIGHT WAY

Remarks : DESIGN ONLY

IN RESPONSE TO RECEIPT OF A DESIGN TICKET, SSOCOF PROVIDES THE ORIGINATOR OF THE DESIGN TICKET WITH A LIST OF SSOCOF MEMBERS IN THE VICINITY OF THE DESIGN PROJECT. SSOCOF DOES NOT NOTIFY SSOCOF MEMBERS OF THE RECEIPT BY SSOCOF OF A DESIGN TICKET. IT IS THE SOLE RESPONSIBILITY OF THE DESIGN ENGINEER TO CONTACT SSOCOF MEMBERS TO REQUEST INFORMATION ABOUT THE LOCATION OF SSOCOF MEMBERS' UNDERGROUND FACILITIES. SUBMISSION OF A DESIGN TICKET WILL NOT SATISFY THE REQUIREMENT OF CHAPTER 556, FLORIDA STATUTES, TO NOTIFY SSOCOF OF AN INTENT TO EXCAVATE OR DEMOLISH. THAT INTENT MUST BE MADE KNOWN SPECIFICALLY TO SSOCOF IN THE MANNER REQUIRED BY LAW. IN AN EFFORT TO SAVE TIME ON FUTURE CALLS, SAVE YOUR DESIGN TICKET NUMBER IF YOU INTEND TO BEGIN EXCAVATION WITHIN 90 DAYS OF YOUR DESIGN REQUEST. THE DESIGN TICKET CAN BE REFERENCED, AND THE INFORMATION ON IT CAN BE USED TO SAVE TIME WHEN YOU CALL IN THE EXCAVATION REQUEST.

*** LOOKUP BY MANUAL ***

*** Boundary: n 28.670878 s 28.654838 w -81.209379 e -81.207178

Grids : 2839A8112B 2839A8112C 2839B8112B 2839B8112C 2839C8112B
Grids : 2839C8112C 2840C8112B 2840C8112C 2840D8112B 2840D8112C

Work date: 01/09/24 Time: 14:03ET Hrs notc: 000 Category: 6 Duration: UNKNOWN Due Date : 01/11/24 Time: 23:59ET
Exp Date : 02/08/24 Time: 23:59ET Work type: DESIGN Boring: N White-lined: N
Ug/Oh/Both: U Machinery: N Depth: UNK Permits: N N/A Done for : DESIGN

Company : PROTEAN DESIGN GROUP Type: CONT Co addr : 100 EAST PINE ST Co addr2: SUITE 600
City : ORLANDO State: FL Zip: 32301
Caller : KAREN VAN DEN AVONT Phone: 407-246-0044 Ext: 113
BestTime: 8AM TO 5PM
Email : CORLANDO@PROTEANDG.COM

Submitted: 01/09/24 14:11ET Oper: KAR Chan: WEB Mbrs :
CNTL01 BILL MCCLLOUD 850-599-1444
CENTURYLINK
1325 BLAIRSTONE RD RM 113

TALLAHASSEE, FL 32301

Level 1: CASE BY CASE BASIS

Level 2: CASE BY CASE BASIS

Level 3: CASE BY CASE BASIS

Level 4: CASE BY CASE BASIS

CVCFTV JOHN SMITH 407-532-8520

CHARTER COMMUNICATIONS

3767 ALL AMERICAN BLVD

ORLANDO, FL 32810

Level 1: \$91.50 PER HR / 2 HR MINIMUM REQUEST WILL NEED TO BE IN WRITING

Level 2: \$91.50 PER HR / 2 HR MINIMUM REQUEST WILL NEED TO BE IN WRITING

Level 3: \$55.54 PER HOUR / 2 HOUR MINIMUM

Level 4: SERVICES NOT PROVIDED BY MEMBER

FLW941 HENRY KLOBUCAR 406-496-6510

ZAYO GROUP / FORMERLY LIGHTWAVE, LLC

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BUTTE, MT 59701

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Level 2: SERVICES NOT PROVIDED BY MEMBER

Level 3: SERVICES NOT PROVIDED BY MEMBER

Level 4: EXTRA ORDINARY EXPOSURE TIME AND EXPENSE

FPC322 DUKE'S CUSTOMER SERVICE CEN 407-629-1010

DUKE ENERGY

1150 GREENWOOD BLVD

LAKE MARY, FL 32746

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Level 2: SERVICES NOT PROVIDED BY MEMBER

Level 3: SERVICES NOT PROVIDED BY MEMBER

Level 4: SERVICES NOT PROVIDED BY MEMBER

L3C900 NETWORK RELATIONS 877-366-8344 Ext: 2

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1025 ELDORADO BLVD

BROOMFIELD, CO 80021

Level 1: CONTACT MEMBER DIRECTLY FOR FEE SCALE

Level 2: CONTACT MEMBER DIRECTLY FOR FEE SCALE

Level 3: CONTACT MEMBER DIRECTLY FOR FEE SCALE

Level 4: CONTACT MEMBER DIRECTLY FOR FEE SCALE

MCIU01 INVESTIGATIONS TEAM 800-624-9675 Ext: 2

MCI

400 INTERNATIONAL PKWY

RICHARDSON, TX 75081

Level 1: \$0

Level 2: SERVICES NOT PROVIDED BY MEMBER

Level 3: SERVICES NOT PROVIDED BY MEMBER

Level 4: SERVICES NOT PROVIDED BY MEMBER

OVI795 LAWSON HODGE 407-971-5695

CITY OF OVIEDO FAX 407-971-5808

400 ALEXANDRIA BLVD

OVIEDO, FL 32765

Level 1: SERVICES NOT PROVIDED BY MEMBER

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Level 3: SERVICES NOT PROVIDED BY MEMBER
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PGSORL JOAN DOMNING 813-275-3783

TECO PEOPLES GAS- ORLANDO
8416 PALM RIVER RD
TAMPA, FL 33619

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SC1284 GUY BOWER 407-828-6744

SMART CITY TELECOM
3100 BONNETT CREEK RD
LAKE BUENA VISTA, FL 32830

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SCT994 KEITH BROWN 321-377-2405
SEMINOLE COUNTY TRAFFIC ENGINEERING FAX 407-665-5623
140 BUSH LOOP
SANFORD, FL 32773

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Level 3: SERVICES NOT PROVIDED BY MEMBER
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SEM814 PAUL ZIMMERMAN 407-665-2040
SEMINOLE COUNTY FAX 407-665-2029
500 WEST LAKE MARY BLVD
SANFORD, FL 32773

Level 1:
Level 2:
Level 3:
Level 4:

SL1086 CHARLIE CROFT 251-214-7059
UNITI FIBER LLC
107 ST FRANCIS ST, SUITE 1800
MOBILE, AL 36602

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Level 4: SERVICES NOT PROVIDED BY MEMBER

SMN407 ROBERT GODEK 407-830-3421

EMBARQ COMMUNICATIONS INC. FAX 407-260-2683

952 1ST ST.

ALTAMONTE SPRINGS, FL 32701

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Level 4: SERVICE NOT PROVIDED BY MEMBER

Todd Davis

From: Thompson, Cheyenne <CThompson2@tecoenergy.com>
Sent: Wednesday, February 7, 2024 1:19 PM
To: Courtney Orlando
Subject: Sunshine Ticket#009405629
Attachments: 04-Orlando.doc

Please see attached document & map below. Thank you,

Cheyenne Thompson

Admin Specialist SR.

Peoples Gas

Email: CThompson2@tecoenergy.com

Office Phone: 813-275-3710, ext.53710

Cell Phone: 813-743-7164



This map is provided for convenience purpose only and is not intended to be used for detail locations.

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Appendix M

Environmental Data





Figure M-1 - Wetlands

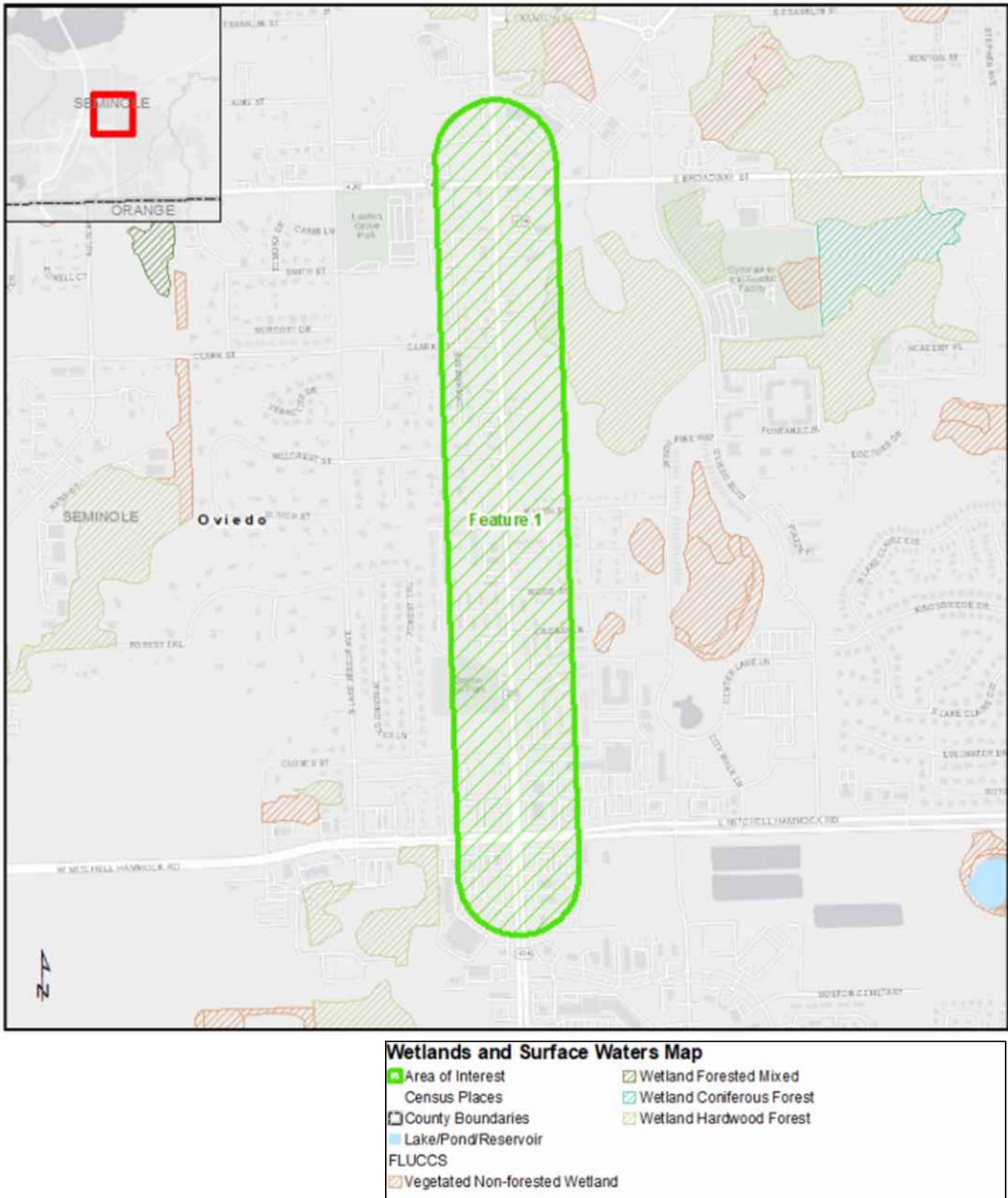
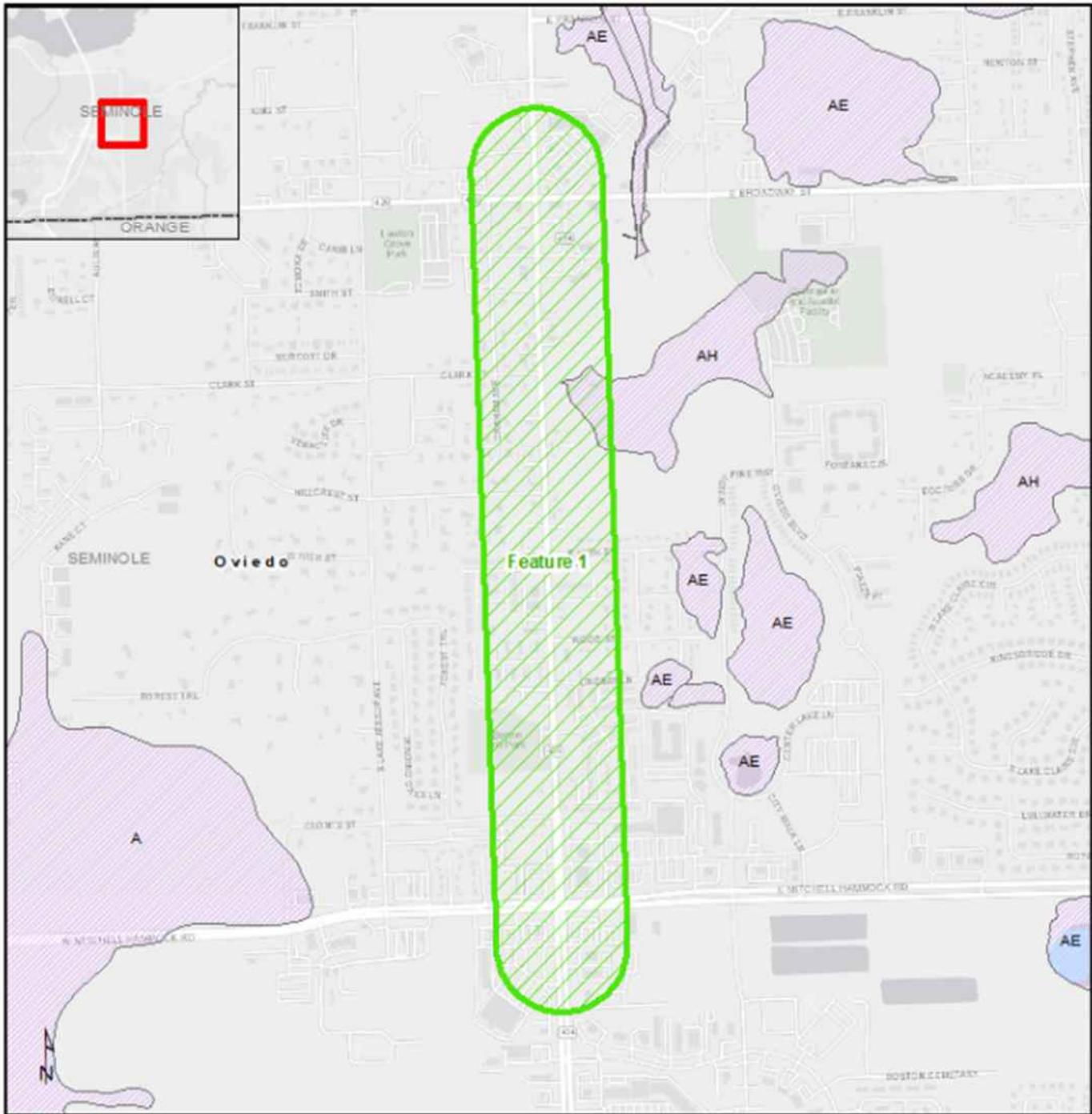




Figure M-2 – Floodplains

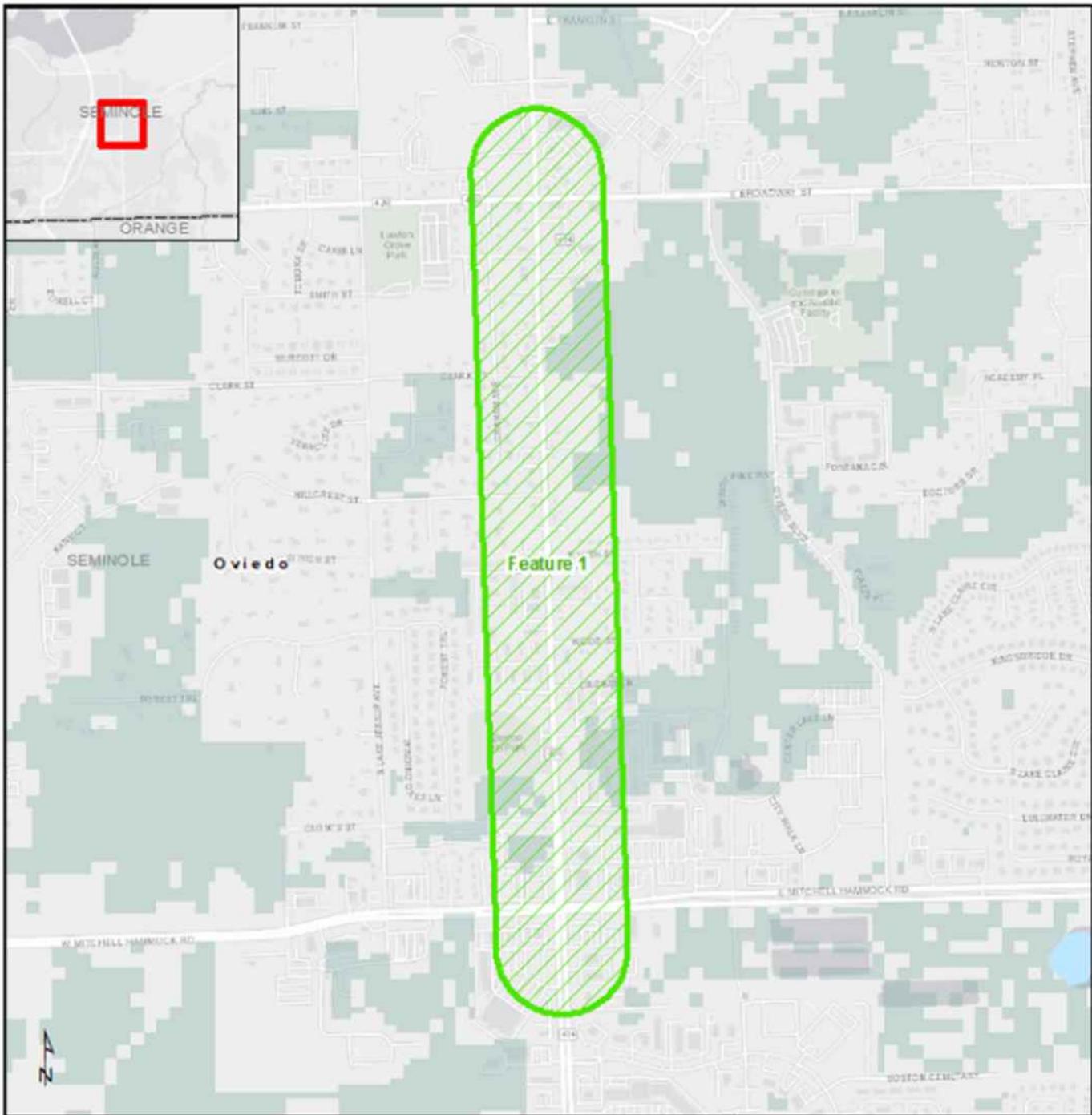


Floodplains Map

- Area of Interest
- Census Places
- County Boundaries
- Special Flood Hazard Area
- Lake/Pond/Reservoir



Figure M-3 – Species Potential

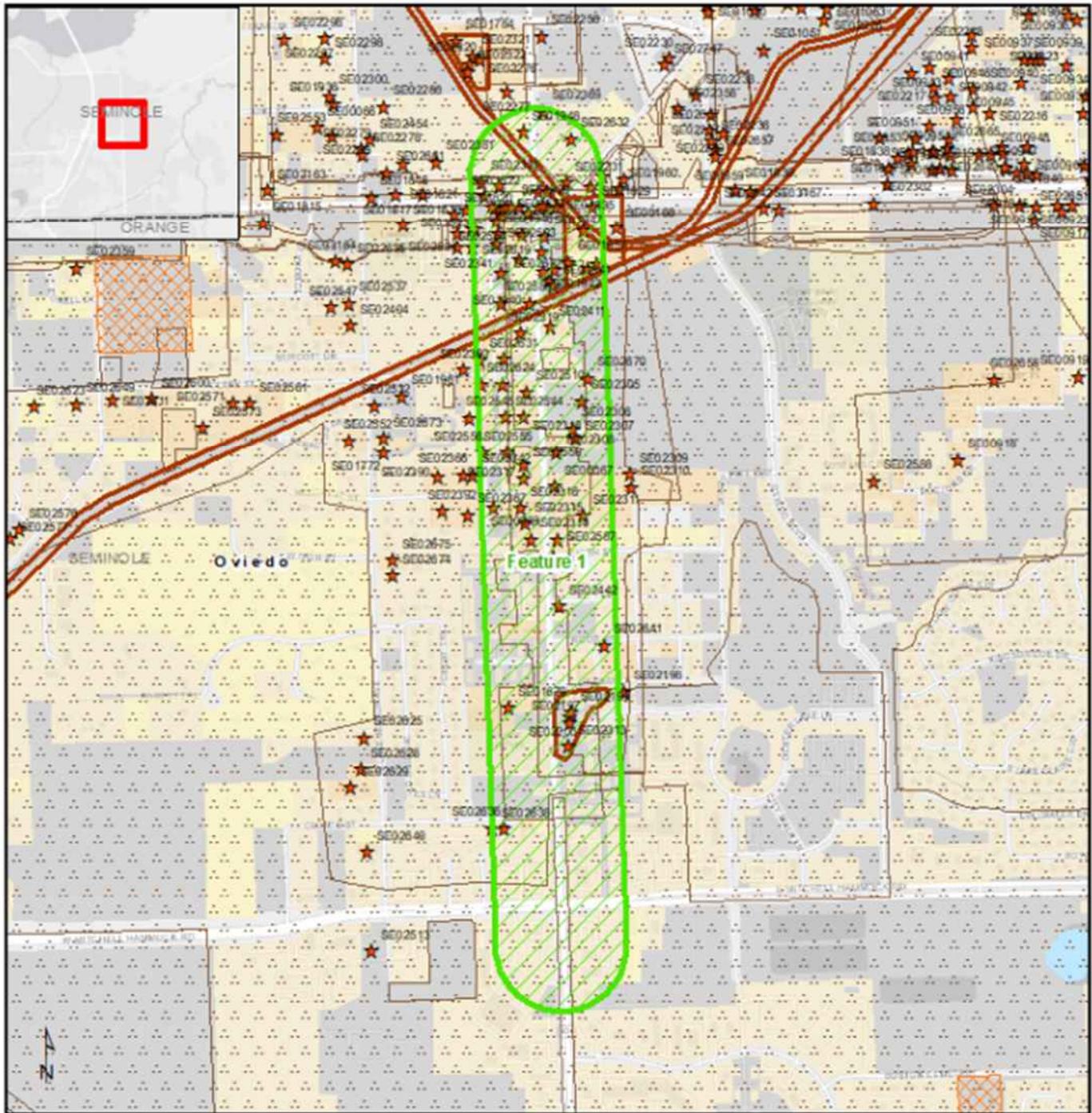


Species Potential Map

- Area of Interest
- Census Places
- County Boundaries
- Lake/Pond/Reservoir
- VALUE
- 3 - 5 Species



Figure M-4 – Historic Resource Map

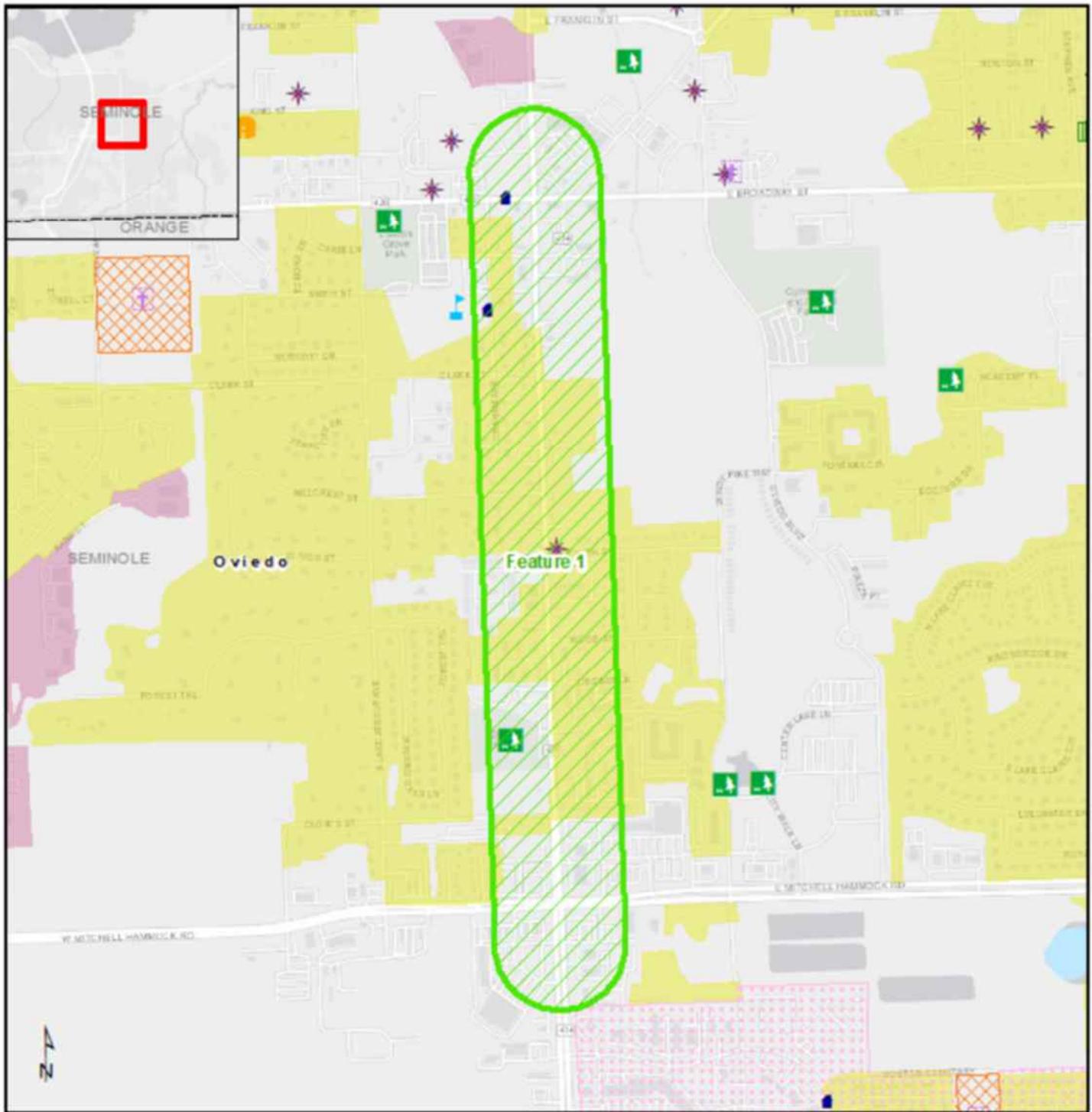


Historic Resource Map

- Area of Interest
- Cultural Resource Field Survey Area
- Census Places
- Lake/Pond/Reservoir
- Historic Structure
- Parcels (Forty to Forty-nine years old)
- County Boundaries
- Pre 1970
- Historic Cemetery
- Post 1970
- Historic Resource Group
- Parcels with no values



Figure M-5 – Noise

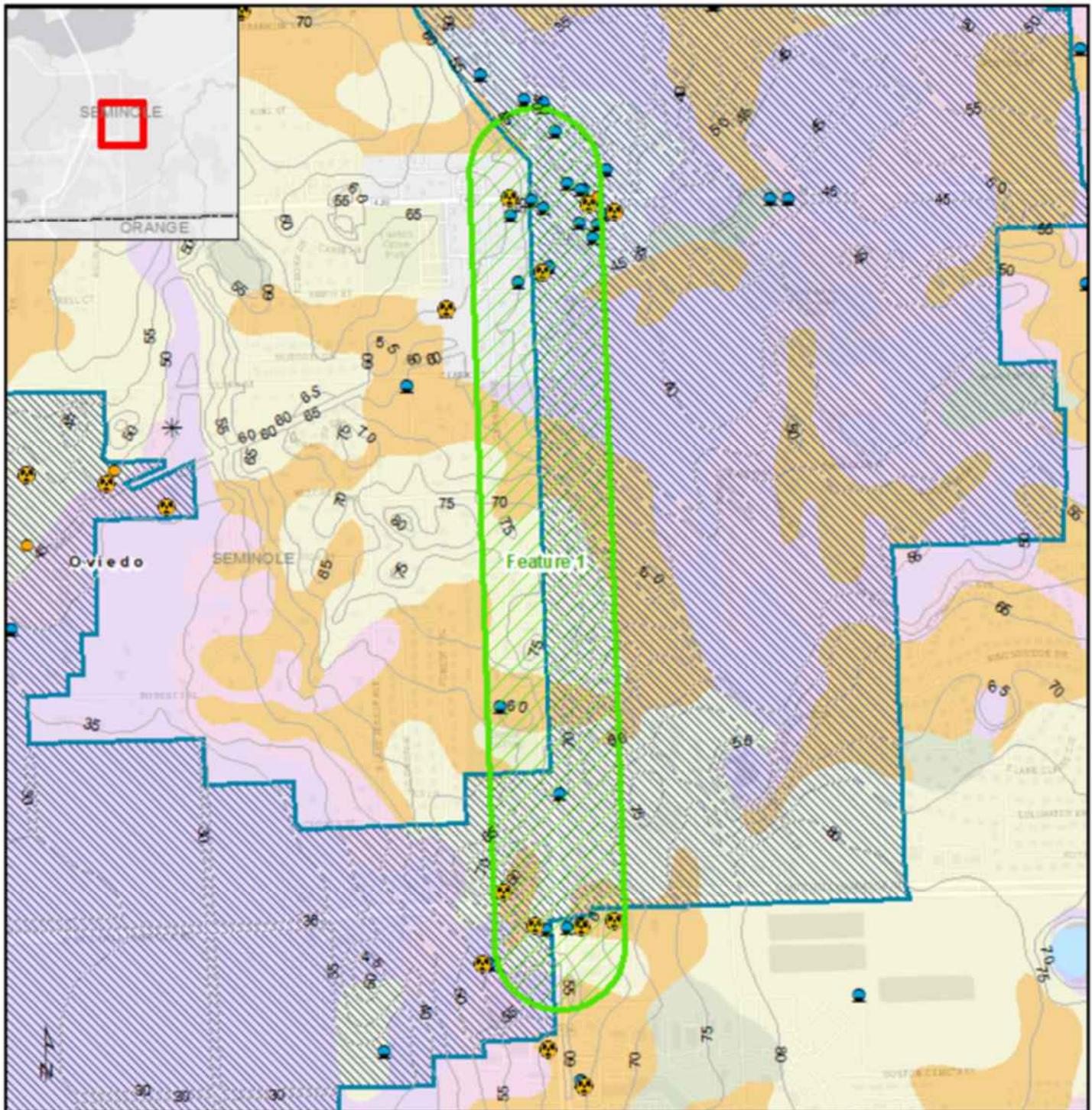


Noise Map

Area of Interest	Park	National Park Projects
Census Places	Place of Worship	DRI
Group Care Facilities	School	Lake/Pond/Reservoir
Cemetery	County Boundaries	Residential
Community Center	Historic Cemetery	Industrial



Figure M-6 - Contamination

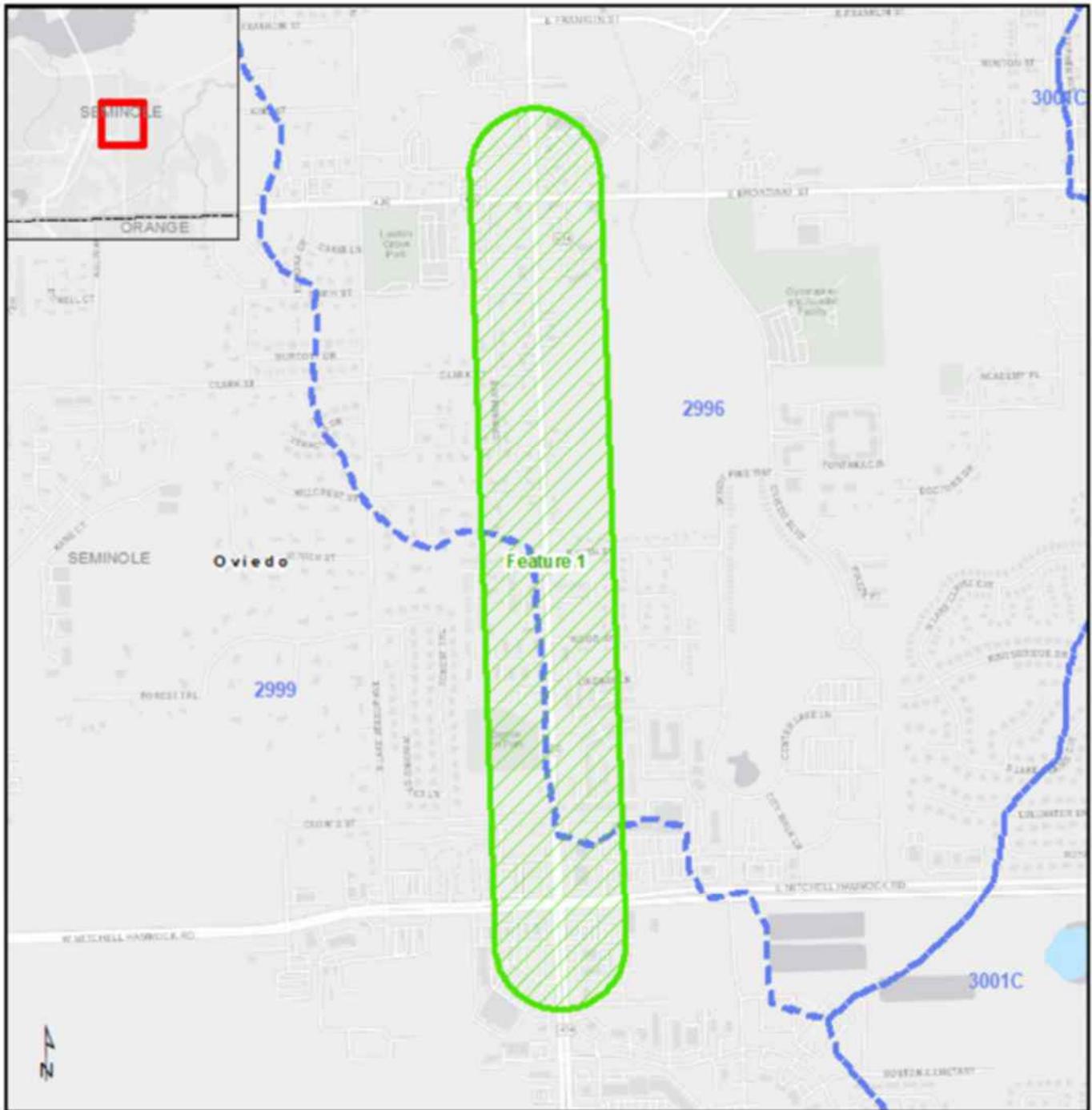


Contamination Map

Area of Interest	5 FT Contour	Somewhat Poorly Drained
Toxic Release Inventory	County Boundaries	Poorly Drained
Solid Waste Facility	Brownfield Area	Very Poorly Drained
Hazardous Material Site	Soil Drainage	Lake/Pond/Reservoir
FDEP Tanks	Excessively Drained	
Census Places	Moderately Well Drained	



Figure M-7 - Water Resource Map

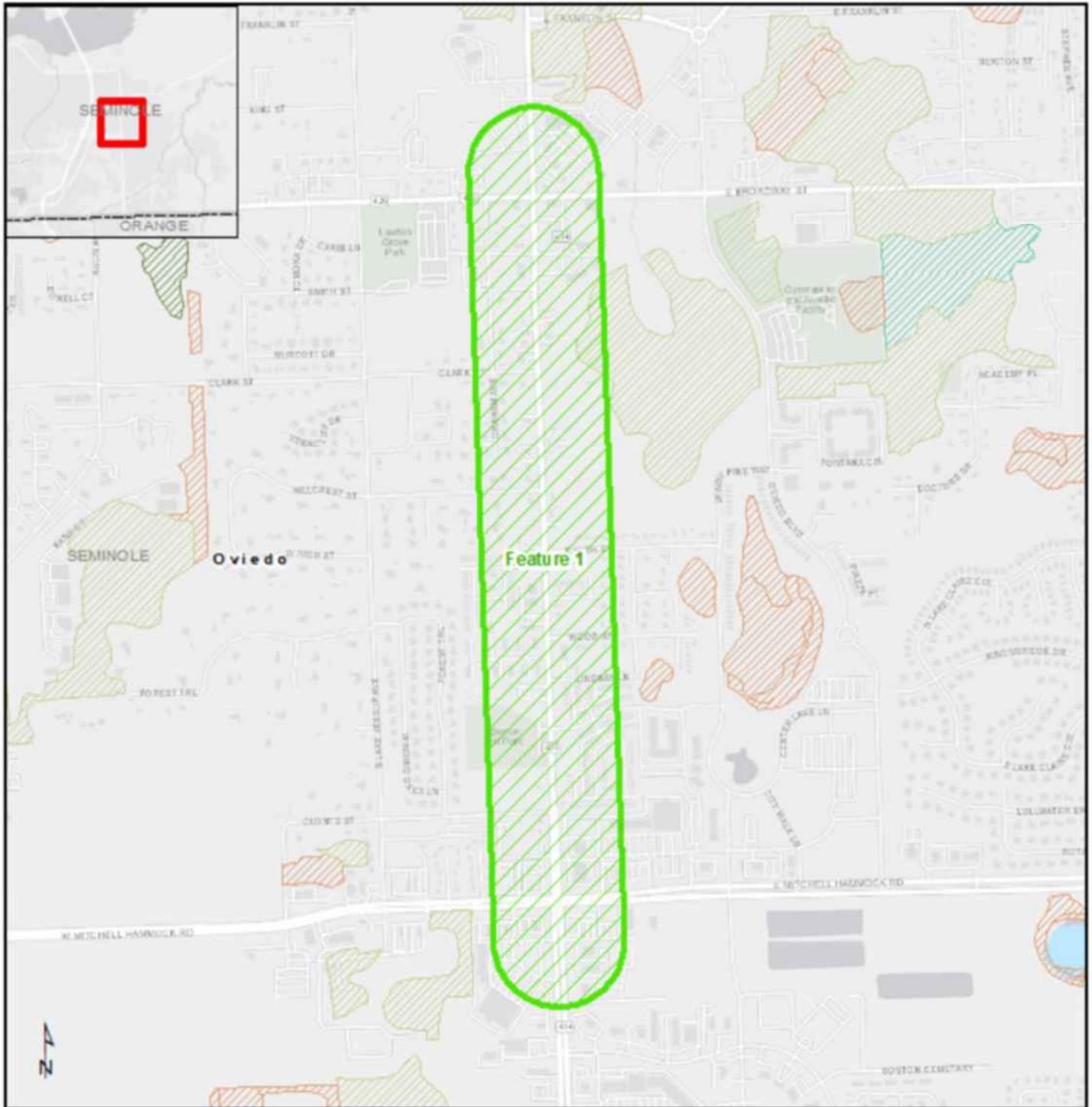


Water Resource Map

-  Area of Interest
-  Census Places
-  County Boundaries
-  Drainage Basin
-  Lake/Pond/Reservoir



Figure M-8 – Coastal and Marine Map



Coastal and Marine Map

- Area of Interest
- Census Places
- County Boundaries
- FLUCCS
- Vegetated Non-forested Wetland
- Wetland Forested Mixed
- Wetland Coniferous Forest
- Wetland Hardwood Forest
- Lake/Pond/Reservoir



ALTERNATIVES MEMORANDUM
S.R. 434 – Oviedo Corridor Study
02/04/2025

This memorandum provides an overview of the planning and evaluation of alternative options for the State Road (S.R.) 434 corridor, stretching from Mitchell Hammock Road to S.R. 426 (Broadway Street) in Oviedo, Florida, a study area spanning approximately 1.2 miles. The majority of S.R. 434 within the study limits consists of a two-lane, undivided corridor without exclusive turn lanes, extending from Lindsay Lane to Smith Street. From Clonts Street to south of Mitchell Hammock Road, the corridor is a four-lane, divided roadway. Between Clonts Street and Lindsay Lane, there is a two-way left-turn lane with exclusive left-turn lanes at Broadway Avenue, briefly creating a five-lane section. North of Smith Street to S.R. 426, the corridor transitions back to a four-lane, divided roadway with exclusive turn lanes. The study evaluates two primary alternatives for the corridor's future development: a 2-lane divided option, and a 4-lane divided option.

1. Purpose and Need

The study aims to address growing transportation demands in the area while improving safety, reducing congestion, and supporting future development in Oviedo. With increasing population and traffic volumes, the corridor is experiencing significant strain, leading to the need for upgraded infrastructure.

2. Evaluation of Alternatives

Two key alternatives were evaluated for the corridor:

- 2-lane, Divided Alternative: Two 11-ft travel lanes, 15.5-ft divided median, curb and gutter, utility strips, and sidewalks/pathways. Right of way (R/W) varies from 71.5 to 84.5 feet, impacting 44 parcels, including three buildings.
- 4-lane, Divided Alternative: Four 11-ft travel lanes, 15.5-ft divided median, curb and gutter, utility strips, and sidewalks/pathways. Right of way varies from 91.5 to 100.5 feet, impacting 48 parcels, including ten buildings.

3. Critical Elements / Factors for Review

- Boston Hill Park
- City of Oviedo Fire Station 46
- Septic Tanks and Well
- Constrained Area (52.75-ft ROW)
- Drainage
- Utilities
- Pedestrian Safety / Access
- Impacts of Construction projects
 - Chelonian Wharton
 - S.R. 426 widening (FM# 415030-6)



1.0 Introduction

The main goal of this study is to improve the safety of the SR 434 corridor by assessing speed management strategies, addressing connectivity issues, and enhancing mobility for pedestrians and cyclists. Additionally, the study took into account nearby land use and expected growth to reduce potential impacts on the surrounding area.

This memo outlines the design alternatives, focusing on key design considerations, a summary of the alternatives, potential design constraints, and important points for the upcoming Project Development and Environment (PD&E) study.

2.0 Initial Design Considerations

Before initiating the design process, several key considerations were factored into the decision of the design limits based on focusing on the project's goals, areas that should be avoided, and mandatory design elements to be incorporated, regardless of the chosen alternative.

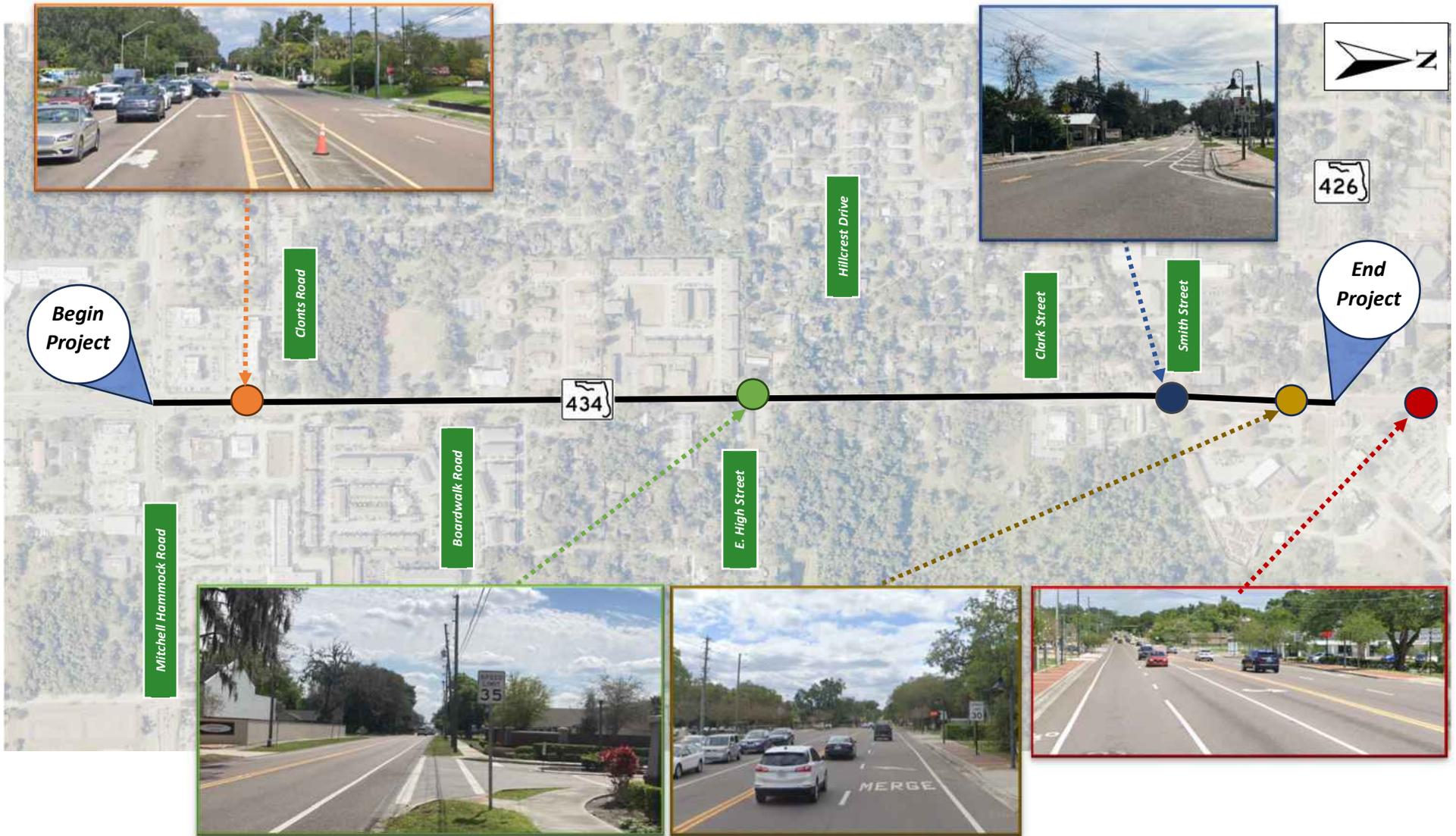
The project limits were defined to address the connectivity issues along the corridor. SR 434 serves as a major arterial road in Oviedo, Florida, supporting both commercial and residential areas. South of Mitchell Hammock Road, SR 434 is a six-lane divided facility (three lanes in each direction). From Mitchell Hammock Road to SR 426, it is a two-lane undivided facility. North of SR 426 is a four-lane undivided road (two lanes in each direction). Concurrently, SR 426 was being expanded to a four-lane divided facility (two lanes in each direction) during the period this study was conducted. Given these factors, the study limits for this project was determined to be the section from Mitchell Hammock Road to SR 426. Refer to Figure 1, next page, for the study area map.

Feedback from the key stakeholders and residents in the area highlighted peak-hour congestion as a significant issue, especially in the stretch between Clont Street and Garden Street. In this area, vehicles often have to wait for gaps in traffic to make turning movements due to frequent stops along the side streets. This segment is primarily residential, with a few local businesses situated between Clonts Street and Clark Street. Additional local businesses are expected to be developed along SR 434 north of Clark Street, which will increase connections to the corridor.

The City of Oviedo expressed a strong desire to improve connectivity between the "old downtown" area, located north of SR 426, and the "new downtown" near the Oviedo on the Park development. To address this, the city has requested the addition of a shared-use path along the east side of the corridor to support non-vehicular users. This path would also connect to the Cross Seminole Trail, enhancing safe mobility, particularly for schoolchildren walking home from school or bus drop-offs along SR 434.



Figure 1: Study Area and Existing Aerials





The right-of-way (ROW) along the corridor generally averages 65 feet in width. However, at the southern end of the study limits, from Mitchell Hammock Road to Clonts Street, the ROW expands to 100 feet. Similarly, at the northern limits, from Garden Street to the SR 426 intersection, the ROW measures 90 feet.

Existing ROW data was obtained from the Seminole County Property Appraiser's website and cross-referenced with available survey maps from the Department. The resulting ROW was then mapped using an aerial base map of the corridor (**see Appendix / Figure A-1**). Should any improvements necessitate an increase in ROW, it is recommended that the first step in any subsequent phase be the acquisition of a new survey.

To minimize the impact of potential widening in these constrained areas, careful consideration was given to the surrounding environment, focusing on reducing disruption. The following sections outline key locations where these considerations influenced the development of the alternatives.

- **Boston Hill Park:** The Boston Hill Park is located along the west side of the corridor, across from Boardwalk Avenue. Should the park be impacted, this would result in potential impacts to public lands (Section 4(f)). Therefore, the alternatives utilized an eastern alignment, avoiding any impact.

Figure 2: Boston Hill Park



- **City of Oviedo Fire Station 46:** The city fire station is located adjacent to the Boston Hill Park, on the northeast corner of the property. As this is also public land, there is the potential to meet Section 4(f). Therefore, the alternatives utilized an eastern alignment, avoiding any impact. In addition, there is an existing mast arm located on the west side of SR 434. As such, any proposed widening would require a design and structural analysis to evaluate the mast arm length and location of the emergency signal heads.



Figure 3: City of Oviedo Fire Station 46



Source: Google Maps

- Septic Tanks and Well:** Concerns have been raised regarding the potential impact of the proposed improvements on underground septic tanks and wells located within the study area. Specifically, a well has been identified at the northeast corner of S.R. 434 and Lawn Street, while septic tanks are present on both sides of S.R. 434 between Wood Street and south of Hillcrest Drive. Any construction activities or changes to the roadway may disrupt these systems, leading to potential environmental (contamination) or functional issues.

Figure 4: Septic Tanks and Well



- Constrained Area:** As previously noted, the right-of-way (ROW) along the corridor is narrow, with the most constrained section located between Sandalwood Court and Hillcrest Drive, where the ROW measures approximately 52.75 feet. A particular concern arises in the segment between Lawn Street and Mayo Court, where neighborhood walls flank both sides of the corridor



(Graceland Square to the east and the gated Central Park neighborhood to the west). Given the need for space to accommodate vehicles queuing when the gate is closed and visitors using the key code, an eastern alignment was considered.

Additionally, consideration was given to the layout of the Graceland Square townhomes. Because of their construction, if additional ROW were required, it would be possible to acquire a single end-unit without affecting the structural integrity of the entire townhome row. In contrast, a western alignment would impact multiple homes, the neighborhood park, the neighborhood gate, and the retention pond. Therefore, any proposed alignment would favor the eastern side, aligning with the eastern alignment to the south, which is influenced by the park and fire station.

Figure 5: Constrained area between Lawn Street and Mayo Court



- Drainage:** The existing stormwater management system includes a range of collection and conveyance systems that discharge to multiple outfall points along the corridor. The corridor lies within the Howell Creek and Lake Jesup watersheds. The drainage infrastructure is varied, featuring an intermittent curb and gutter with a closed drainage system along S.R. 434, except for the section between Sandalwood Court and Hillcrest Drive. The current drainage system appears to be undersized for the area, and there is potential to construct new curb/gutter and underground drainage systems, which would impact the right-of-way (ROW).
- Utilities:** Utility poles supporting overhead electrical and communication lines are primarily located behind the sidewalk along the east side of S.R. 434, with occasional overhead lines crossing to a single utility pole on the west side of the roadway. There is a brief section where the overhead utility line shifts to the west side at E. High Street, before returning to the east side at Sandalwood Court. A Sunshine One Call was conducted to identify the companies operating the various utilities along the corridor, and the details are included in the *S.R. 434 Existing Conditions Report (March 2024)*. Any proposed widening or additional impacts to the corridor will affect the overhead utilities and the associated wooden poles. Coordination with Duke Energy will be necessary to assess the feasibility of removing and replacing the poles (whether wooden or metal) or relocating the utilities underground.



Figure 6: Corridor Overhead Utilities



In addition to the highlighted areas, the following summarizes the key design elements to be considered as part of the alternative's development.

- Cross Seminole Trail: The Cross Seminole Trail currently crosses S.R. 434, south of Smith Street, and is controlled by a Rapid Rectangular Flashing Beacon (RRFB). As part of the proposed alternatives, a Pedestrian Hybrid Beacon (PHB) is recommended to reduce driver confusion and prevent potential vehicle avoidance. The PHB will include a red light, enhancing visibility and safety for both pedestrians and drivers.
- Chelonian Wharton Development (*under construction*): The residential development, which is currently under construction, is located along the east side of the corridor, across from Hillcrest Drive. As part of this project, the development is currently constructing an exclusive north/south left-turn lane along S.R. 434.
- Pedestrian Connectivity: Given the limited pedestrian crossing options along S.R. 434 between the Cross Seminole Trail and Mitchell Hammock Road, both the city and local residents have expressed a need for an additional safe, marked crosswalk in this area. As a result, several locations were evaluated. The intersection at Boston Hill Park / Boardwalk Avenue has already been designed by the City of Oviedo to provide safe access for park visitors and students using the bus stop. This crosswalk, which will include an RRFB, is awaiting funding. Further crosswalks should be considered between Lawn Street and Hillcrest Drive to accommodate local residents, students walking home from school, and those being dropped off by bus.
- Pedestrian Safety and Accessibility: In addition to improving connectivity, pedestrian safety along the corridor should be prioritized. Potential improvements could include the reconstruction of adjacent driveways to meet ADA standards, restriping or adding side-street crosswalks, and constructing smooth tie-ins. The city has also requested the addition of a shared-use path along



the east side of S.R. 434, which would be approximately 14 to 18 feet wide and as a result impact the existing ROW. To further enhance pedestrian safety, it has been suggested that landscaping be incorporated between the travel lanes and the shared-use path, offering shade and creating a barrier between pedestrians and vehicles.

- S.R. 426 widening (under construction): There is currently a major construction project to widen S.R. 426, from two to four travel lanes, from Pine Avenue to Avenue B. The project will consist of two travel lanes in each direction, separated by a 22-foot-wide raised grassed median, bike lanes, and sidewalks on both sides. The project will also include improvements to the signalized intersection at S.R. 434.

3.0 Alternatives Development

Based on the initial design considerations, constraints, and feedback outlined in the previous section, two distinct design alternatives were developed and thoroughly evaluated. These alternatives were crafted to address the specific needs of the corridor while balancing the various factors such as safety, connectivity, and community impact. In this section, we provide a detailed summary of each alternative, highlighting their key features, anticipated benefits, and potential impacts on the surrounding environment, infrastructure, and local stakeholders. The goal is to offer a clear understanding of how each alternative could shape the future of the corridor and its role in supporting both current and future transportation needs.

The first alternative included a two-lane, divided typical section. This typical included a 14-ft shared-use path and 6-ft utility strip on the east side of the corridor, as requested by the City of Oviedo. Additional parameters are:

- Two (2) 11-ft Travel Lanes
- 15.5-ft Divided Median
- Type E Curb and Gutter (inside)
- Type F Curb and Gutter (outside)
- 2-ft Utility Strip on the west side
- 6-ft Concrete Sidewalk on the west side
- 2-ft Tie Down on each side (subject to variation based on location and profile)

The total right-of-way (ROW) required for this 2-lane, divided typical section ranges from 71.5 feet to 84.5 feet, depending on the specific area. To minimize the impacts on surrounding properties and communities, an eastern alignment was chosen between Clonts Road and E. High Street, with a center alignment used north of E. High Street. This alignment strategy is particularly significant as it avoids any direct impact to the Boston Hill Park and the City of Oviedo Fire Station 46, preserving these critical community assets. The existing sidewalk on the west side of the corridor, stretching from Clonts Street to Sandalwood Court, will remain intact under this alternative. However, despite efforts to limit disruptions, this alternative will affect a total of 44 parcels due to the proposed widening of the corridor, potentially involving property acquisitions or easements, including three buildings.

The typical section can be seen in the **Appendix / Figure A-2**.



Additional improvements included:

- Constructing an exclusive left-turn lane at Clark Street
- PHB at the Cross Seminole Trail Crossing
- RRFB at the Boston Hill Park / Boardwalk Avenue intersection (per City's request)

The second alternative included a widening from two-lane to four-lane, divided typical section. This typical included a 12-ft shared-use path (with a maximum of 14-ft), and a 2-ft utility strip on the east side of the corridor, as requested by the City of Oviedo. Due to the existing ROW constraints, a 2-ft utility strip was the maximum width proposed. Additional parameters are:

- Four (4) 11-ft Travel Lanes
- 15.5-ft Divided Median
- Type E Curb and Gutter (inside)
- Type F Curb and Gutter (outside)
- 2-ft Utility Strip on both sides
- 6-ft Concrete Sidewalk on the west side
- 2-ft Tie Down on each side (subject to variation based on location and profile)

The total right-of-way (ROW) required for the 4-lane, divided typical section ranges from 91.5 feet to 100.5 feet, depending on the specific segment of the corridor. In order to minimize the impact on surrounding properties and communities, an eastern alignment was selected for the entire length of the corridor within the study area. This alignment is particularly advantageous as it avoids any direct impacts to Boston Hill Park and the City of Oviedo Fire Station 46, helping to preserve these important community resources. Despite these efforts to minimize disruptions, the proposed 4-lane widening alternative will affect a total of 48 parcels, including potential property acquisitions or easements. Additionally, ten buildings may be impacted, further complicating the planning process and potentially requiring relocation or adjustments to current land use. Notable impacts include Church of God of Prophecy at the E. High Street intersection and two schools (Oviedo Montessori Bilingual Academy and The Goddard School of Oviedo) at the Clark Street intersection.

The typical section can be seen in the **Appendix / Figure A-3**.

Additional improvements included:

- PHB at the Cross Seminole Trail Crossing
- RRFB at the Boston Hill Park / Boardwalk Avenue intersection (per City's request)



4.0 Design Limitations and Future Design Considerations

This section outlines the key limitations and constraints encountered during the development of the design alternatives. These limitations stem from a variety of factors, including existing infrastructure, environmental considerations, community impacts, and regulatory requirements. Understanding these challenges is essential for evaluating the feasibility of the proposed alternatives and ensuring the success of the final design. The following discussion highlights the most significant design limitations and their implications for the proposed improvements.

- **Survey:** Survey data was not provided for this analysis and was instead based on aerial imagery and field verification. As a result, detailed roadway profiles were not assessed. It is recommended that these profiles be thoroughly examined during the Project Development and Environment (PD&E) phase, as additional ROW may be necessary depending on the profile conditions. Additionally, there are vertical roadway curves that could affect sight distance and other design elements, which should be carefully considered in future phases of design.
- **Median / Turn Lanes:** As part of the proposed alternatives, no specific median locations were identified or presented. Therefore, additional attention should be given to determining the placement of median openings and exclusive left-turn lanes, particularly considering the offset intersections and limited access for adjacent properties. A detailed analysis should be conducted to identify appropriate turn lane locations and median openings, ensuring compliance with Rule 14-97, F.A.C. Furthermore, the design should incorporate the ability to conduct u-turns.
- **Other Considerations:** In addition, elements such as corridor lighting, drainage, signage, and landscaping should be thoroughly evaluated during the PD&E phase to ensure their effectiveness and integration with the overall design.

5.0 Future Design Considerations

As the project progresses into subsequent phases (i.e., PD&E), several key design considerations will need to be carefully evaluated and addressed. These considerations encompass a variety of factors, including but not limited to, survey, environmental impact assessments, and coordination with utility providers. Additionally, further studies and stakeholder engagement will be essential to refine the design and ensure that the final solution aligns with both regulatory requirements and community needs. The following section outlines the critical design elements and actions that should be prioritized in future stages to ensure a comprehensive and effective development of the corridor.

- Identify a preferred alternative and gain consensus.
- Survey will be required to confirm design elements and ROW boundaries.
- Survey of underground drainage structures to determine the need to reconstruct structures, with construction of new curb and gutter.
- Identify median openings and exclusive left-turn locations (in accordance with 14-97, F.A.C.)



- Construct an exclusive left-turn lane at the Clark Street intersection.
- Consideration in maintaining exclusive left-turn lane into the Pharmacy on the Park (owner request)
- Additional crosswalk(s):
 - City requested funding for crosswalk at Boston Hill Park / Boardwalk Avenue (consider PHB)
 - Identify location between Lawn Street and Hillcrest Drive
- Connectivity concerns with adjacent properties, based on the identification of median openings.
- Conduct an AutoTURN analysis to assess the turning radii. This should accommodate delivery trucks, where applicable, and u-turns, based on the location of the median openings. Additional ROW may be required to accommodate u-turns.
- ROW estimates were provided within the study; however, additional ROW may be needed to accommodate tie-down slopes and/or drainage requirements.
- Acquisition of additional land may be required for a pond, based on detailed drainage analysis.

6.0 Recommendations

Based on the analysis of the design alternatives, as well as the identified limitations and community feedback, the following recommendations are provided to guide the next steps in the planning and design process. The following section outlines specific actions and considerations to ensure that the project moves forward effectively, with a focus on long-term sustainability and minimal disruption to the surrounding area.

Feedback from the study Community Event was mixed, with some attendees supporting the 4-lane option while others opposed any improvements. Concerns were raised about speeding and queuing at the signalized intersections at the north and south project limits. Several short-term improvements were suggested, including marked crosswalks and exclusive turn lanes at specific locations.

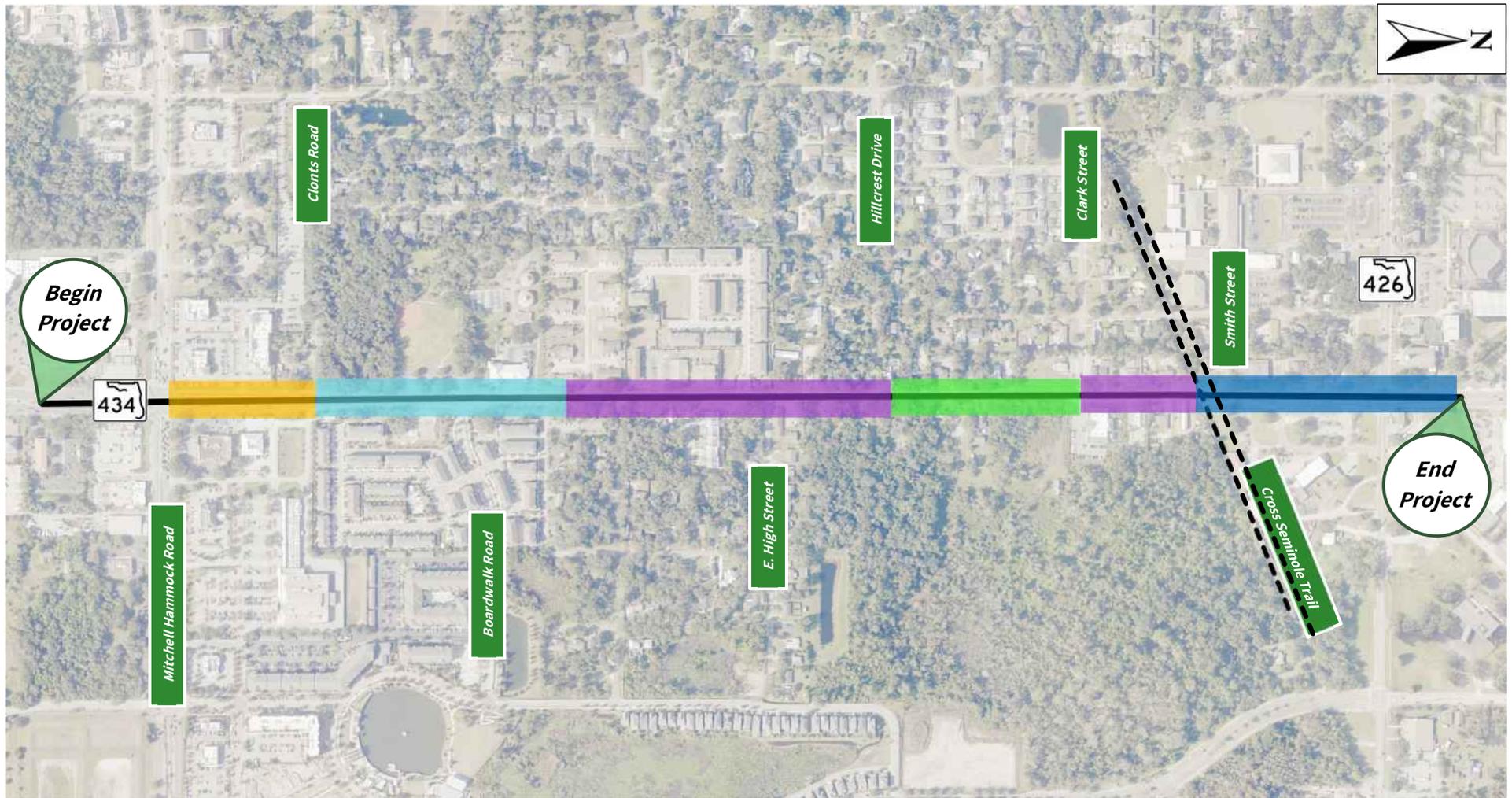
- Short-term Improvements: These include (1) marked crosswalks at Boardwalk/Boston Hill Park, (2) a pedestrian hybrid beacon at the Cross Seminole Trail crossing, (3) restriping for marked crosswalks, (4) sidewalk maintenance, (5) lane narrowing for bicycle space, (6) evaluating additional crosswalk locations, and (7) evaluating additional exclusive turn lane locations.
- Long-term Improvements: A PD&E study is recommended to determine the preferred alternative and assess potential impacts. However, the Department currently does not have funding for any future phase of the project. The City of Oviedo could work with the MetroPlan Orlando MPO to secure funding and add the project to the MPO Project Priority List (PPL). The PD&E study would also require planning consistency, which is not yet in place based on input from local residents and stakeholders.

Attachments

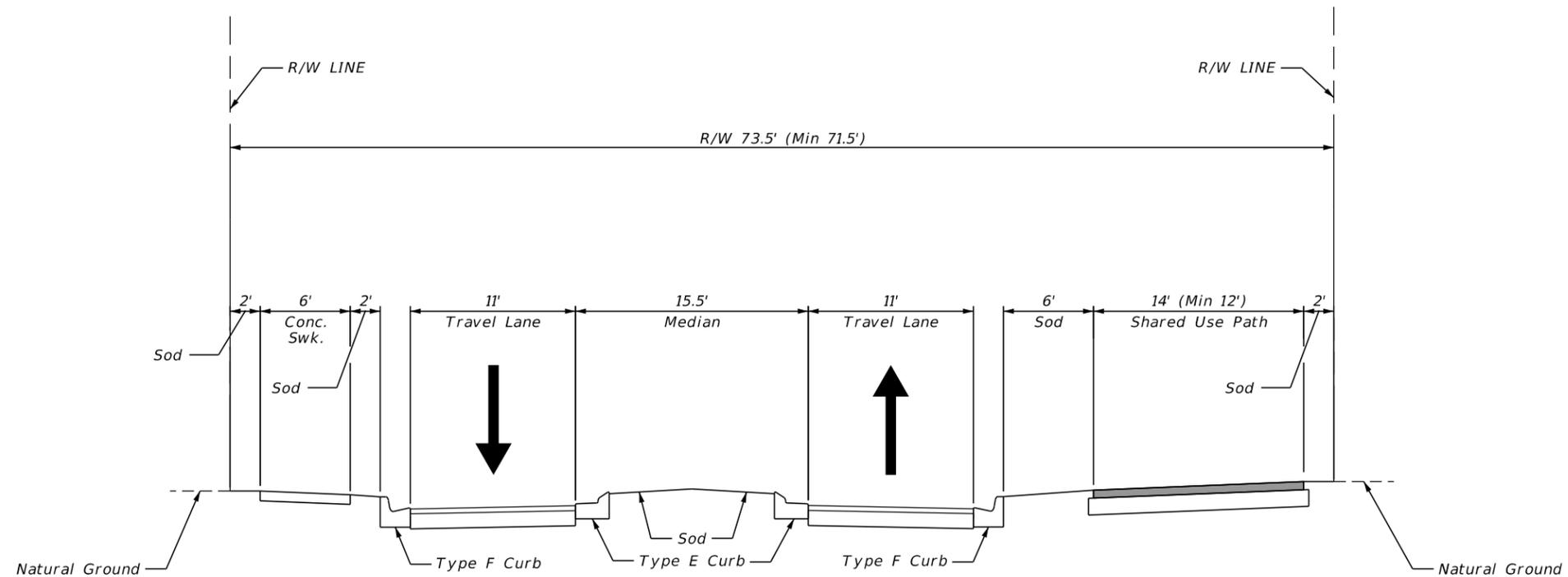




Figure A-1 – Typical Right of Way Widths



LEGEND		WIDTHS		
 Study Corridor		109-ft		55-ft
		65-ft		84.5-ft
		52.75-ft		



2-LANE DIVIDED TYPICAL
 SR 434
 STA. 75+23 TO STA. 114+66
 LEGEND



2/4/2025 3:09:29 PM bnesbitt
 T:\Worksets\FDOT\4467561201\Roadway\TYP\SRD01.dgn

SYMBOLS

PROTEAN DESIGN GROUP
 100 EAST PINE STREET, SUITE 600
 ORLANDO, FL 32801
 PH: 407-246-0044

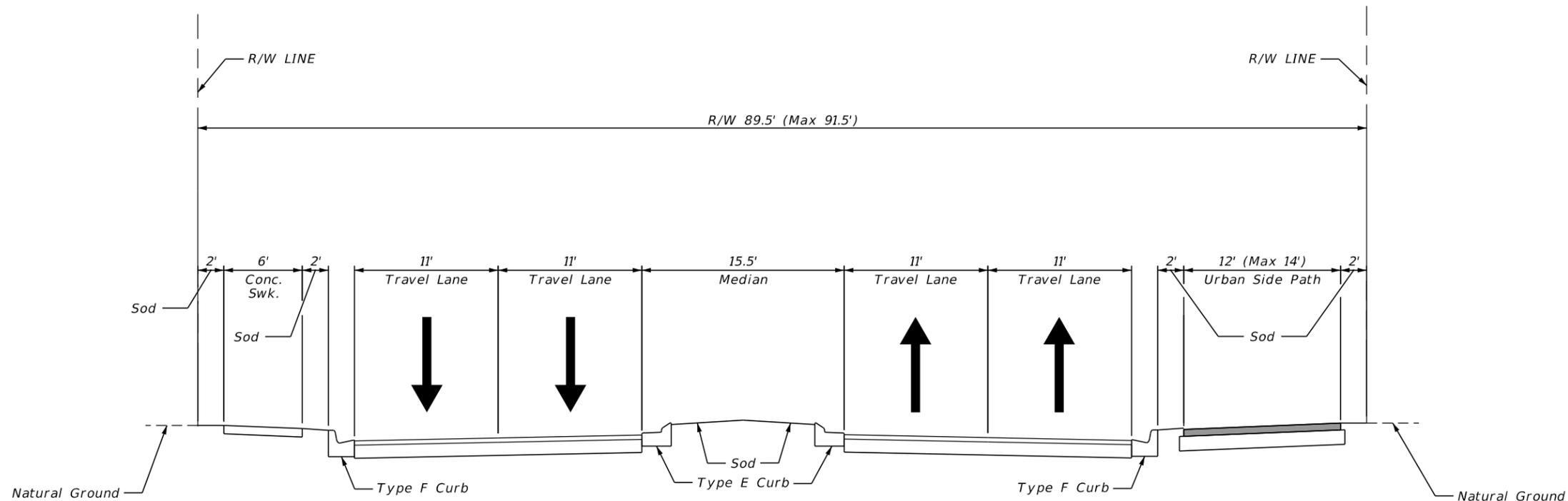


STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 DISTRICT 5

TYPICAL SECTION
 ALTERNATIVE
 - 2-LANE, DIVIDED

PAGE NO.

A-2



4-LANE DIVIDED TYPICAL
 SR 434
 STA. 74+83 TO STA. 115+60
 LEGEND



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PROTEAN DESIGN GROUP
 100 EAST PINE STREET, SUITE 600
 ORLANDO, FL 32801
 PH: 407-246-0044



STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION
 DISTRICT 5

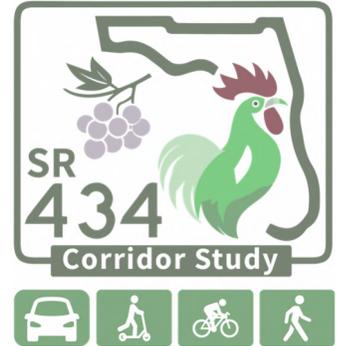
TYPICAL SECTION
 ALTERNATIVE
 - 4-LANE, DIVIDED

PAGE NO.

A-3

Community Engagement Summary

April 2025



Produced for:

Florida Department of Transportation District Five
Planning & Environmental Management Office (PEMO)



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1. Introduction

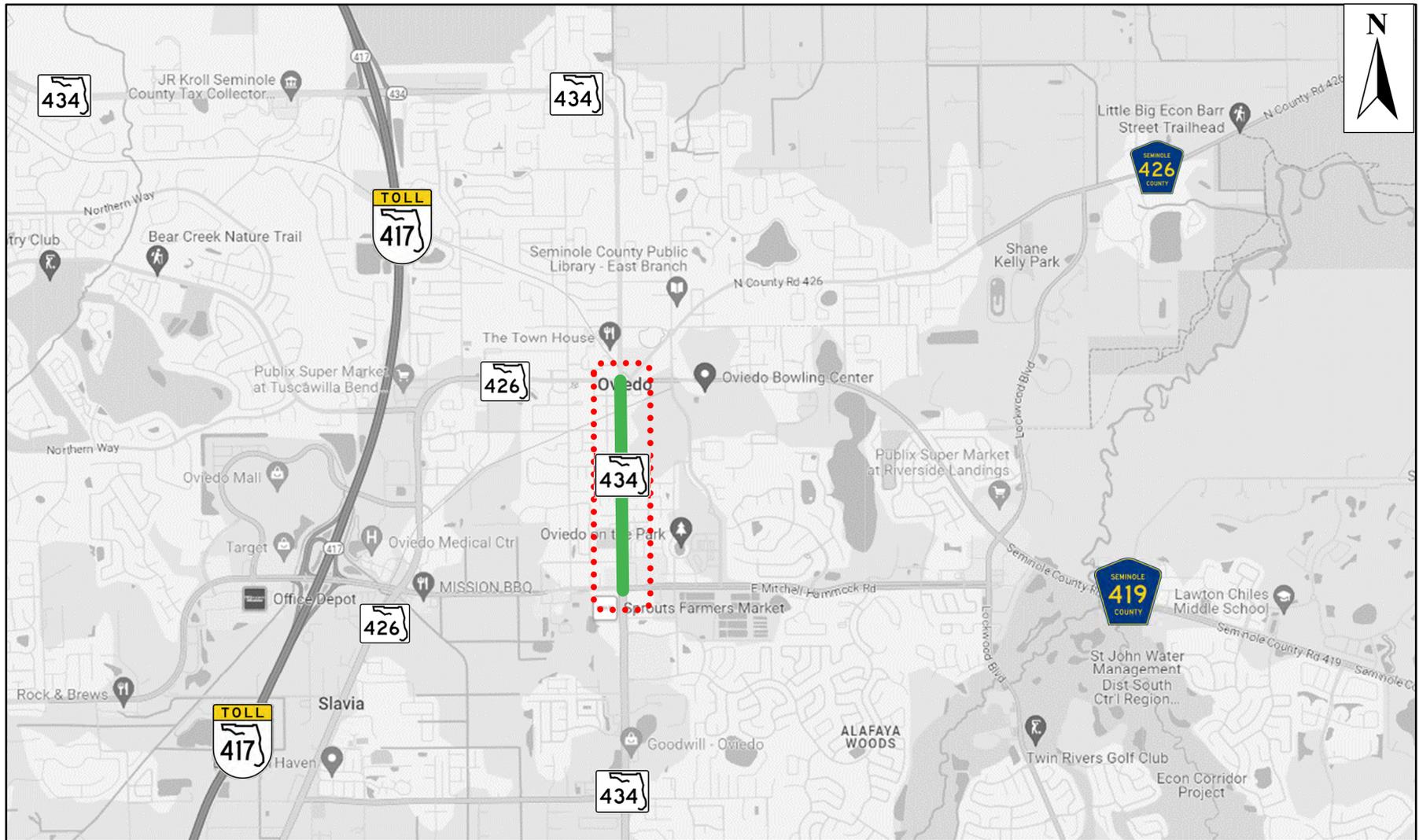
The Department, in collaboration with the City of Oviedo, Seminole County, and the MetroPlan Orlando Metropolitan Planning Organization (MPO), conducted a Corridor Study along S.R. 434 (Central Avenue), extending from south of Mitchell Hammock Road to north of S.R. 426 (Broadway Street) in Oviedo, Florida. The study area, as shown in **Figure 1**, covers approximately 1.2 miles in length.

The goal of the SR 434 Corridor Study is to evaluate options that improve safety, speed management, connectivity, and overall operation for all road users. The study will also consider surrounding land use, anticipated growth, and potential impacts to the area in order to identify the location and conceptual design for preferred roadway improvements.

This report aims to summarize the community engagement activities conducted throughout the project, highlight key stakeholders, outline recommendations, and address major concerns.



Figure 1 – Project Location



LEGEND  Study Corridor



2. Stakeholder Meetings

Based on input from key stakeholders within the study area, the Study Team made an effort to understand the City’s vision for the corridor and address any concerns or suggestions. Meetings were held to gather insights from local residents and business owners about their experiences and to discuss potential improvements. Understanding the corridor from the perspective of those who use it daily was a priority. As a result, all alternatives and recommendations were reviewed and discussed with these key stakeholders before being presented to the public. Below is a summary of each meeting held, and the topics discussed or presented.

2.1. Kick-off Meeting with the City of Oviedo (January 8, 2024)

This FDOT corridor study followed up on the corridor planning study conducted by Seminole County, as requested by the City of Oviedo. To facilitate this, the Study Team met with the City of Oviedo in person at the City of Oviedo Public Works Administration & Engineering Facility, located at 1655 Evans Street, Oviedo, FL 32765.

The meeting was attended by key city staff, representatives from the City of Oviedo CRA, and FDOT staff. A copy of the attendee list and meeting minutes can be found in **Appendix A**.

During the meeting, FDOT staff provided an overview of the project, discussed the project schedule, and clarified key staff roles. The City offered input on its goals and desires for the corridor, as well as addressing issues and concerns related to the study area. The following is a summary of the key topics discussed:

City of Oviedo Goals: The city expressed a strong desire to create a safe and efficient corridor that accommodates all modes of transportation, linking the northern redevelopment of the ‘old downtown’ area to the southern ‘new downtown’ at Oviedo on the Park. This vision includes the integration of a non-vehicular grid network, which is closely tied to the development design and offers seamless connections to the Cross Seminole Trail. The goal is to provide accessible, multi-modal transportation options that foster connectivity and support the growth of both downtown areas, while ensuring safety and convenience for pedestrians, cyclists, and motorists alike.



City of Oviedo Desires: The City envisions the SR 434 corridor operating at lower speeds with some degree of congestion, prioritizing the improvement of key pressure points without a full roadway widening. The city would like the corridor design to align with its ongoing Mobility Plan and Comprehensive Plan. Currently, the sidewalk is situated directly next to the roadway, but City commissioners prefer a buffer zone between the sidewalk and the road for safety and comfort. If a median is constructed, there are concerns about the location of safe U-turns, especially given the narrow right-of-way. Additionally, the City seeks to encourage alternative modes of transportation due to the existing vehicular conditions. There is also a strong desire to construct a shared-use path along the east side of the corridor, extending from the Cross Seminole Trail to Clonts Street, to enhance non-vehicular mobility and connectivity.

City of Oviedo Issues Identified: There are several issues associated with the intersections at Hillcrest Drive and Clark Street, primarily due to the corridor's vertical curvature, which can affect sightlines and traffic flow. Drainage is another significant concern, as the current gutter system and drainage pipes are undersized, leading to potential flooding and water accumulation. Additionally, there is an ongoing legal dispute between FDOT, the City of Oviedo, and a property owner directly south of the Goddard School related to existing drainage conditions. With the addition of impervious surfaces through potential roadway improvements, the need to enhance the drainage system becomes even more critical. This may involve constructing a new pond or other stormwater management infrastructure. Currently, all drainage flows toward the wetlands east of S.R. 434, and any changes to the corridor's layout could further impact these natural areas, necessitating careful planning and mitigation measures.

City of Oviedo Safety Concerns: The City has expressed concerns regarding the safety of non-vehicular traffic, particularly at the current Cross Seminole Trail crossing. There are worries about pedestrians and cyclists crossing S.R. 434, as the area may not provide adequate protection or clear access. Additionally, there is concern about the lack of an exclusive left-turn lane at the Clark Street intersection, which could lead to traffic congestion and unsafe conditions for drivers and pedestrians. Another concern raised is the operation of S.R. 434 at Mitchell Hammock Road, particularly during peak hours when congestion and queuing can become problematic, affecting overall traffic flow and safety at this key intersection.

City of Oviedo Typical Section Request: The City provided a marked up typical section, which revised the recommended typical section provided by the Seminole County Corridor Planning



Study. The city feels that the vehicular traffic volumes from the previous study do not warrant a widening to 4-lanes.

City of Oviedo Traffic Data: The City discussed the previously conducted Seminole County study, which recommended a 3-lane typical section featuring buffered bike lanes and a 6-foot sidewalk. The city has requested that the Level of Service (LOS) target outlined in the City's Comprehensive Plan be considered when evaluating alternatives for the SR 434 corridor. Additionally, the City and CRA representatives highlighted the plans for the 'old downtown' area, located north of S.R. 426. The CRA representative shared a development summary and proposed layout for the area, although these plans have not yet been reviewed or approved by the city.

City of Oviedo Coordination Efforts: The City provided a summary of the proposed City representatives for the PVT and identified those from Seminole County that could be recommended as well. The city is actively participating in the MetroPlan Orlando MPO VisionZero efforts, which are led by HDR. Additionally, the city requested the opportunity to review any materials before they are made public to ensure alignment with local priorities and

2.2. Project Visioning Team (PVT) Meeting #1 (March 28, 2024)

As part of the SR 434 corridor study, a Project Visioning Team (PVT) was established, bringing together key stakeholders from various sectors along the corridor. This included representatives from the City of Oviedo staff, Oviedo Police, Oviedo CRA, Seminole County staff, MetroPlan Orlando MPO, local business owners, neighborhood HOA representatives, and residents. A complete list of PVT members and PVT meeting #1 meeting minutes can be found in **Appendix B**.

The goal of the first PVT meeting was to provide an overview of the corridor study's purpose and background, explaining how the study came to be. It was emphasized that this is a corridor study, not a Coalition project, though the study could eventually lead to a Project Development and Environment (PD&E) study. The project team also clarified the role of the PVT members, emphasizing their responsibility to provide input, represent the public, and help shape recommendations. During the meeting, the team discussed the study area limits, existing roadway conditions, and a summary of crash data, setting the stage for future discussions and planning.

PVT members were invited to provide feedback on various aspects of the study. As a result, questions were raised regarding the modeling being conducted, the accessibility and potential



impacts on bicycle traffic, and pedestrian crossing needs. Additionally, Seminole County requested an extension of the study limits northward to Franklin Street to encompass a broader area for analysis.

The City of Oviedo also highlighted their plans to implement a pedestrian marked crosswalk with a Rectangular Rapid Flashing Beacon (RRFB) at the Boardwalk Road / Boston Hill Park intersection, aiming to enhance pedestrian safety.

The project team also emphasized the importance of developing a project logo to better identify the specific section of S.R. 434 under study. This logo would contribute to creating continuity and recognizing future phases of the project, ensuring the community can easily identify and connect with ongoing improvements.

2.3. Project Visioning Team (PVT) Meeting #2 (April 24, 2024)

The intent of the second PVT meeting was to present the proposed alternatives and recommendations, gather feedback from PVT members, and gain a deeper understanding of the community's perspective on the area. This meeting aimed to ensure that the proposed alternatives aligned with the needs and concerns of both stakeholders and the broader public. A copy of the PVT meeting #2 minutes, a list of attendees, and the presentation materials can be found in **Appendix C**.

The proposed alternatives were presented and discussed in detail. There are 2 proposed typical sections: a 2-lane, divided typical section and a 4-lane, divided typical section. Each alternative was discussed in detail regarding the proposed typical section and corresponding impacts to the surrounding residential/commercial buildings. In addition, a pedestrian hybrid beacon (PHB) was proposed at the Cross Seminole Trail crossing, located south of Smith Street, which was recommended for both alternatives.

A summary was provided of the project schedule. The community event will be occurring on May 15, 2024, at the Oviedo Amphitheatre and Cultural Center. An image of the flyer was shown as part of the presentation.

PVT members were invited to provide feedback on various aspects of the study, and their input led to several requests and concerns. These included a request for a digital copy of the flyer, clarification on how the community event would be conducted, and concerns regarding the potential for U-turns given the already tight right of way. Members also expressed a desire for landscaping improvements, such as planting trees between the sidewalk and roadway, and raised concerns about the maintenance of shared-use paths and sidewalks. Additionally, they



requested access to roll plots and the KMZ file, as well as a preference to avoid impacting structural buildings as part of the proposed improvements.

The project team addressed these concerns by clarifying that landscaping would be addressed during future phases of the project, but due to the narrow right of way, avoiding impacts to buildings would not be feasible. The team also noted that the community event would be conducted with a focus on providing clear explanations of the alternatives and gathering feedback from the public.

2.4. Project Visioning Team (PVT) Meeting #3 (April 9, 2025)

The third and final Project Visioning Team (PVT) meeting focused on reviewing the final recommendations of the S.R. 434 corridor study. Two primary alternatives were considered: a two-lane divided section and a four-lane divided section. However, following public engagement and stakeholder feedback, no clear consensus emerged in support of either option. Given the significant costs and substantial right-of-way impacts—including effects on historical properties and public facilities—the Study Team recommends advancing to a Project Development and Environment (PD&E) study if widening is pursued in the future. In the meantime, a range of interim improvements were identified, including targeted pedestrian safety enhancements. A copy of the PVT meeting #3 minutes, a list of attendees, and the presentation materials can be found in **Appendix D**.

PVT member input included a range of questions and comments, particularly regarding safety, project feasibility, and next steps. A key concern was the safety of the Seminole Trail crossing, where members noted that even with the RRFB in place, drivers frequently failed to stop. This observation was confirmed by the project team during field reviews.

Questions were raised about how the “easy,” “medium,” and “hard” feasibility categories were determined. The team explained that these categories reflect a mix of factors, including construction cost, right-of-way availability, and the complexity of the required development process. Members also asked whether this was the final meeting for the study. The project team confirmed that it is, and that final materials will be uploaded to the CFLRoads.com project website.



3. Community Event

A community event for the S.R. 434 Corridor Study, spanning from Mitchell Hammock Road to S.R. 426, was scheduled to engage local residents, business owners, and stakeholders in discussions about potential improvements to the corridor. The event aimed to gather valuable input on the proposed alternatives, address community concerns, and ensure that the needs and priorities of those who use the corridor daily are considered in the planning process. This event provided an opportunity for attendees to learn more about the study, ask questions, and share feedback directly with the project team. The following sections summarize the efforts and materials provided associated with the community event.

3.1. Grassroots Effort (April 26, 2024)

On April 26, 2024, the Project Manager visited the field and walked the corridor to distribute project flyers and promote the upcoming community event. As part of this grassroots effort, local businesses directly along the corridor were specifically targeted to gather input on the project study, discuss concerns and ideas, and personally invite them to the community event. A summary of the businesses visited, along with information on whether the flyer was dropped off or if the business was not present, can be found in **Figure 2**.

For the residential neighborhoods included in the PVT efforts, as well as the other PVT members, a digital copy of the flyer was distributed to these areas. Additionally, for the commercial development on the northwest corner of S.R. 434 and Mitchell Hammock Road, the Study Team coordinated with the City of Oviedo to provide a copy of the flyer for distribution to the property owner, property manager, and/or individual business owners.

A copy of the flyer can be found in **Appendix E**.



Figure 2 – Summary of Grassroots Effort



LEGEND	GRASSROOTS EFFORT
Study Corridor	Hand Delivered
	No Answer or Response
	Sent Electronically



3.2. Community Event (May 16, 2024)

The SR 434 Corridor Study incorporated a thorough community engagement plan aimed at informing and involving local residents, business owners, and other stakeholders. A community event plan summary was submitted to FDOT for approval, detailing essential aspects of the event. This summary included information on the local demographics, the layout of the event venue, the technology to be used, and the virtual platform available for remote participation. Additionally, it outlined the expected number of attendees and the event format, ensuring that the community would have ample opportunity to learn about the study and provide input on the proposed corridor improvements.

Through collaboration with the City of Oviedo and site visits, the community event was scheduled to take place at the Oviedo Amphitheatre & Cultural Center, located at 357 Center Lake Lane, Oviedo, FL 32765. This venue, situated just east of the study area within the Oviedo on the Park development, was selected to accommodate the event.

The community event summary, which includes a graphic of the venue layout, was approved by FDOT and can be found in **Appendix F**.

To prepare for the meeting, talking points were developed, covering key details of the existing corridor and outlining the materials related to the proposed alternatives. A brochure summarizing similar information was also prepared and provided to attendees. Copies of the talking points and brochure can be found in **Appendix G**.

Hybrid Component

As part of this hybrid meeting, off-site staff were available to run the virtual meeting. FDOT staff ran the GoToWebinar, with a consultant team member available to assist with technical assistance and/or documenting public input. A similar presentation was presented to the in-person attendees regarding the project summary and alternatives presented. A copy of the presentation can be found in **Appendix H**.

In-person Component

At the in-person community event, attendees signed in upon arrival and were provided with a comment form and project brochure. The venue was set up with informative boards that highlighted key elements of the project, including the project limits, study process, typical sections, a comparison of the proposed alternatives, and improvement renderings. Additionally, roll plots were prepared to showcase the proposed alignments for both the 2-lane, divided and 4-lane, divided options.



The roll plots were equipped with sticky notes, allowing attendees to identify key issues, express agreement with the recommendations, or provide comments on the proposed alternatives. These roll plots enabled attendees to zoom in on their specific properties to assess potential impacts of the alternatives. Copies of the boards and roll plots displayed at the event can be found in **Appendix I**.

The PVT members, along with local citizens living, working, or commuting within the study area, were invited to the Community Event. Based on the feedback received, the following summary provides a list of those who expressed support for or opposition to any widening (impacts to right of way) to the corridor within the study area:

PRO IMPROVEMENTS (2LD or 4LD)		AGAINST WIDENING IMPROVEMENTS (no-build)	
Name	Representing	Name	Representing
Lawrence Ewaldt	Technical Sales & Applications (TSA)	Megan Sladek	resident
Ian Tasman	Pharmacy on the Park *	Pearl/Van Sullivan	Church of God of Prophecy
Barbara Barbour	Resident (Graceland Square Townhomes)	Jake Vigliotti	Resident
Bob Huth	Resident	Sandra and Dennis Erny	Residents
		Alex and Hillary Moore	Residents (28 Hillcrest Drive)

*Note: * Pharmacy on the Park supports the recommendation to widen, if desired; however, requests a median opening into the property along S.R. 434 (south of Broadway Street), as he considers this median access essential for his business operations.*

The sign-in sheet summarizing those who attended the community event (digitally or in-person) can be found in **Appendix J**.

The corridor has limited right of way available and local stakeholders anticipate that any recommendation will have implications for the right of way. This would require additional funds for right of way purchases, as well as impact any future project schedules. There is a question of whether a 4-lane corridor is necessary or if a median, with exclusive left turn lanes, are needed for safety. Commissioner Dallari has maintained close communication with the Department regarding this corridor study and future needs.



A summary of the comments received, electronically, through mail, and included as part of the roll plot comments can be found in **Appendix K**.

It is important to note that S.R. 426, located at the northern end of the study area, is currently under construction and is expected to complete the 4-lane widening by summer 2024, after the community event. This improvement could potentially alleviate queuing and safety concerns along the northern limits of the study area.

Following the community event, the Study Team found that there was no consensus among the stakeholders regarding the proposed alternatives. As a result, no preferred alternative could be determined at this time.

All project materials were uploaded to the project website at:

www.cflroads.com/project/962200-4.



4. Summary of Key Desires, Issues, and Concerns

The following is a summary of the key desires, issues, and concerns expressed by stakeholders during the community engagement process for the SR 434 Corridor Study. These insights reflect the diverse perspectives of local residents, business owners, and other community members, highlighting their priorities for improving the corridor as well as the challenges they foresee. The feedback received has been instrumental in understanding the community’s needs and will help guide the development of potential solutions that balance safety, accessibility, and mobility for all users.

4.1. Stakeholder Desires

Based on the community event, there were consistent key desires expressed by stakeholders. These desires reflect the community’s vision for the future of the corridor, focusing on improvements that enhance safety, mobility, and overall quality of life. Stakeholders shared their preferences for specific enhancements:

Comments - Desires
Safe connection between the ‘Old Downtown’ at the S.R. 434/S.R. 426 intersection and the ‘New Downtown’ at the Oviedo on the Park.
Crosswalk Opportunities – 2 school bus stop locations (at Broadway Street and at High Street)
Improved signal timings at S.R. 434 at Mitchell Hammock Road and at S.R. 426.
Need for exclusive left-turn lanes, where applicable (i.e., at Clark Street / Goddard School)
Improved safety for pedestrians walking along and/or crossing S.R. 434 (i.e., school bus drop-off locations or at the Cross Seminole Trail crossing).
Preference for installing pedestrian hybrid beacons (PHBs) instead of rectangular rapid flashing beacons (RRFBs), where possible. There are concerns about the limited potential for vehicles to stop when RRFBs are used, and the desire for a more effective and reliable traffic control device to enhance pedestrian safety along the corridor.



4.2. Stakeholders Issues and Project Concerns

In addition to desires expressed by the community event attendees, key issues and concerns were raised based on the alternatives/recommendations presented. These concerns reflect the community's input regarding potential impacts, safety considerations, and overall effectiveness of the proposed improvements. This feedback will guide the next steps in the planning process, helping to identify solutions that mitigate concerns and improve the corridor for all users.

Comments – Issues and Concerns
<p>Signal Operations: Queuing and operational concerns at the signalized intersections of Mitchell Hammock Road and S.R. 426 have been highlighted by stakeholders. The flow of traffic at these intersections, particularly during peak hours, is a significant issue, with long queues often forming due to congestion. These operational concerns may lead to delays, increased travel times, and potential safety hazards as vehicles back up and attempt to navigate through the intersections. Addressing these concerns will be crucial in the design of improvements to ensure smoother traffic flow, reduce congestion, and improve overall safety for both drivers and pedestrians at these key locations.</p>
<p>Right of Way Impacts: Right of way impacts associated with any potential widening of the SR 434 corridor have raised concerns among local residents and businesses. Stakeholders are particularly concerned about the possible loss of property or access that may result from the acquisition of right-of-way to accommodate the proposed improvements. These impacts could disrupt local businesses, reduce available space for residents, or alter the character of neighborhoods. Careful consideration of these impacts will be necessary to minimize disruptions and ensure that any proposed changes are balanced with the needs of the community. Effective communication and mitigation strategies will be essential in addressing these concerns as the project progresses.</p> <p>Specific concerns have been raised regarding the potential impacts to the Church of God of Prophecy, located at the corridor’s pinch point, where the right-of-way (R/W) is at its narrowest, only 55 feet. In addition, there have been a few historical buildings that have been identified along the corridor.</p>
<p>Non-Vehicular Safety: Safety concerns have been raised regarding the speed of commuting vehicles, the volume of cut-through traffic, and the safety of pedestrians walking along S.R. 434 or crossing the roadway. Stakeholders have expressed particular concern about the risks posed to pedestrians, especially children walking to and from school buses or crossing at locations like the Cross Seminole Trail. The speed of vehicles traveling along the corridor and the amount of cut-through traffic may exacerbate these risks, creating dangerous conditions for pedestrians. Addressing these concerns will be essential to improving the safety of all road users and ensuring safe crossings for pedestrians in high-traffic areas.</p>



Median Openings: If a median is constructed along the SR 434 corridor, concerns have been raised regarding the location of median openings and the placement of exclusive left-turn lanes. Stakeholders emphasize the importance of carefully considering where these openings and turn lanes are positioned to maintain smooth traffic flow and provide proper access to properties that only have access to S.R. 434. The design of these elements must accommodate traffic patterns while also addressing the accessibility needs and potential U-turn movements, particularly due to the offset intersections and limited access for neighborhoods, residences, and businesses.

The Pharmacy on the Park has expressed strong concerns regarding the need for a median opening and an exclusive left-turn lane at their property. The business owner has been adamant that these features are essential to ensure safe and convenient access for customers, particularly given the volume of traffic along the corridor.

Cyclist: Some cyclists are hesitant about sharing the path with pedestrians, particularly those walking with schoolchildren or pets, as this could lead to safety concerns or congestion. Additionally, cyclists have raised issues about the slope changes at driveways and street connections, which could pose challenges for both cyclists and pedestrians, affecting the overall usability of the path.

Given these concerns, finding a solution that accommodates both types of traffic effectively while minimizing potential conflicts will be essential in the final design.

Underground Impacts: Concerns have been raised regarding the potential impact of the proposed improvements on underground septic tanks and wells located within the study area. Specifically, a well has been identified at the northeast corner of S.R. 434 and Lawn Street, while septic tanks are present on both sides of S.R. 434 between Wood Street and south of Hillcrest Drive. Any construction activities or changes to the roadway may disrupt these systems, leading to potential environmental or functional issues.

Utility Impact: There is concern regarding the overhead utility poles supporting electric and communication lines along S.R. 434. All existing poles are expected to be impacted by the proposed improvements, as they are primarily located behind the sidewalk on the east side of the roadway throughout the corridor. Additionally, there is a small section where the overhead utility lines switch to the west side of the roadway at E. High Street and then return to the east side at Sandalwood Court. These utility poles may require relocation or adjustments, which could add to the complexity and cost of the project.

No Preferred Alternative: As a result of the Community Event, no consensus was reached on a preferred alternative for the SR 434 Corridor improvements. Therefore, as part of the next steps, establishing planning consistency will be necessary before moving forward with the project.

Appendix A

Kick-off Meeting with City of Oviedo (January 8, 2024) Meeting Minutes





Florida Department of Transportation

RON DESANTIS
GOVERNOR

JARED W. PERDUE, P.E.
SECRETARY

Project: S.R. 434 – Oviedo Corridor Study

Subject: Project Champion (City of Oviedo) Kick-Off Meeting

Meeting Date: January 8, 2024

Attendees:

Erika Shellenberger, FDOT	Paul Yeargain, Oviedo
Mark Trebitz, FDOT	Bryan Cobb, Oviedo
Joseph Fontanelli, FDOT	Dave Axel, Seminole CRA
Bobby Wyatt, Oviedo	Arturo Perez, Seminole County

Discussion Items

➤ **Introduction and Roles**

- **City of Oviedo:** Bryan Cobb (City Manager), Bobby Wyatt (Public Works Director), Paul Yeargain (Assistant City Engineer)
- **Dave Axel:** He is currently on the Seminole County Community Redevelopment Agency board. Term could be ending in a week or so. Also is a real estate agent, primarily serving the City of Oviedo. He is also a developer representative and helps with rezoning, comprehensive plan amendments, etc. in the City and County.
- **Seminole County:** Arturo Axel (Public Works)
- **Mark T. (FDOT)** explained that the goal of the Project Champion kickoff meeting is to ensure that FDOT and the City are in agreement on what the study includes, the City's desires for the corridor, and gather concerns in the corridor.

➤ **Project Overview**

- **Erika S. (FDOT)** explained that the goal of the project is to come to agreement on a typical section and concept to move into PD&E. FDOT wants to ensure the study provides a useful product and aligns with the City's vision for the roadway.
- **Erika S. (FDOT)** discussed anticipated deliverables, including concepts and typical section graphics, as well as the high level plan for the Community

Engagement, including meeting types and anticipated graphics to be shown at those meetings.

- *TO-DO: Erika S. (FDOT) will provide Seminole County staff with the scope for their reference.*
- **Erika S. (FDOT)** stated that the schedule is approximately 8 months.
 - *TO-DO: Erika S. (FDOT) will provide City of Oviedo staff with the DRAFT schedule for their reference.*

➤ **City's Issues and Needs in Corridor**

- City's Goals for the Corridor:
 - Continue to be Historic District
 - Connect North Redevelopment to South Redevelopment
 - Non-Vehicular Grid and SR 434 connects them.
- City's Desires:
 - Congestion and lower speeds.
 - Would like pressure points improved without full roadway widening.
 - Align with Mobility Plan (in progress) and Comprehensive Plan.
 - Do not have sidewalk next to road. Commissioners do not like this.
 - Forcing other modes due to poor vehicular conditions.
 - Safe U-turn Location(s)
 - Shared Use Path on East Side of SR 434 (from Cross Seminole Trail to Clonts Street)
- City's Issues:
 - Hillcrest Drive and Clark Street – impacted by vertical curvature
 - Drainage
 1. Property directly south of the Goddard School is suing FDOT and City of Oviedo due to drainage conditions in the area.
 2. If adding impervious area, going to need improved drainage. Currently, everything flows to the wetlands east of SR 434.
 - Safety Concerns:
 1. SR 434 & Mitchell Hammock
 2. Clark Street (NBL)
- Previously Completed Study:
 - **Erika S. (FDOT)** shared that the previous study recommended a 3-Lane typical section with buffered bike lanes and 6 feet sidewalk.
 - **Paul Y. (Oviedo)** provided a typical section more in line with what the City would want to see. It includes:
 1. 12'-16' Shared Use Path on East, 6' Sidewalk on West
 2. 6' Landscape Buffer on East, 3' Landscape Buffer on West
 3. Curb & Gutter on Both Sides

- 4. 2 11' Through Lanes
- 5. 11' Median
- 6. Turn Lanes (where appropriate)
 - City felt that volumes from the previous study do not warrant widening to 4-lanes.
- Current Projects – No Comments from City

➤ **Traffic Data**

- City Requests that LOS in City's Comprehensive Plan be considered.
- Redevelopment of "Old Downtown" (NE Quadrant of SR 426 & SR 434)
 - Not Approved by City
 - Proposing ~ 2,000 MF, 200 Hotel Rooms, 350+ KSF Retail, Total of 16,000+ Daily Net, New Trips
- Connected Wood Street to Windy Pine to provide additional E-W Travel
- Discussion of SR 417 Widening – Possible Impact to SR 434 Traffic
- Appropriately placed turn lanes will make a difference in the operation of this roadway.
- Townhome development along SR 434
 - Donated ROW to FDOT for future widening.

➤ **Community Engagement**

- Attendees of Kickoff Meeting will be a part of the PVT.
- The City would like to review the PVT list.
- City only anticipates loud opposition if the typical section gets out of hand. Dave feels confident that residents along the corridor would be supportive of improved bicycle/pedestrian facilities.
- Seminole County would like their Traffic Engineer Charles Wetzel to be on the PVT.

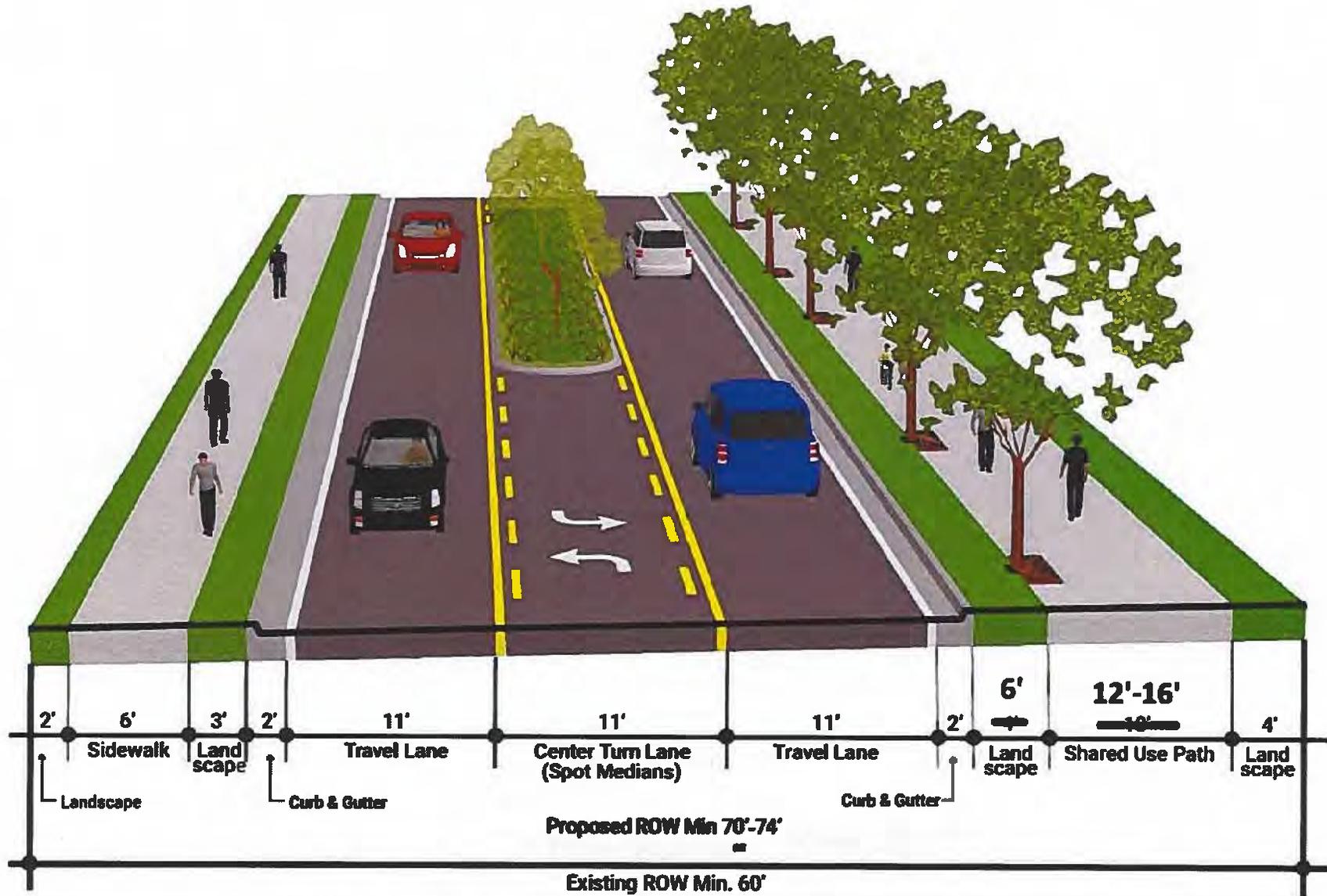
➤ **Future Coordination**

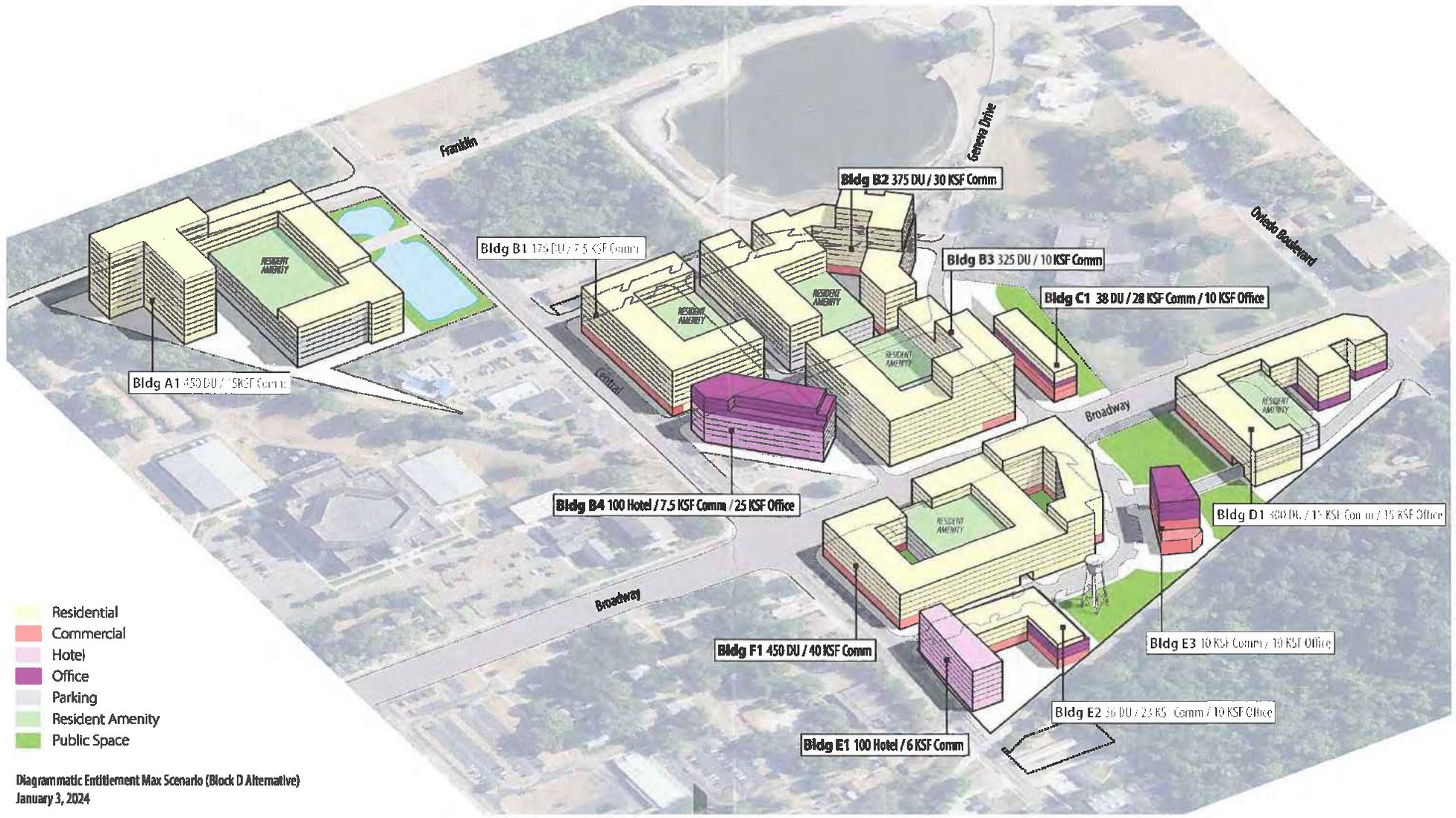
- City of Oviedo is participating in the MetroPlan VisionZero efforts. Consultant: HDR
 - Future Coordination may be needed on proposed safety improvements.
- City agreed to review meeting (PVT and Community Event) materials prior to meetings.
- City requested meetings at major milestones throughout the project. Likely around deliverables.

NOTE: THE ABOVE REFLECTS THE WRITER'S UNDERSTANDING OF THE CONTENTS OF THE MEETING. IF ANY MISINTERPRETATIONS OR INACCURACIES ARE INCLUDED, PLEASE CONTACT TODD DAVIS (407-246-0044 X122) AS SOON AS POSSIBLE FOR RESOLUTION AND REVISIONS, IF NECESSARY.

Suggestion 16-NOV-2022

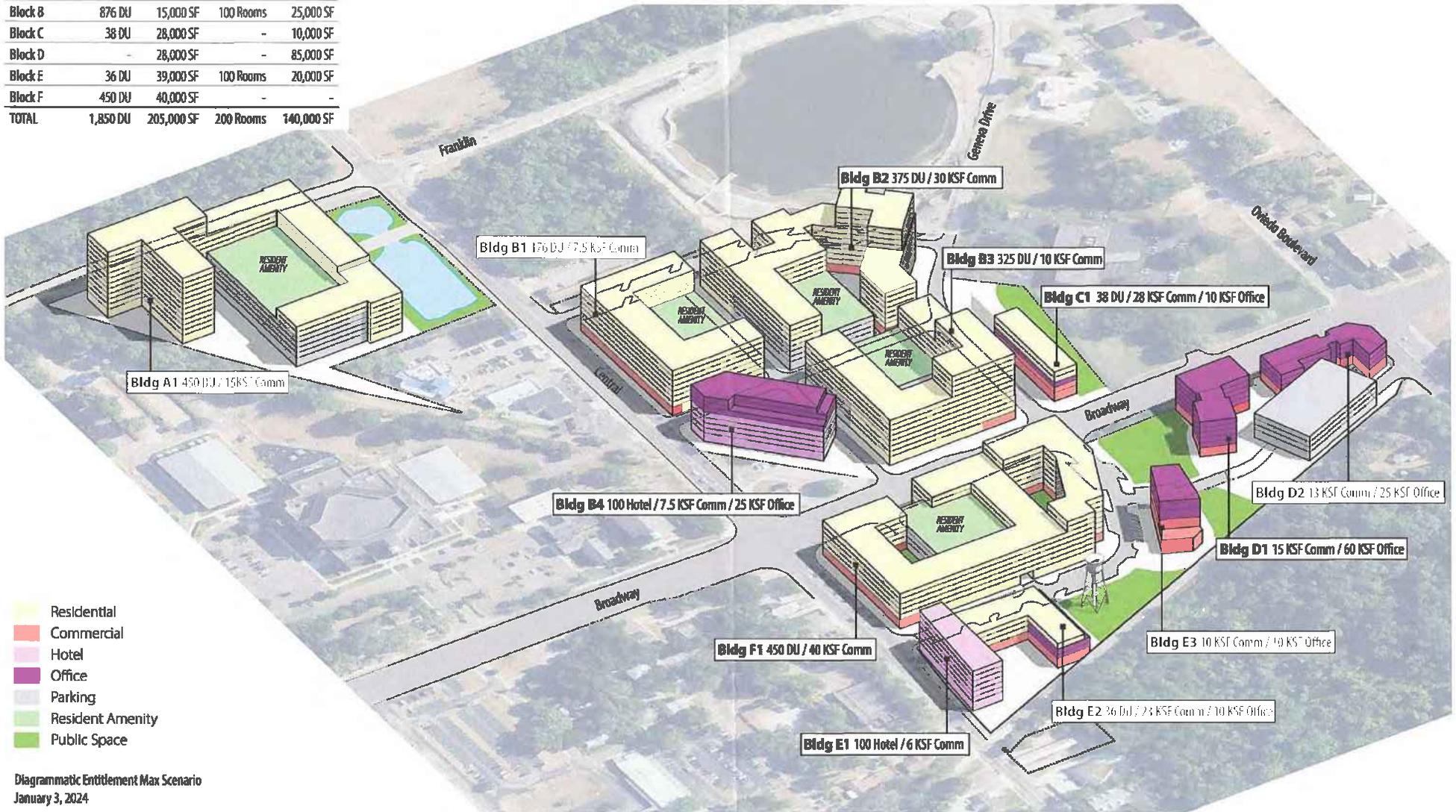
Section 2 - Alternative 2 3-Lanes with Shared Use Path





Diagrammatic Entitlement Max Scenario (Block D Alternative)
January 3, 2024

	Residential	Commercial	Hotel	Office
Block A	450 DU	15,000 SF	-	-
Block B	876 DU	15,000 SF	100 Rooms	25,000 SF
Block C	38 DU	28,000 SF	-	10,000 SF
Block D	-	28,000 SF	-	85,000 SF
Block E	36 DU	39,000 SF	100 Rooms	20,000 SF
Block F	450 DU	40,000 SF	-	-
TOTAL	1,850 DU	205,000 SF	200 Rooms	140,000 SF



Diagrammatic Entitlement Max Scenario
January 3, 2024

**Water Town District
Trip Generation Summary**

ITE Code	Land Use	Size	Daily		AM Peak Hour			PM Peak Hour				
			Rate	Trips	Rate	Total	Enter	Exit	Rate	Total	Enter	Exit
221	Multi-Family Apartments	1,969 DU	4.75	9,346	0.43	855	197	658	0.39	768	468	300
310	Hotel (1)	200 Rooms	7.99	1,598	0.46	93	52	41	0.60	120	61	59
822	Specialty Retail (<40k)	50 KSF	46.79	2,340	2.36	118	71	47	6.59	330	165	165
822	Food & Beverage Retail	10 KSF	65.17	652	2.36	24	14	9	6.59	66	33	33
822	Food Hall/Market	40 KSF	47.94	1,918	2.36	94	57	38	6.59	264	132	132
565	Private Education	10 KSF	47.62	476	11.00	110	58	52	11.12	111	52	59
495	Recreational Community Center	15 KSF	28.82	432	1.91	29	19	10	2.50	38	18	20
560	Community Serving	20 KSF	7.60	152	0.32	6	4	2	0.49	10	4	5
110	Light Industrial	10 KSF	4.87	49	0.74	7	6	1	0.65	7	1	6
932	Restaurant	45 KSF	107.20	4,824	9.57	431	237	194	9.05	407	248	159
971	Brewery Tap Room	20 KSF	61.69	1,234	0.68	14	12	2	9.83	197	116	81
880	Pharmacy without Drive-Thru	5 KSF	90.08	450	2.94	15	10	5	8.51	43	21	22
720	Medical-Dental Office Building	20 KSF	36.00	720	2.83	57	45	12	3.91	78	23	55
710	General Office Building	120 KSF	11.33	1,360	1.63	196	172	23	1.61	193	33	160
926	Food Truck Pod	8 VEH	--	493	--	--	--	--	6.16	49	25	24
Total Trips				26,043		2,048	954	1,094		2,680	1,401	1,279
Internal Capture (25%)				6,511		512	238	274		670	350	320
Total External Trips				19,532		1,536	716	820		2,010	1,051	959
Pass-By Trips for Retail Uses (40%)				1,473		71	36	36		198	99	99
Pass-By Trips for Restaurants (43%)				1,556		139	70	70		131	66	66
Pass-By Trips for Pharmacy (53%)				179		6	3	3		17	9	9
Total Net External Trips				16,324		1,320	608	712		1,664	878	786

Based on ITE Trip Generation Manual, 11th Edition

(1) Hotel size was estimated at 135,000 sq. ft. (based on 675 sq. ft./room)

(2) Total Non-Residential buildings are 500,000 sq. ft.

Appendix B

Project Visioning Team
(PVT) Meeting #1
(March 28, 2024) Meeting
Minutes





Project Visioning Team (PVT) Meeting #1

S.R. 434 – Oviedo Corridor Study
March 28, 2024 | 1:00 p.m. – 2:00 p.m.
(Virtual TEAMS Meeting)

Welcome & Introductions

Erika Shellenberger, FDOT In-House Consultant Project Manager, summarized the agenda and invited participants to introduce themselves. The list of attendees for Project Visioning Team Meeting #1 are attached as Appendix A. The PowerPoint presentation (attached as Appendix B) was utilized during the PVT meeting.

Project Overview

Mark Trebitz, FDOT Project Development Manager, provided an overview of what the purpose of the corridor study was and a background of how the study came about. This study will focus on safety, speed management, etc. He described this project will include a consensus-based approach that focuses heavily on public engagement with input from the community and local champions. It is important to note that this is a corridor study and not a Coalition project, as the project could lead into a Project Development and Environment (PD&E) study.

Ms. Shellenberger defined "What is the Project Visioning Team (PVT)?" and noted the important role of PVT members to provide input, represent the public, and help to shape recommendations.

Purpose and Focus of the Study

Todd Davis, Consultant Project Manager, provided an overview of the study area and described the purpose of the study. He noted that a previous study was conducted by the County's engineer that included a broader study area, which included the northern segment, up to S.R. 417. This study is focusing on the 2-lane section south of the S.R. 426 widening project, where the right of way is limited. FDOT is seeking to understand the community perspective for this area.

Existing Conditions

Existing conditions in the study area were reviewed to discuss the preliminary findings of the project team.

Key points included:

- The project limits for this S.R. 434 corridor is from Mitchell Hammock Road to S.R. 426, a distance of approximately 1.02 miles.
 - The 2-lane section is from Clonts Street to Smith Street, with a trail crossing located south of Smith Street.
- There are limited exclusive left turn lanes along the mainline of S.R. 434.
- There is limited right of way, as there is enough right of way to serve the current corridor typical sections, nothing more. The narrowest sections vary between 52.75 and 55 feet.
- There is an existing bike lane (5 to 6 feet) throughout the study limits, with an existing sidewalk.
- The primary type of crash within the study limits are rear-end crashes (78%).

A summary was provided of the project schedule. The community event will be occurring in mid-May. The study will continue after the community event and determine the next steps. Note, the next steps can vary based on the different options (i.e., right of way, PD&E study, identification of funding, etc.). Therefore, the specific next steps will be determined based on the recommendation.

Project Visioning Team Input

The virtual meeting was changed from the PowerPoint slides to Google maps to discuss the specific locations and study area limits.

PVT Member input included the following comments:

- Was the heat map shown based on Signal4 data?
 - Yes.
- Who is the model consultant on the team?
 - InNovo Partners.
- Will the study team be recommending a new posted speed limit? If widening to 4-lanes, would a higher speed limit (i.e., 45 MPH) be recommended?
 - No speed limit adjustments anticipated; however, will revisit upon determining the final recommendation.
- Based on the adopted Comprehensive Plan of the City of Oviedo, there is an increase in density within the Community Redevelopment Area (CRA). Therefore, based on the proposed mixed uses north of S.R. 426, it was recommended to ensure the model includes the appropriate land use data.
- If the desire is multi-laning, consideration should be given to avoid a wider/faster roadway. There is a desire to preserve this area of residential without destroying the home along the corridor.

- The desire is to keep the micromobility aspects (i.e., bike lanes) separate from travel lanes with a separator.
- Why did the study limits end at S.R. 426 to the north? There are also concerns with the 2-lane sections north of SR. 426. Recommend extending to Artesian Way.
 - The original request was to study this section of S.R. 434, as it has limited right of way and the study was analyzing the impacts of the S.R. 426 widening project. In addition, the County is currently looking into construction multiple roundabouts along the northern section. Based on the results of this study, there could be the potential to analyze the northern section next.
- There are bikes everywhere. As such, there are concerns related to the discontinuation of the trail/bike network. For instance, the bike lanes ending abruptly, or the bike lane transitions to the sidewalk. There are high safety concerns with the transitions as one of the biggest safety concern is the bikes traversing over the multiple driveways.
- If a shared-use path (12 to 14 foot sidewalk) could be recommended. As such, it would be a good option IF the path is maintained, such as avoiding benches within the path, well landscaped, avoid tree roots or overgrowth of trees, etc.
- As a trainer, the bikers would avoid shared-use paths, as the desire would be to avoid dog walkers, kids, and others.
- Consideration of an elevated overpass for the trail?
 - FDOT is looking at other options first.
- For the City of Oviedo Recreation unit, connectivity is key. One example is the ability to cross from the Oviedo on the Park area to the Boston Hill Park. In addition, the connection to Friendship Park, which is highly used by residents of the City.
- Consideration of future developments and the safety of pedestrians and cyclists. Potential requests for additional crossings.
- The City has reviewed and is proposing improvements to the Boston Hill Park. The project team will meet with the City staff to discuss the proposed improvement plan and potential impact to the corridor within the study limits.
- Residents and commuters have begun using Wood Street at S.R. 434 as a cut-through roadway into the Oviedo on the Park area, rather than use Boardwalk. The intersection of S.R. 434 needs an exclusive left-turn lane.
- Have there been any consideration or thoughts of a roundabout at S.R. 434 and Boardwalk Avenue?
 - A roundabout would result in significant right of way impacts. In addition, there are concerns related to the resultant impact to a public park.

- There was desire to extend the project limits to the north and clarification of the current study limits.
 - The current study limits are from Mitchell Hammock Road (to the south) to S.R. 426 (to the north). Basically, signal to signal.
 - The northern section from S.R. 426 to S.R. 417 can be discussed at a later stage after gaining consensus on a recommendation.

Next Steps

Ms. Shellenberger concluded the meeting by reminding the PVT members of the schedule. PVT Meeting #2 is expected to take place in late April and will focus on obtaining input to inform development of alternatives. The Community Event is expected to be in May 2024. The project web page (www.cflroads.com/project/962200-4) and contact information were also provided. A copy of these notes will be sent via email to all the team members, with a copy of the presentation. Included in the presentation is the contact information for Ms. Shellenberger and Mr. Davis.

Project Visioning Team (PVT)

Name	Firm/Jurisdiction	Role	Email Address
Paul Yeargain	City of Oviedo	Assistant City Engineer/Engineering Manager	pyeargain@cityofoviedo.net
Bobby Wyatt	City of Oviedo	Public Works Director	bw Wyatt@cityofoviedo.net
Michael Woodward (or designee)	City of Oviedo Fire	Fire Chief	mwoodward@cityofoviedo.net
Dale Coleman	City of Oviedo Police	Chief of Police	dcoleman@cityofoviedo.net
Paul Belden	City of Oviedo Parks & Rec	Parks and Rec Director	pbelden@cityofoviedo.net
Dave Axel	City of Oviedo CRA Board	Board Member	daveAxel@AxelRealEstate.com
Charlie Wetzel	Seminole County	County Engineer	cwetzels@seminolecountyfl.gov
Arturo Perez	Seminole County	Project Manager	aperez@seminolecountyfl.gov ; anelson@seminolecountyfl.gov
Richard Leblanc or Jordan Smith	Seminole County Public Schools	Director of Project Management & Facilities	leblanz@scps.k12.fl.us ; smithjs@scps.k12.fl.us
Alex Trauger or Taylor Laurent	MetroPlan Orlando	Director of Transportation Planning	Alex.Trauger@MetroPlanOrlando.gov ; Taylor.Laurent@MetroPlanOrlando.gov
Myles O'Keefe	LYNX	Manager of Strategic Planning	MOKeefe@golynx.com
Mary Beth Brown	Sandalwood Village Association	HOA President	marybeth.brown@gmail.com
Yvette Baldonado	Oviedo Park Terrace HOA (Res)		yvette@cmsorlando.com ; anne.r.stark@gmail.com ; brian.donahue@finsentech.com ; walstib1961@gmail.com ; gbjordan06@gmail.com ; karendieff@icloud.com
Ryan McIntosh	Preserve of Oviedo on the Park HOA (Res)	HOA Rep / Treasurer	ryanwmc@gmail.com ; yvette@cmsorlando.com ; emartin737@gmail.com ; rob.klinger@gmail.com ; susankmurray@hotmail.com
	Oviedo on the Park HOA (Comm)		
Chris Browne	Central Park HOA	Towers Property Management	chris@towerspropertymgmt.com
Kevin Davis	Community Managemnet Specialists	Owner	kevin@cmsorlando.com
Scott Sheldon	Goddard School of Oviedo	Owner	askgoddardschool@goddardsystems.com
Roxane Ferguson	City of Oviedo Chamber of Commerce	President & CEO	info@owsrcc.org

* Note, LYNX bus route 434 operates N-S along SR 434; however, continues west along Mitchell Hammock Road and does not have a route within the study limits.



Appendix A

Project Visioning Team Meeting #1 Attendees



Project Visioning Team Meeting #1 Attendee List

FDOT Project Team

- Erika Shellenberger – FDOT (in-house consultant)
- Mark Trebitz – FDOT
- Joseph Fontanelli – FDOT
- Todd Davis – Protean Design Group

Project Visioning Team Members

- Bobby Wyatt – City of Oviedo
- Paul Yeargain – City of Oviedo
- Paul Belden – City of Oviedo
- Teresa Correra – City of Oviedo
- Dale Coleman – City of Oviedo Police
- Dave Axel – City of Oviedo CRA
- Michael Blinn – Seminole County
- Dino Lucarelli - Seminole County
- Arturo Perez - Seminole County
- Medhat Hassan - Seminole County
- Taylor Laurent – MetroPlan Orlando MPO
- B.J. Donahoe – Oviedo Terrace HOA
- Derek Catlin – Oviedo Park Terrace HOA
- Ryan McIntosh – Preserve of Oviedo on the Park HOA



Appendix B

PowerPoint Presentation



S.R. 434 – Oviedo Corridor Study

PVT Meeting #1
March 28, 2024



Meeting Agenda

01

Introductions

02

Project Overview

03

Project Goals

04

Existing Conditions

05

Project Schedule





01

Introductions





What is a Corridor Study?



Local Partnerships
Community Champions
Consensus Building

Project Visioning,
Team Collaboration,
Community-Centric Design

**Changes to the FDOT
Process:**
Context-Based Decision
Making and Solutions

Improvements for all
modes of transportation





The Role of the PVT



1

**Serve
as a Project
Champions
for the
Community**



2

**Help to
Shape
Recommendations**



3

**Participate
and
Provide Input
through PVT
Meetings**

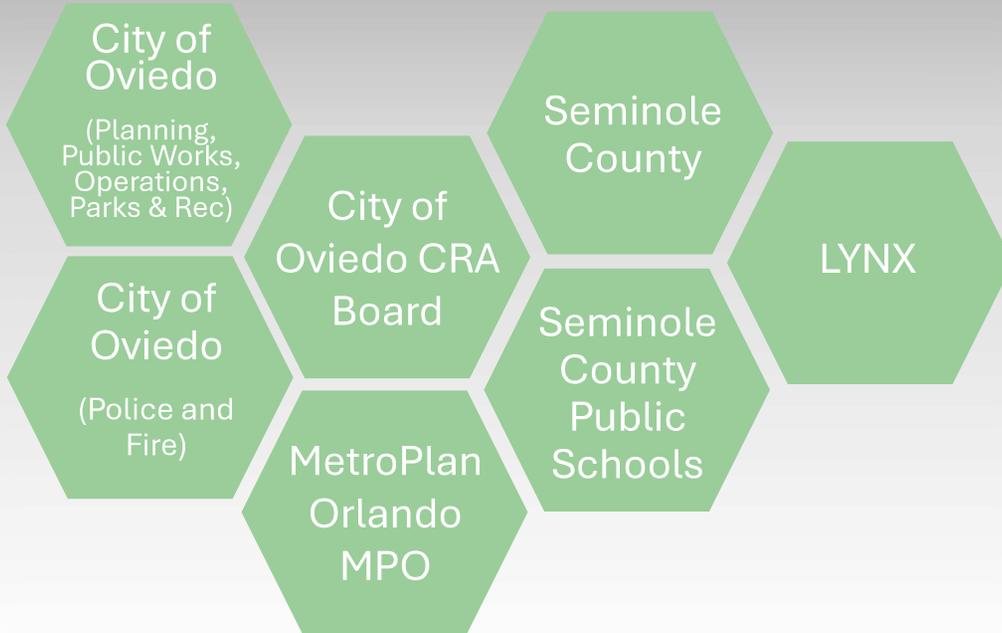




Who are the Project Visioning Team Members?



Government Partners



Community Partners





02

Project Overview



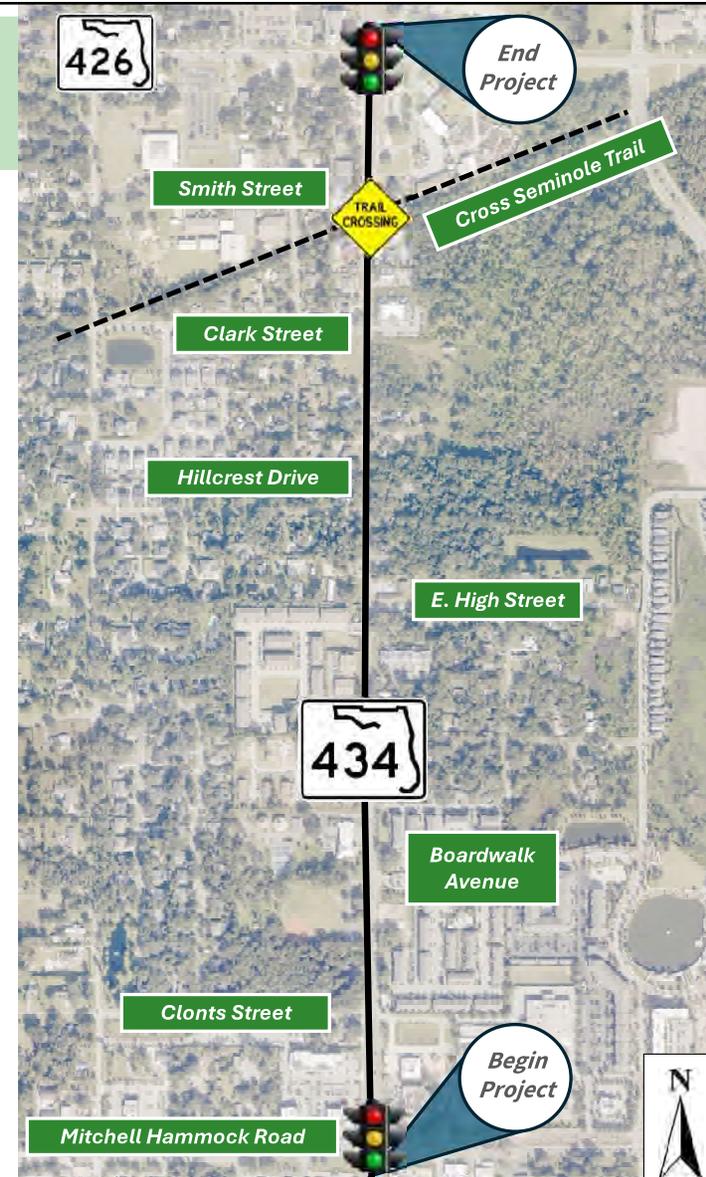


Corridor Details

- Limits: from Mitchell Hammock Road to S.R. 426 (Broadway Street)
- Length: 1.02 miles
- 2 lanes, from Clonts Street to Smith Street
- Posted Speed Limit:
 - 35 mph, south of Trail Crossing
 - 30 mph, north of Trail Crossing
- 2 Signalized Intersections



PVT Meeting #1 – March 28, 2024



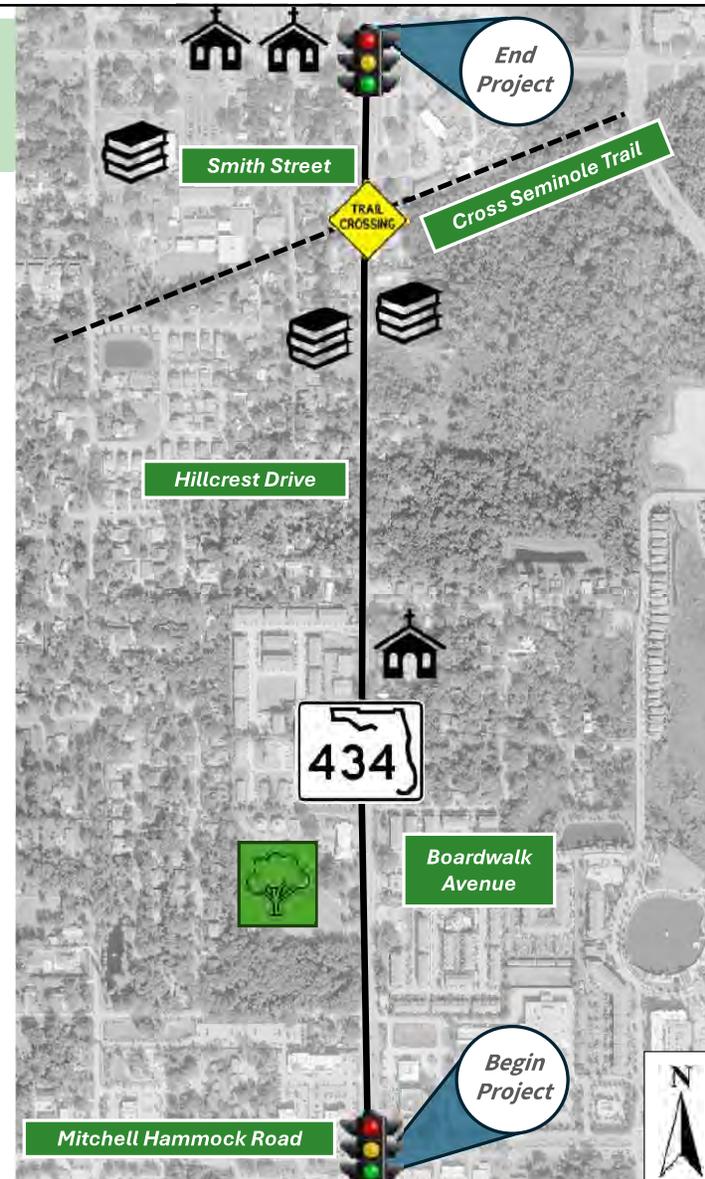


Corridor Details

- Cross Seminole Trail (crossing south of Smith Street)
 - Additional trail crossing, with trailhead, north of study limits
- Boston Hill Park 
 - Across from Boardwalk Avenue
- 3 Schools 
 - Goddard School, Oviedo Montessori Bilingual Academy at Clark Street, and Lawton Elementary School
- 3 Places of Worship 



PVT Meeting #1 – March 28, 2024





03

Project Goals





Project Goals



- Determine best way to meet **current** and **future** needs of roadway users
ES0
- Establish a **long-term plan** considering safety and speed management
- **Balance** land use and transportation planning effectively
- **Understand** surrounding land uses and future growth.
- **Data-driven**





04

Existing Conditions





Existing Conditions



Preliminary Findings:

- Safety:
 - Limited Exclusive Turn Lanes
- Limited Right of Way
- Drainage
- Lighting
- On-street bike lane
- 6 foot Sidewalk on both sides





Existing Conditions - Safety

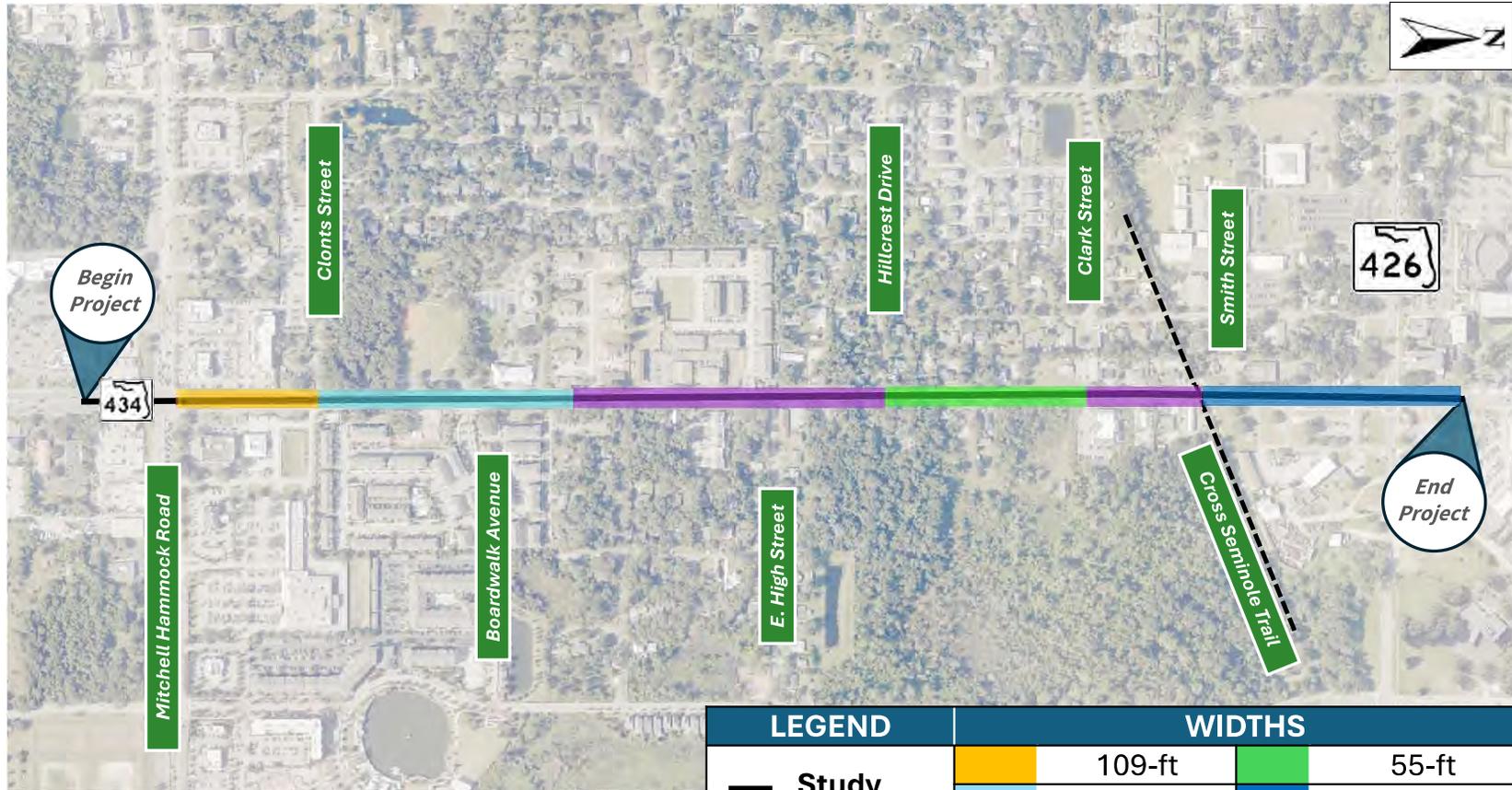


- Crashes from 2019 to 2023:
 - 107 crashes
 - 22 injury crashes
 - (29 injuries / 0 fatalities)
 - 2 ped crashes
 - 0 bike crashes
- 10% Night time
- 22% Wet Pavement
- 78% rear end





Existing Conditions – Right of Way



LEGEND		WIDTHS		
Study Corridor		109-ft		55-ft
		65-ft		84.5-ft
		52.75-ft		

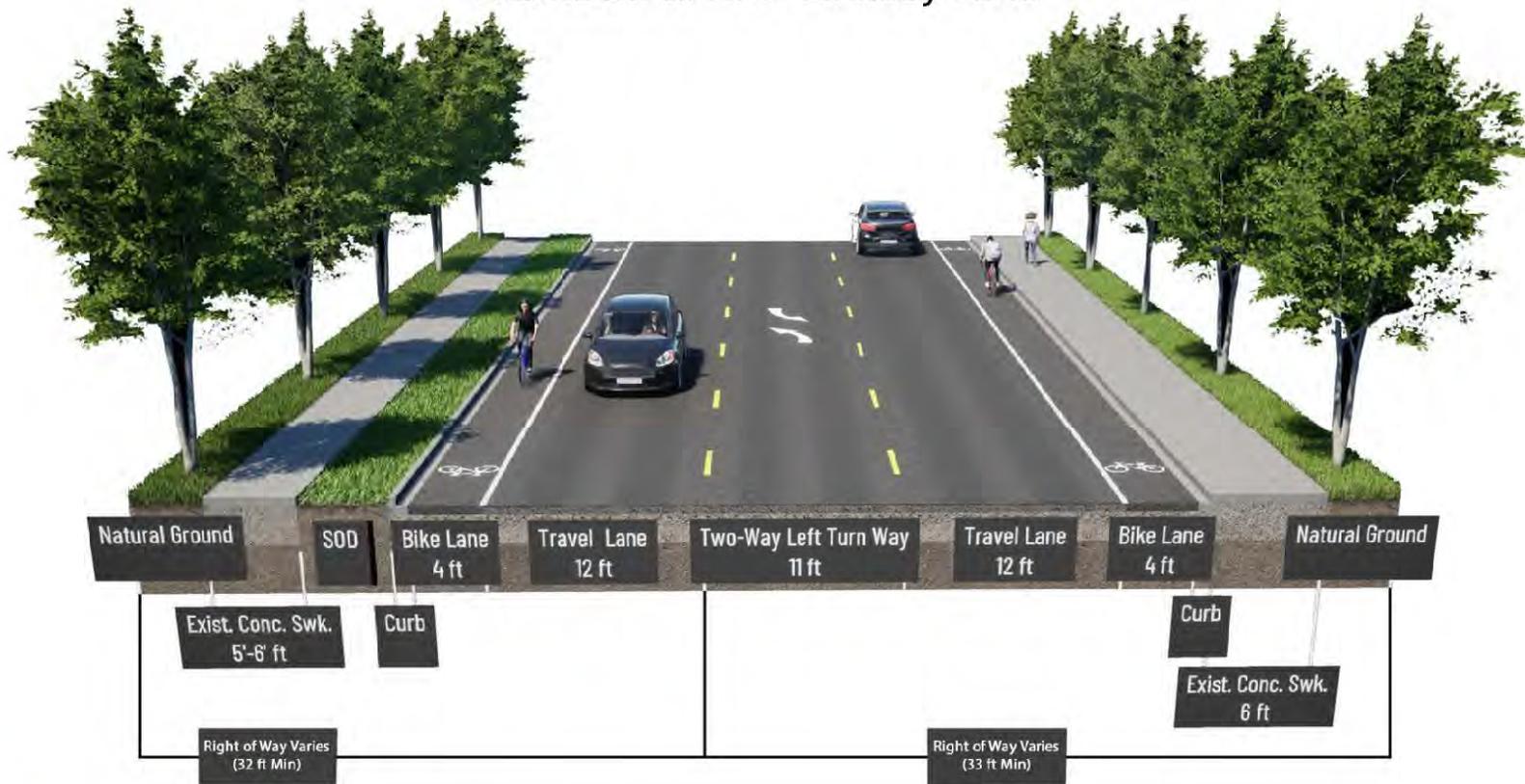




Existing Typical Sections



Clonts St. to N. of Lindsay Lane



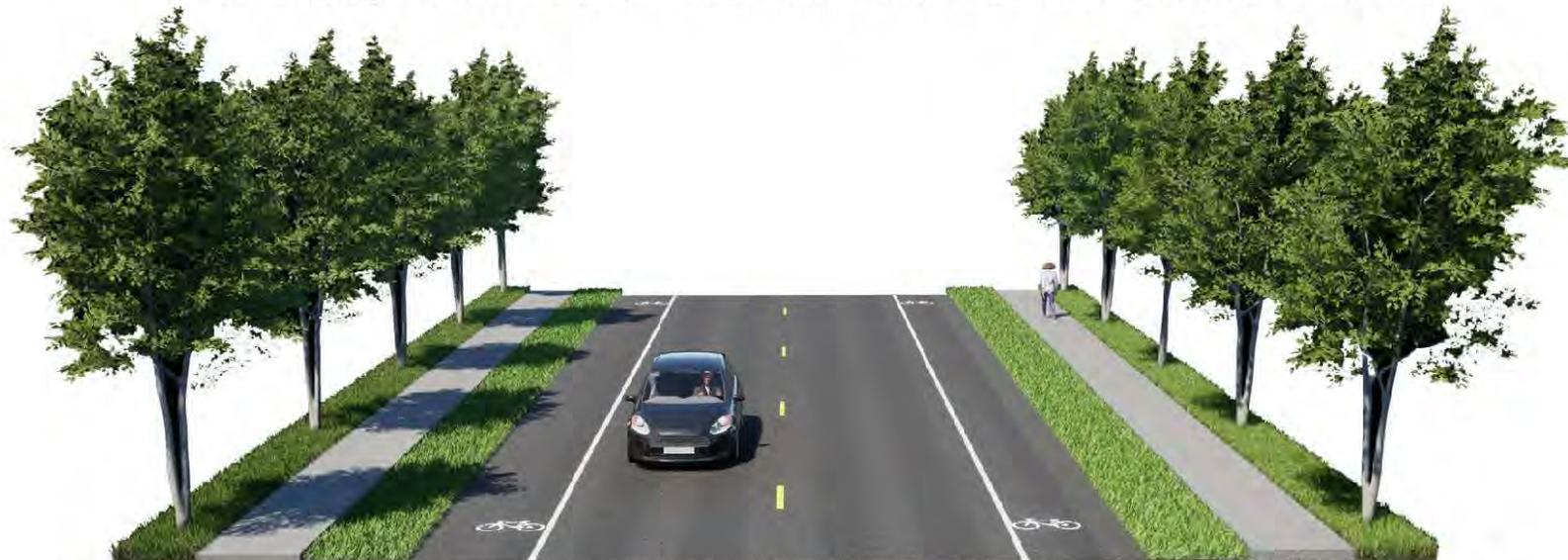
PVT Meeting #1 – March 28, 2024



Existing Typical Sections



N. of Lindsay Lane to Hillcrest Drive and Clark Street to Cross Seminole Trail



PVT Meeting #1 – March 28, 2024



05

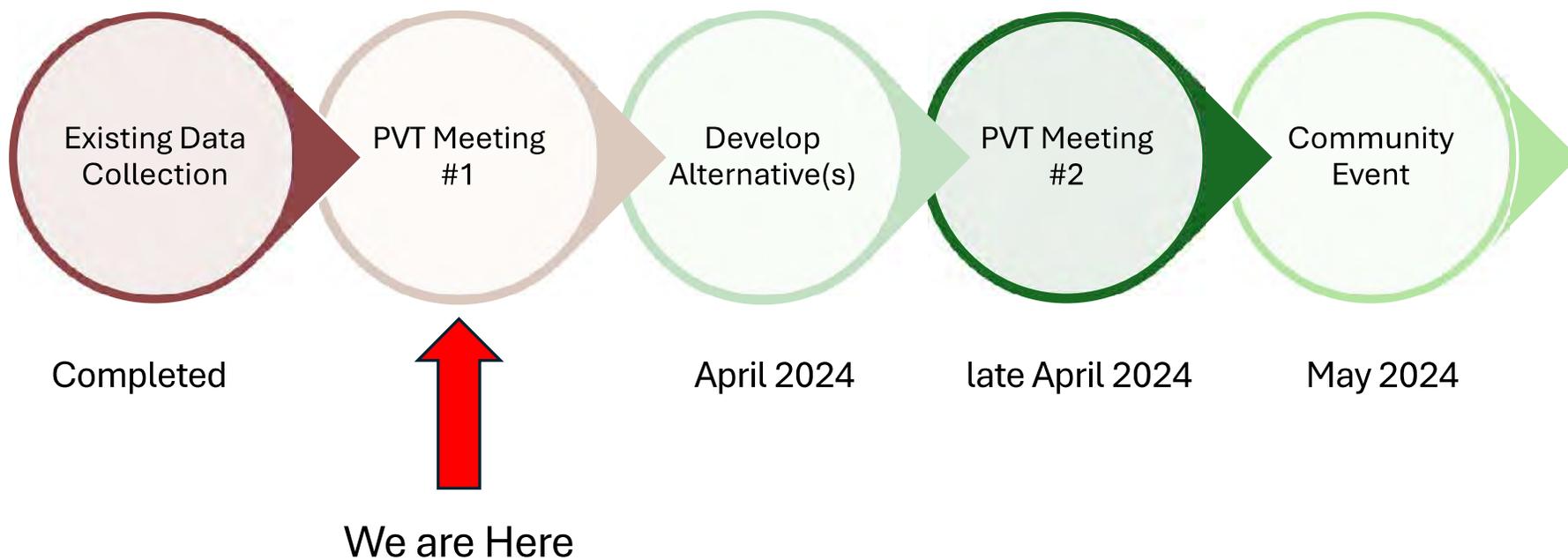
Project Schedule





Next Steps

Timeline



PVT Meeting #1 – March 28, 2024

ES1

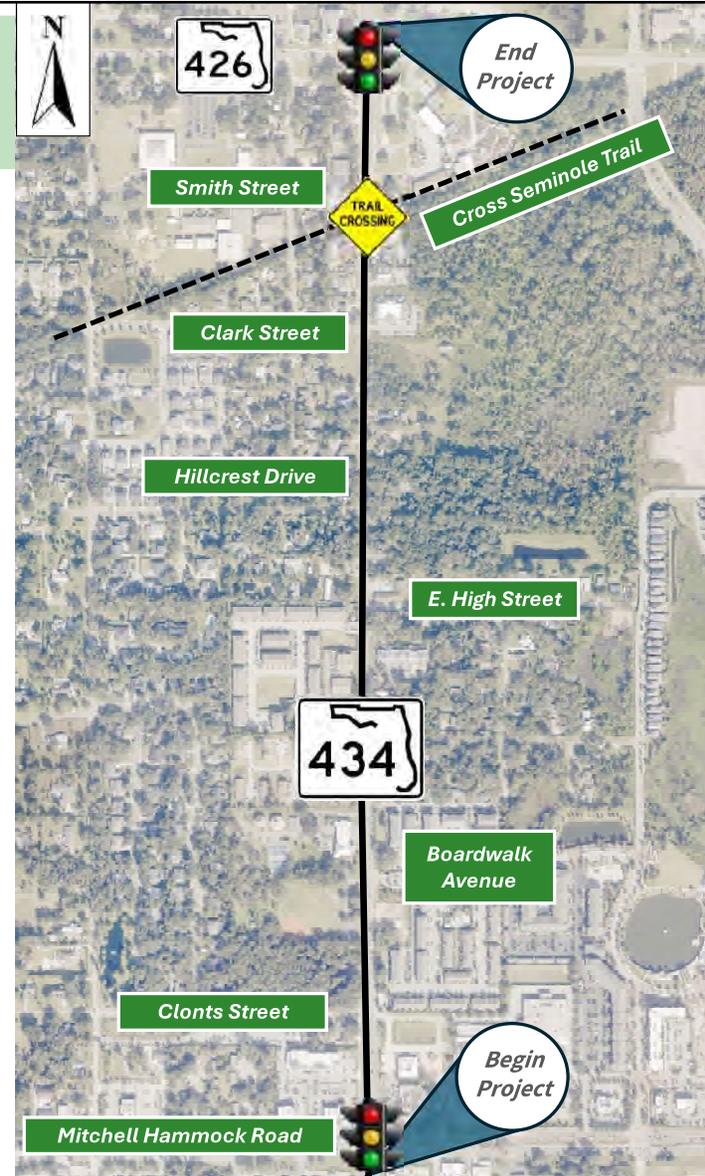


PVT Input

- What **issues / needs** should the project ^{ES2}eam be **aware** of?
- What is your **experience** commuting along the corridor?
- How can the project team further **engage the community**? ^{ES3}



PVT Meeting #1 – March 28, 2024



Contact Us!

Erika Shellenberger

ESO

Planning Project Manager
Planning & Environmental Management (PLEMO)
FDOT District Five
Erika.Shellenberger@dot.state.fl.us
(386) 943 - 5378

Todd Davis, P.E., PTOE, RSP₁

Consultant Project Manager
Protean Design Group
TDavis@proteandg.com
(407) 246 - 0044 (x122)

PROJECT WEBSITE: <https://www.cflroads.com/project/962200-4>





Options

LOGO



CORRIDOR STUDY NAME

- S.R. 434 – Oviedo
- S.R. 434 (Central Avenue)
- S.R. 434

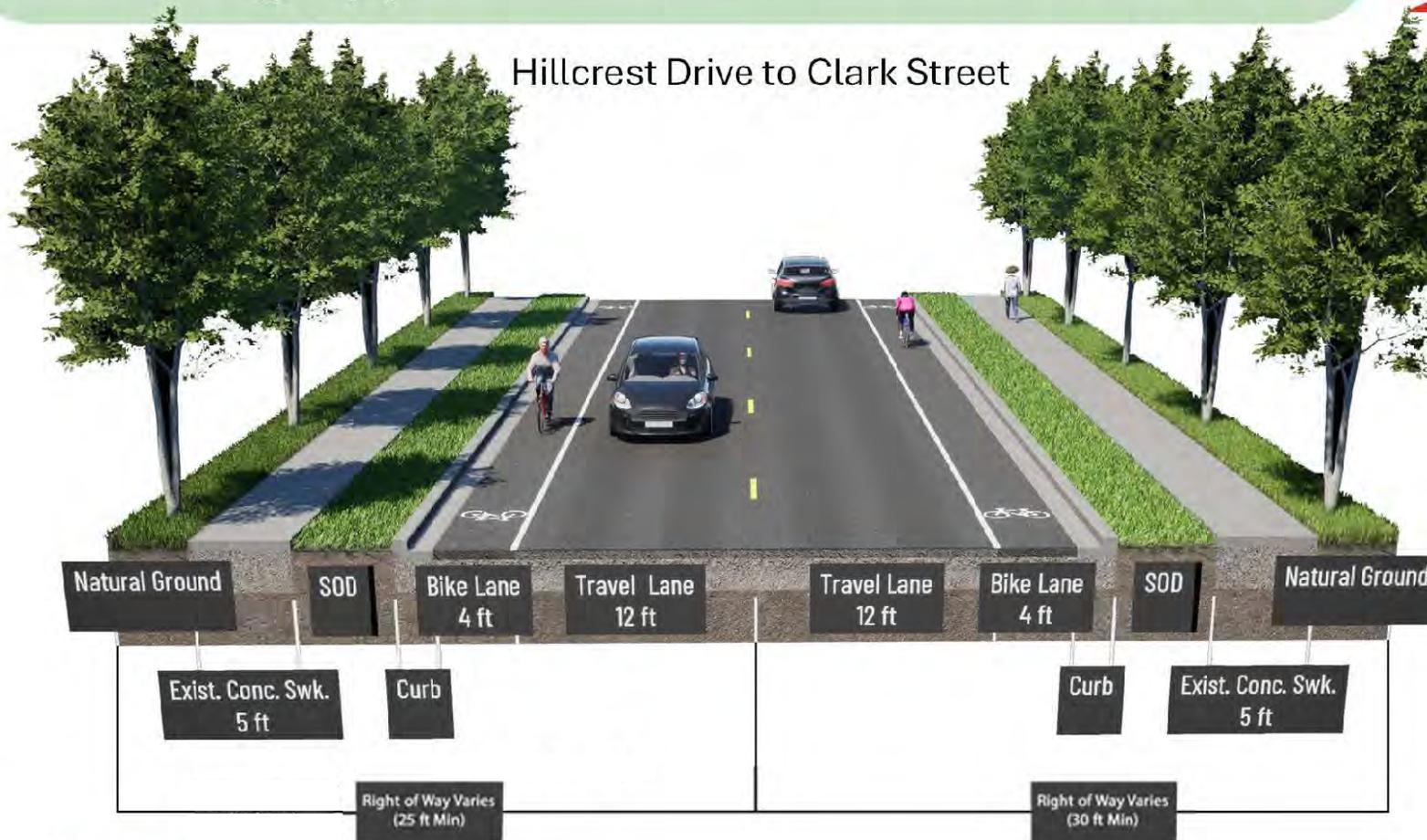




Existing Typical Sections



Hillcrest Drive to Clark Street



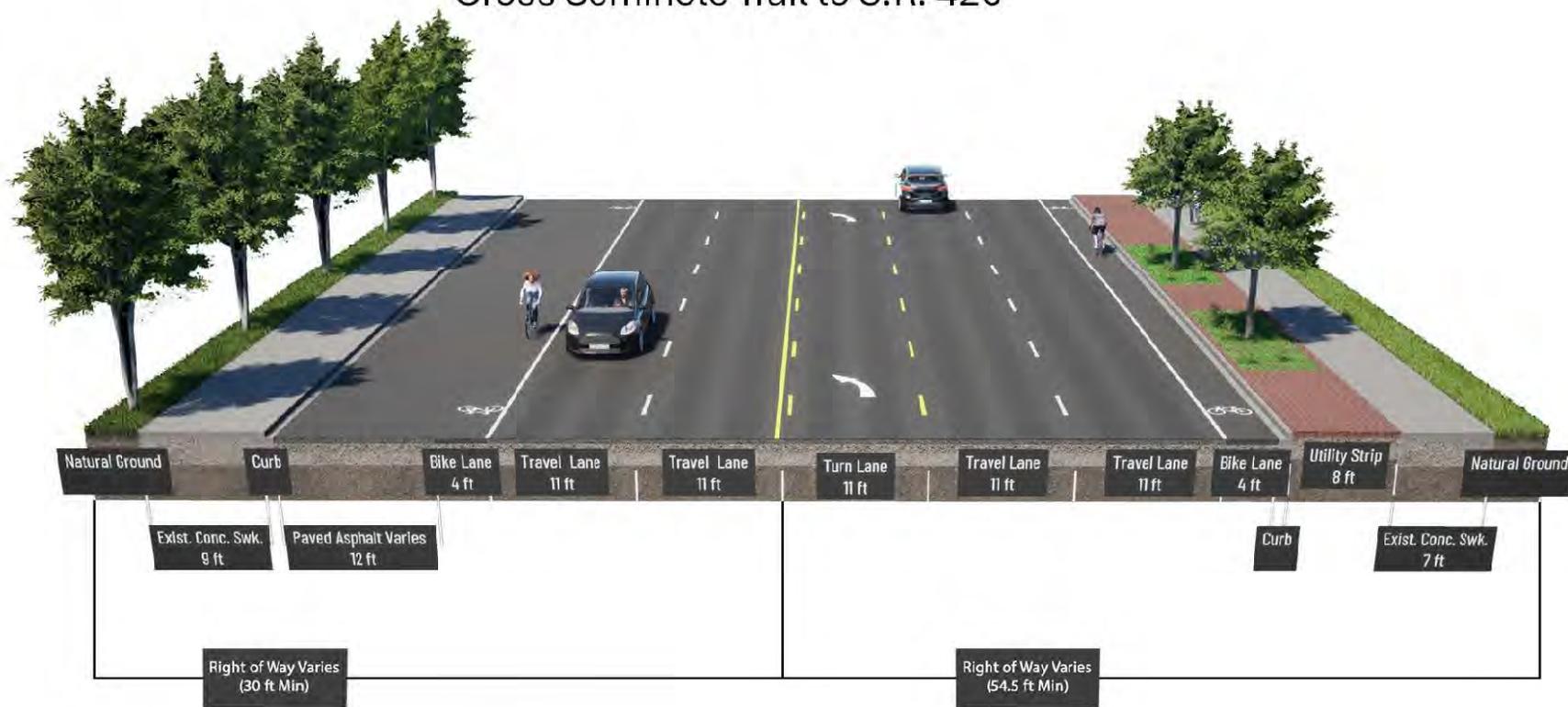
PVT Meeting #1 – March 28, 2024



Existing Typical Sections



Cross Seminole Trail to S.R. 426



PVT Meeting #1 – March 28, 2024



Demographics



3.67

PERSONS PER FAMILY

32

MEDIAN AGE

22%

BACHELOR'S OR HIGHER

3.7%

HOUSEHOLDS WITH NO VEHICLE

\$103,962

MEDIAN HOUSEHOLD INCOME

19%

POPULATION BELOW POVERTY LEVEL

6.7%

HAVE DISABILITY (20-64)





Projects along Corridor



- Chelonian Wharton Development (*under construction*)
 - MOT – Temporary Pedestrian Crossings
 - NB/SB Exclusive Turn Lanes on S.R. 434
- S.R. 426 widening (*under construction*)
 - Widening to 4-lanes
 - Completion by Summer 2024.
- Clonts Street Townhomes
 - Approved with no access to S.R. 434
- City of Oviedo Community Redevelopment Agency (*planned*)

ESO





Project Visioning Team (PVT) Overview



PVT Responsibilities:

- Three (3) meetings at key project milestones
- Identifying additional stakeholder engagement
- Provide input on project needs
- Recommend alternatives to be advanced
- Share local knowledge and history





Next Steps



- Identify and recommend alternatives
- Develop concept plans with renderings
- Meet with FDOT D5 Management to discuss alternatives
- Conduct PVT Meeting #2 to Present alternatives
- Conduct Community Event in May 2024, to present Alternatives
- Finalize Preferred Alternative and Cost Estimate



Appendix C

Project Visioning Team
(PVT) Meeting #2
(April 24, 2024) Meeting
Minutes



Project Visioning Team (PVT) Meeting #2

S.R. 434 (Central Avenue) Corridor Study

April 24, 2024 | 1:00 p.m. – 2:00 p.m.

(Virtual TEAMS Meeting)

Welcome & Introductions

Erika Shellenberger, FDOT In-House Consultant Project Manager, summarized the agenda and invited participants to introduce themselves. The list of attendees for Project Visioning Team Meeting #2 are attached as Appendix A. The PowerPoint presentation (attached as Appendix B) was utilized during the PVT meeting.

Project Overview

Ms. Shellenberger provided an overview of what the purpose of the corridor study was and defined “What is the Project Visioning Team (PVT)?” She noted the important role of PVT members to provide input, represent the public, and help to shape recommendations. Ms. Shellenberger also provide a summary of the members of the community invited to be part of the PVT.

Project Overview and Summary of Previous Meeting

Todd Davis, Consultant Project Manager, provided an overview of the study area, project goals, and summarized the comments previously discussed at the first PVT meeting. FDOT is seeking to understand the community perspective for this area.

Proposed Improvements

The proposed alternatives were presented and discussed in detail. There are 2 proposed typical sections: a 2-lane, divided typical section and a 4-lane, divided typical section.

Key points included:

- The 2-lane, divided option includes:
 - 15.5-foot median, which would include exclusive left turn lanes at identified median openings.
 - 14-foot shared use path, along the east side
 - A minimum right-of-way width of 75.5-feet. This would result in an additional of a minimum 21-feet throughout the corridor.
- The 4-lane, divided option includes:

- 15.5-foot median, which would include exclusive left turn lanes at identified median openings.
- A 12-foot shared use path, along the east side
- A minimum right-of-way width of 91.5-feet. This would result in an additional of a minimum 30-feet throughout the corridor.
- Renderings were shown to represent the proposed alternative concepts.
- A pedestrian hybrid beacon (PHB) is proposed at the Cross Seminole Trail crossing, located south of Smith Street. A rendering was presented with the 2-lane, divided concept; however, a PHB at this crossing is recommended for both alternatives.

A summary was provided of the project schedule. The community event will be occurring on May 15, 2024, at the Oviedo Amphitheatre and Cultural Center. An image of the flyer was shown as part of the presentation.

The study will continue after the community event and summarized at the PVT meeting #3 to determine the next steps. Note, the next steps will be based on the input received at the community event.

Project Visioning Team Input

PVT Member input included the following comments:

- Is the flyer available in a digital format? If so, please forward
 - Yes, we have a digital copy.
 - Flyer will be sent digitally with the meeting minutes.
- What type of event will the community event be?
 - It will be an open house format, with a presentation at 6 PM. There will be no formal meeting, as this is fact finding.
- Similar to experiences along Oviedo Boulevard, will u-turns be accommodated? With the addition of a raised median, there doesn't appear to be enough room to accommodate u-turns for the 2-lane, divided alternative.
 - Yes, u-turns will be accommodated for. You are correct that the u-turns will need additional room, via u-turn aprons or wayfinding using the existing grid network. However, these specific locations will be further evaluated as part of the next steps, after receiving input from the public on requested median opening locations.
- Due to the existing differences in the landscaping throughout the corridor, have there been any conversations related to potential landscaping for these improvements?
 - Modifying the typical such that trees are placed in the buffer in-between the roadway and sidewalk (we currently show the trees being placed behind the sidewalk).

- Other major landscape improvements (like benches and trash cans) will not occur under this contract.
 - The city will work with the CRA and FDOT regarding the design of future landscaping.
- With the anticipated increase in foot traffic, both pedestrians and cyclists, there are concerns with the maintenance of the shared-use path, as there will also be an increase in littering and other concerns.
 - The CRA has the potential to discuss potential special tax districts. However, the City and the CRA can further discuss this as an on-going effort as the project progresses.
- There are benefits to the interrupted 3-lane concept, as it provides additional left turn lanes with a smaller footprint compared to the 4-lane widening.
- The roll plots will be presented at the public meeting and the attendees will have the opportunity to show staff where they would like the median openings to be placed.
 - The team presented the roll plots associated with the proposed alternatives.
 - Seminole County requested the KMZ files associated with the proposed impacts.
 - Roll plots, when finalized, will be uploaded to CFLroads project website.
- As part of the conceptual designs, was there an effort to try and avoid structures as part of the proposed alignments?
 - Yes, the concepts were developed to avoid impacting any building along the corridor. However, in some areas, a structure was impacted on both sides of the corridor, based on the proposed typical.
- Seminole County requested the inclusion of S.R. 434, from Franklin to Artesia. This area is also excluded from the County design project.
 - Mr. Joseph Fontanelli will pass along request to FDOT management.

Next Steps

Ms. Shellenberger concluded the meeting by reminding the PVT members of the schedule. PVT Meeting #3 is expected to take place in the summer of 2024 and will focus on the feedback provided by the public from the community event, obtain input on the preferred alternatives, and discuss the next steps.

The project web page (www.cflroads.com/project/962200-4) and contact information were also provided. A copy of these notes will be sent via email to all the team members, with a copy of the presentation. Included in the presentation is the contact information for Ms. Shellenberger and Mr. Davis.

Appendix A

Project Visioning Team Meeting #2 Attendees

Project Visioning Team Meeting #2 Attendee List

FDOT Project Team

- Erika Shellenberger – FDOT (in-house consultant)
- Joseph Fontanelli – FDOT
- Amy Beckmann - FDOT
- Todd Davis – Protean Design Group
- Brittany Nesbitt – Protean Design Group

Project Visioning Team Members

- Paul Yeargain – City of Oviedo
- Dave Axel – City of Oviedo CRA
- Jean Jreij – Seminole County
- Charlie Wetzel – Seminole County
- Dino Lucarelli - Seminole County
- Arturo Perez - Seminole County
- Matt Hassan - Seminole County
- Taylor Laurent – MetroPlan Orlando MPO
- Myles O'Keefe - LYNX
- Brian Donahoe – Oviedo Terrace HOA
- Derek Catlin – Oviedo Park Terrace HOA

Appendix B

PowerPoint Presentation



S.R. 434 (Central Avenue) Corridor Study

PVT Meeting #2
April 24, 2024



Meeting Agenda

01 Welcome and Introductions TDO

02 Project Overview

03 What We Heard

04 Proposed Improvements

05 What Happens Next?





01

Introductions





What is a Corridor Study?



Local Partnerships
Community Champions
Consensus Building

Project Visioning,
Team Collaboration,
Community-Centric Design

**Changes to the FDOT
Process:**
Context-Based Decision
Making and Solutions

Improvements for all
modes of transportation





The Role of the PVT



1

**Serve
as a Project
Champions
for the
Community**



2

**Help to
Shape
Recommendations**



3

**Participate
and
Provide Input
through PVT
Meetings**

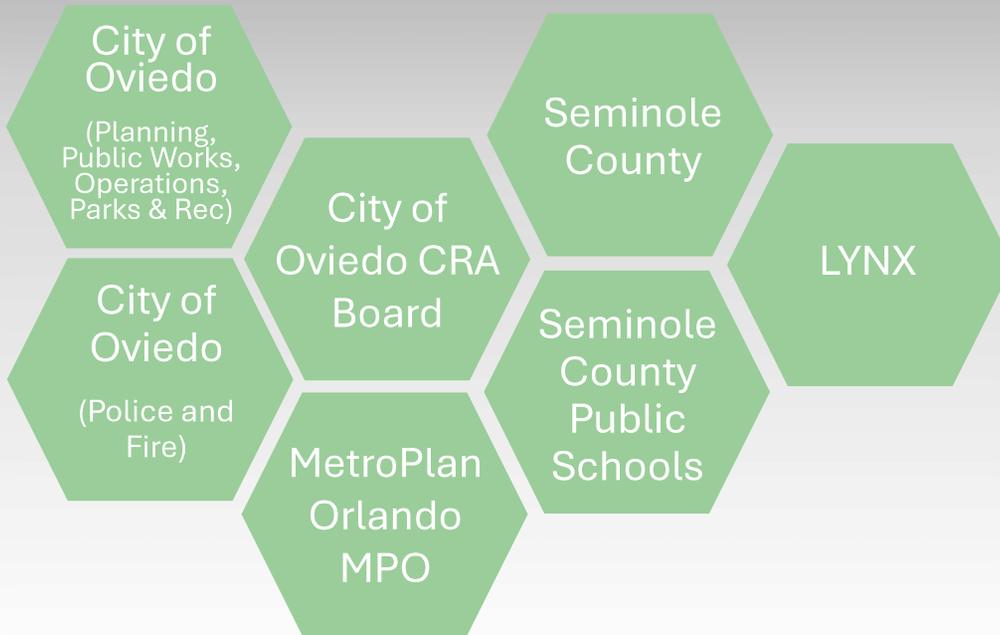




Who are the Project Visioning Team Members?



Government Partners



Community Partners





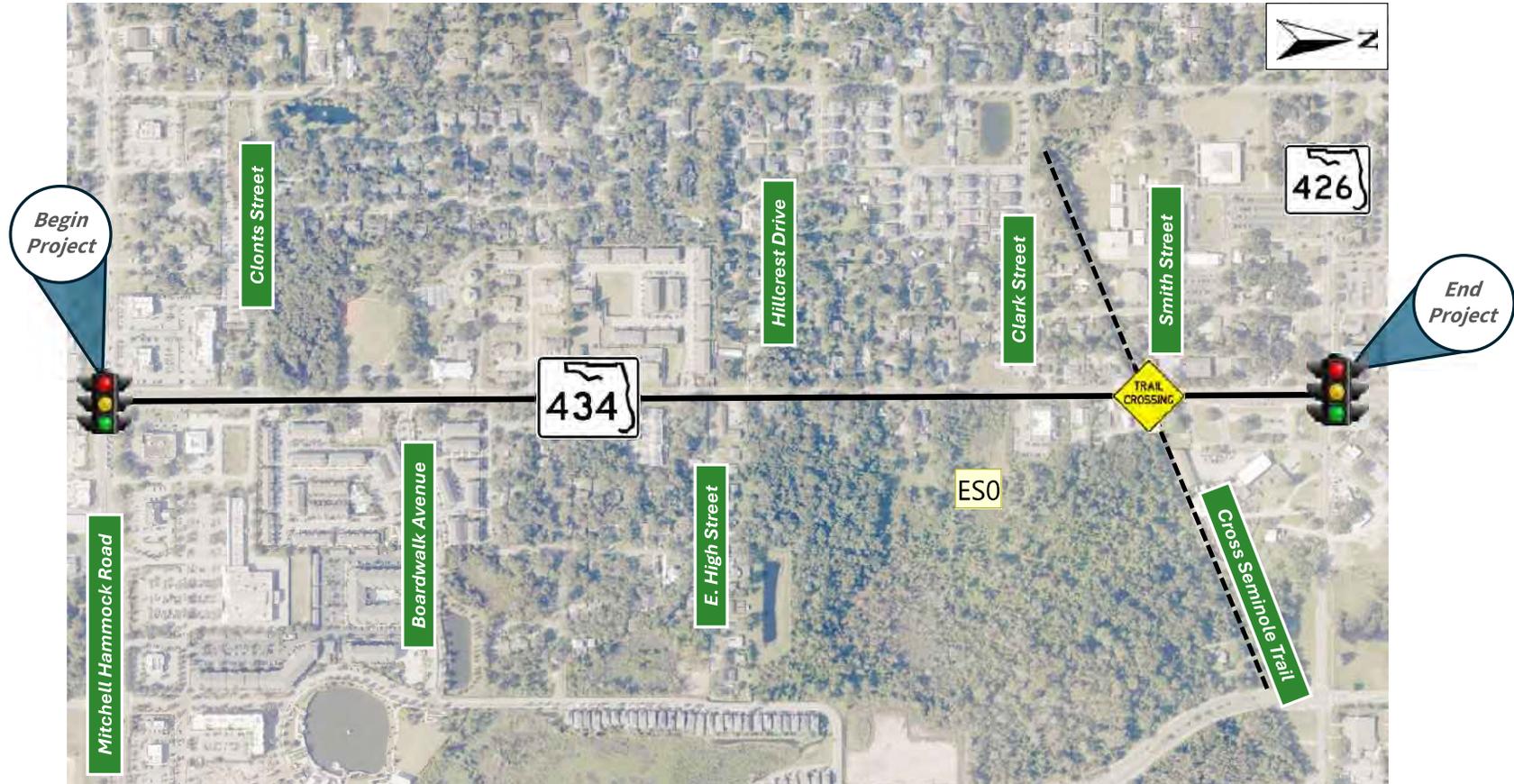
02

Project Overview





Corridor Details



PVT Meeting #2 – April 24, 2024



Project Goals



Safety



**Speed
Management**



**Pedestrian &
Bicycle Mobility**



Data-Driven

- Understand and balance surrounding land uses
- Determine best way to meet current and future needs of roadway users
- Evaluate impacts





03

What we heard...





PVT Meeting #1 – What You Shared



- **Queuing** caused from vehicles waiting to turn left (limited exclusive turn lanes)
- Vehicles using bike lane/shoulder to **pass**
- **Narrow** bike lanes and sidewalks
- Desire to **keep other modes separate** (i.e., bike lanes, walking, etc.) from travel lanes
- **Limited** Right of Way for Improvements

ES0





04

Proposed Improvements



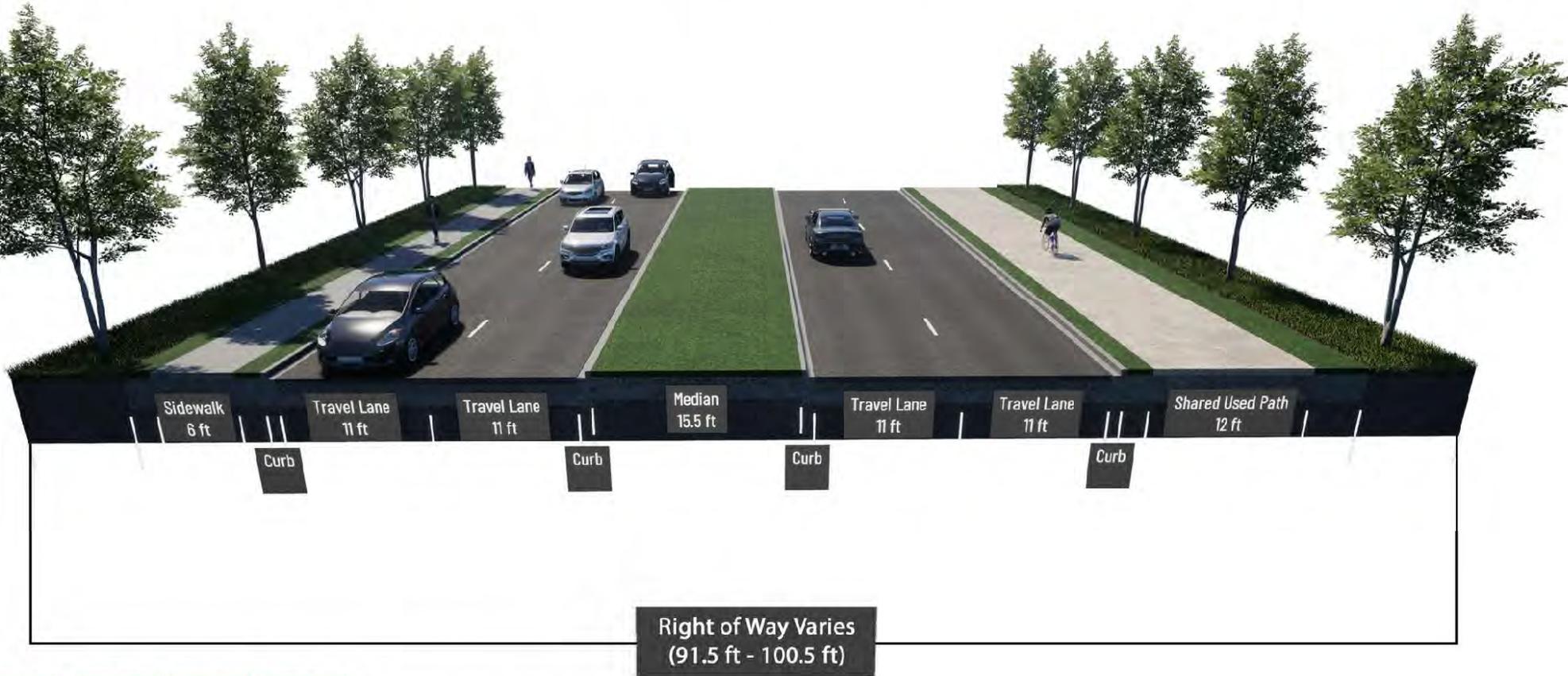


2-Lane, Divided Typical Section





4-Lane, Divided Typical Section





Proposed Rendering at Boardwalk Ave.

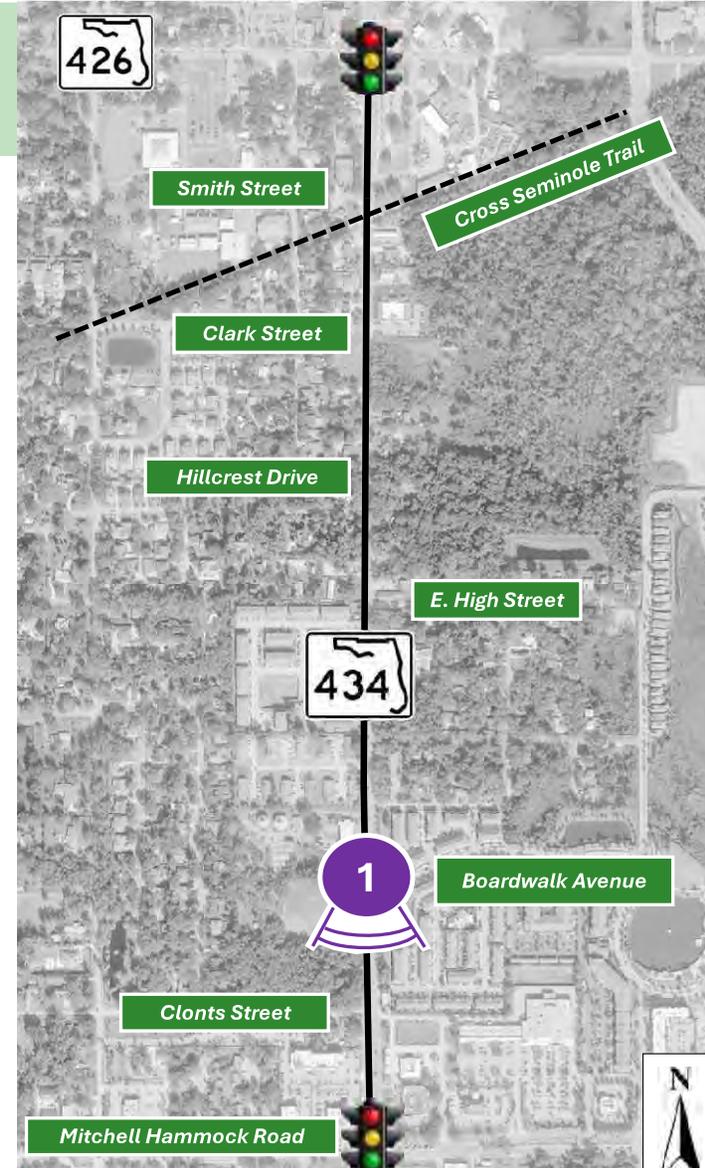
2-lane, Divided



4-lane, Divided



PVT Meeting #2 – April 24, 2024





Proposed Rendering, South of Clark St.

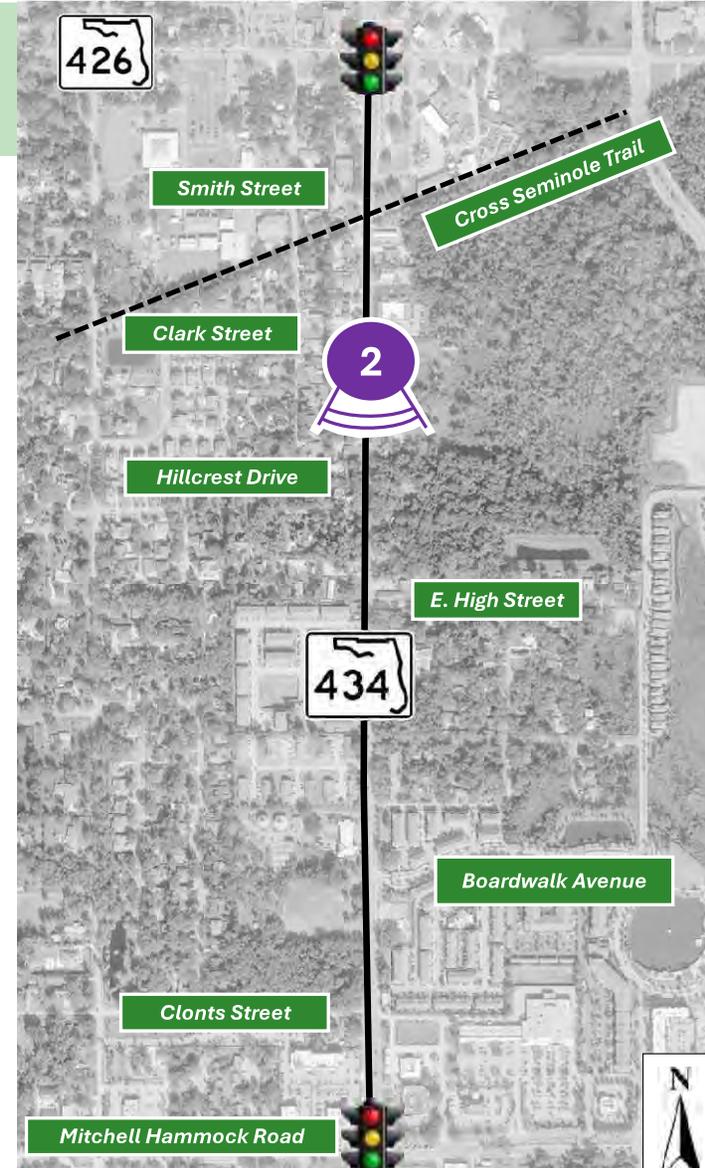
2-lane, Divided



4-lane, Divided



PVT Meeting #2 – April 24, 2024





Pedestrian Hybrid Beacon (PHB)



EXISTING



PROPOSED



PVT Meeting #2 – April 24, 2024



05

What Happens Next?





Next Steps



Timeline



PVT Meeting #2 – April 24, 2024

ES1



Community Event



ES2

In-Person / Open House

May 15, 2024 – 5:30 p.m. to 7 p.m.
Oviedo Amphitheater and Cultural Center
357 Center Lake Lane
Oviedo, FL 32765

Presentation at 6 p.m.

ES0

Virtual Option

[Bit.ly/SR434OviedoStudy](https://bit.ly/SR434OviedoStudy)

Presentation at 5:30 p.m.



PVT Meeting #2 – April 24, 2024

S.R. 434 CORRIDOR STUDY
Financial Project Identification Number (FPID): 162200-4
Community Event
Wednesday, May 15, 2024: 5:30 p.m. - 7:00 p.m.
Oviedo Amphitheater & Cultural Center
357 Center Lake Lane, Oviedo, FL 32765

PROJECT OVERVIEW: The Florida Department of Transportation (FDOT) is conducting a corridor study to evaluate proposed transportation solutions for a segment of State Road (S.R.) 434 (Central Avenue) between Mitchell Hammock Road and State Road (S.R.) 426 (Broadway Street / County Road (C.R.) 419).

The study is considering safety, speed management, network connectivity, and operations for all roadway users along the corridor. The improvements will be considered in the context of surrounding land use, future growth, and potential impacts to the study area.

YOU'RE INVITED: During this community event, solutions will be presented for public feedback. Your participation and involvement is a critical component of the study process. We invite you to attend the community event and stay involved in the process.

WAYS TO PARTICIPATE: FDOT is offering the community event in a hybrid format to provide more ways to participate. As participants, regardless of which platform they choose, will receive the same information.

IN-PERSON OPEN HOUSE: Oviedo Amphitheater & Cultural Center, 357 Center Lake Lane, Oviedo, FL 32765, anytime between 5:30 p.m. and 7 p.m. To view project displays, speak with project team members, and submit comments or questions. A prerecorded presentation will begin at 6 p.m. Free bicycle and vehicle parking available on-site.

VIRTUAL: Interested persons may join from a computer, tablet, or mobile device at 5:30 p.m. For the option, advance registration is required by visiting bit.ly/SR434OviedoStudy. Once registered, participants will receive a confirmation email containing information about joining the community event online. If using a mobile device, use the GoToWebinar app to be able to view the presentation and submit comments. If joining online, please allow adequate time to log in to view the presentation in its entirety.

PROJECT CONTACT:
Erika Shellenberger
FDOT Project Manager
719 S Woodland Blvd.,
Deland, FL 32726
386-943-5378
Erika.Shellenberger@dot.state.fl.us

S.R. 434 NORTHBOUND

All meeting materials, including the presentation, will be available on the project website at: <https://www.CFLroads.com/projects/162200-4>



PVT Meeting #3



- Next PVT Meeting anticipated in Summer 2024
- Share feedback provided during the Community Event
- Obtain input on preferred alternative
- Discuss next steps



Stay Involved!

Erika Shellenberger

Planning Project Manager
Planning & Environmental Management (PLEMO)
FDOT District Five
Erika.Shellenberger@dot.state.fl.us
(386) 943 - 5378

Todd Davis

Consultant Project Manager
Protean Design Group
TDavis@proteandg.com
(407) 246 - 0044 (x122)

PROJECT WEBSITE: <https://www.cflroads.com/project/962200-4>



Appendix D

Project Visioning Team
(PVT) Meeting #3
(April 9, 2025) Meeting
Minutes



Project Visioning Team (PVT) Meeting #3

S.R. 434 (Central Avenue) Corridor Study

April 9, 2025 | 1:00 p.m. – 2:00 p.m.

(Virtual TEAMS Meeting)

Welcome & Introductions

Erika Miller, FDOT In-House Consultant Project Manager, summarized the agenda. The list of attendees for Project Visioning Team Meeting #3 are attached as Appendix A. The PowerPoint presentation (attached as Appendix B) was utilized during the PVT meeting.

Meeting Overview

The S.R. 434 Corridor Study Visioning Team Meeting concluded with a discussion on the final recommendations for the State Road 434 corridor, between Mitchell Hammock and State Road 426 within the City Oviedo. Mrs. Erika Miller provided a brief overview of the study, as it focused on safety, speed management, and pedestrian and bicycle mobility. Current traffic volumes are 16,500 vehicles daily, with 1,500 in the PM peak hour. Two alternatives were considered: a two-lane divided section and a four-lane divided section. Due to high costs and substantial right-of-way impacts, including historical buildings and public facilities, a PD&E study is recommended, should a widening be the ultimate recommendation. Interim improvements, such as pedestrian spot improvements and lane narrowing, were also discussed.

Alternatives Considered

Mrs. Miller provided a summary of the previously presented existing conditions, which included the capacity and safety analysis.

Two alternatives were considered: a two-lane divided section with a median for left turn lanes and a four-lane divided section with a median. Both alternatives include a pedestrian hybrid beacon (PHB) at the Cross Seminole Trail to improve pedestrian safety. Potential environmental impacts include historical buildings, public facilities, and potential contamination from residential septic tanks. Approximately 44 parcels are anticipated to be impacted, with three buildings impacted by the two-lane divided alternative and ten by the four-lane divided alternative.

Community Engagement

The community event in May of last year was well-attended, with 34 general public attendees and 20 comments received. Support for both alternatives and the no-build alternative was noted, along with requests for additional crosswalks and turn lanes. Comments ranged from support for two-lane, to the need for a four-lane alternative, with some suggesting intermediate widening for left turn lanes at select locations. Based on the varied feedback and substantial impacts, the long-term recommendation is a PD and E study to determine the location and conceptual design of feasible build alternatives. No additional funding is programmed for future phases of the project. The anticipated construction cost for the two-lane divided alternative is \$11 million, and for the four-lane divided alternative, it is \$15 million. Right-of-way costs for the two-lane divided alternative are approximately \$5.4 million, and for the four-lane divided alternative, they are almost \$20 million. High costs and impacts are anticipated, similar to the findings of the 2019 county study, leading to the consideration of interim improvements.

Recommendations

Interim Improvements and Operational Considerations for pedestrian/bicycle improvement and vehicular improvements were summarized in slides 14 and 15 (see Appendix B). Note, the vehicular operational improvements include right and left turn lanes at specific locations but require right-of-way and have higher costs. These interim improvements could be implemented via permit or in future safety or operations projects.

Project Visioning Team Input

PVT Member input included the following comments:

- It is very dangerous crossing the Seminole Trail crossing, even with the RRFB.
 - Agreed. As part of our multiple field reviews, we observed drivers not stopping when the yellow light was flashing, with our yellow reflective vests on.
- What does it mean, “these impacts outweigh...”
 - This means the corridor has impacts under both existing and future conditions.
 - The proposed solution is still feasible; however, a PD&E study would be required to evaluate environmental and right-of-way impacts.
 - Interim improvements could still help enhance capacity and safety conditions.
- What was the criteria for establishing the easy, medium and hard categories?
 - Easy, medium, and hard feasibility is based on some of the things listed within the table: the right of way, the construction cost, but also the development process that would be required.

- In addition, the right of way would take time and additional effort to meet the requirements of acquiring the necessary right of way.
- Is this the last meeting for this study?
 - Yes, this is the final meeting before the final materials are uploaded to the project website on cflroads.com
- How does this fit the “vision” of Seminole County, for corridor consistency?
 - The County has been included throughout the study and the County will provide additional feedback when the final report documentation is uploaded.
 - The County is currently working with FDOT on the roundabouts and other improvements along S.R. 434, north of this study’s project limits.
- Will the final report include a recommendation regarding the safety improvements or other next steps?
 - The project team confirmed that the final report will include both long-term and interim recommendations.
 - For long-term improvements—such as whether to maintain a two-lane section or move forward with a four-lane typical—the recommendation is to proceed with a Project Development and Environment (PD&E) study. This would be necessary to further evaluate environmental, cultural, stormwater, engineering, and geometric considerations, along with additional community engagement.
 - The report will also identify interim improvements that can help address safety and operational concerns. These near-term measures can potentially be implemented through coordination with the resurfacing project currently scheduled in the coming years.
- Did the estimates shown in the presentation include costs for construction and right of way acquisition?
 - Yes, the estimates include both construction and right-of-way acquisition costs (subject to change). However, a pond site was not identified as part of this study. The project team anticipates that a pond site will be required to support the proposed widening, which would result in additional right-of-way acquisition costs once a location is determined through further drainage design.
- **Post-meeting Input:** A member of the PVT expressed concern about increased traffic using Oviedo Blvd as an alternate route to S.R. 434 (Central Avenue), particularly when congestion worsens. Oviedo Blvd is primarily residential, with parks, schools, and frequent pedestrian activity, yet drivers often exceed speed limits and disregard pedestrian signals. The concern stems from the study’s overall findings, which suggest that the costs—financial, environmental, and otherwise—outweigh the benefits of widening S.R. 434. As a result, several alternative improvement strategies

have been recommended in lieu of widening. To address potential spillover traffic impacts, it was suggested that the final recommendations also consider specific safety measures for Oviedo Blvd. These include reducing the speed limit to 25 MPH and implementing traffic calming strategies such as speed humps or bumps. These measures would help preserve the safety and quality of life for residents who live along this corridor.

Mr. Mark Trebitz provided final remarks to clarify the study findings and close-out the meeting. He explained that the study assessed both the existing and future conditions of the corridor, including the feasibility of widening from a two-lane typical to a four-lane typical section. While four-laning may be justified from a traffic operations perspective, such long-term improvements carry substantial impacts, including right-of-way acquisition, environmental considerations, and substantial costs. These types of improvements would require a PD&E study and are not currently funded.

In addition to the original scope, the study also identified interim improvements that are more cost-effective and less impactful. These measures, such as improvements to turn lanes and operational enhancements, could be considered for inclusion in the corridor's upcoming resurfacing project, currently scheduled for Fiscal Year (FY) 2029. Coordination will continue with the City of Oviedo and MetroPlan Orlando to determine which of these interim improvements may be incorporated during the project's scoping phase in FY 2027.

Mr. Trebitz emphasized that while four-laning could address capacity issues, interim solutions—such as the addition of left-turn accommodations—could substantially relieve congestion in the near term without the disruptive impacts of full widening.

Next Steps

Ms. Miller concluded the meeting by informing the PVT members that the meeting minutes will be distributed to them the following week. The final presentation and documentation will be posted to the project website on CFLRoads.com. Feedback is welcome, and team members are encouraged to contact Erika Miller (FDOT Project Manager) or Todd Davis (Consultant Project Manager) with any questions or comments.

The project web page (www.cflroads.com/project/962200-4) and contact information were also provided. A copy of these notes will be sent via email to all the team members, with a copy of the presentation. Included in the presentation is the contact information for Ms. Shellenberger and Mr. Davis.

Appendix A

Project Visioning Team Meeting #3 Attendees

Project Visioning Team Meeting #3 Attendee List

FDOT Project Team

- Erika Miller – FDOT (in-house consultant)
- Joseph Fontanelli – FDOT
- Mark Trebitz - FDOT
- Todd Davis – Protean Design Group
- Brittany Nesbitt – Protean Design Group

Project Visioning Team Members

- Paul Yeargain – City of Oviedo
- Dave Axel – City of Oviedo CRA
- Charlie Wetzel – Seminole County
- Dino Lucarelli - Seminole County
- Arturo Perez - Seminole County
- Matt Hassan - Seminole County
- Tony Nelson – Seminole County
- Alex Trauger – MetroPlan Orlando MPO
- Derek Catlin – Oviedo Park Terrace HOA
- Yvette Baldonado – Oviedo Park Terrace HOA

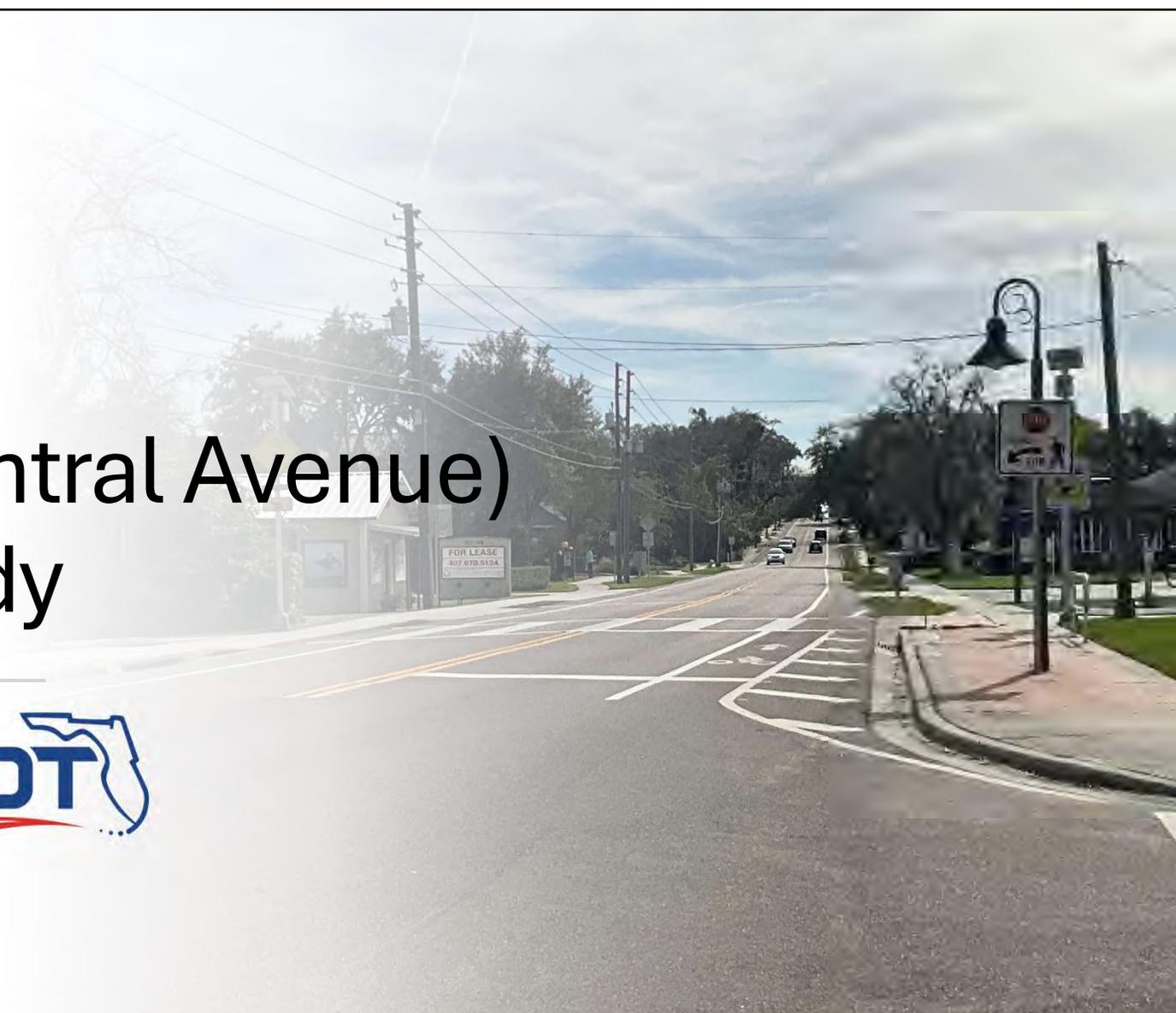
Appendix B

PowerPoint Presentation



S.R. 434 (Central Avenue) Corridor Study

April 9, 2025



Meeting Agenda

01

Project Overview

02

Alternatives Considered

03

Community Engagement

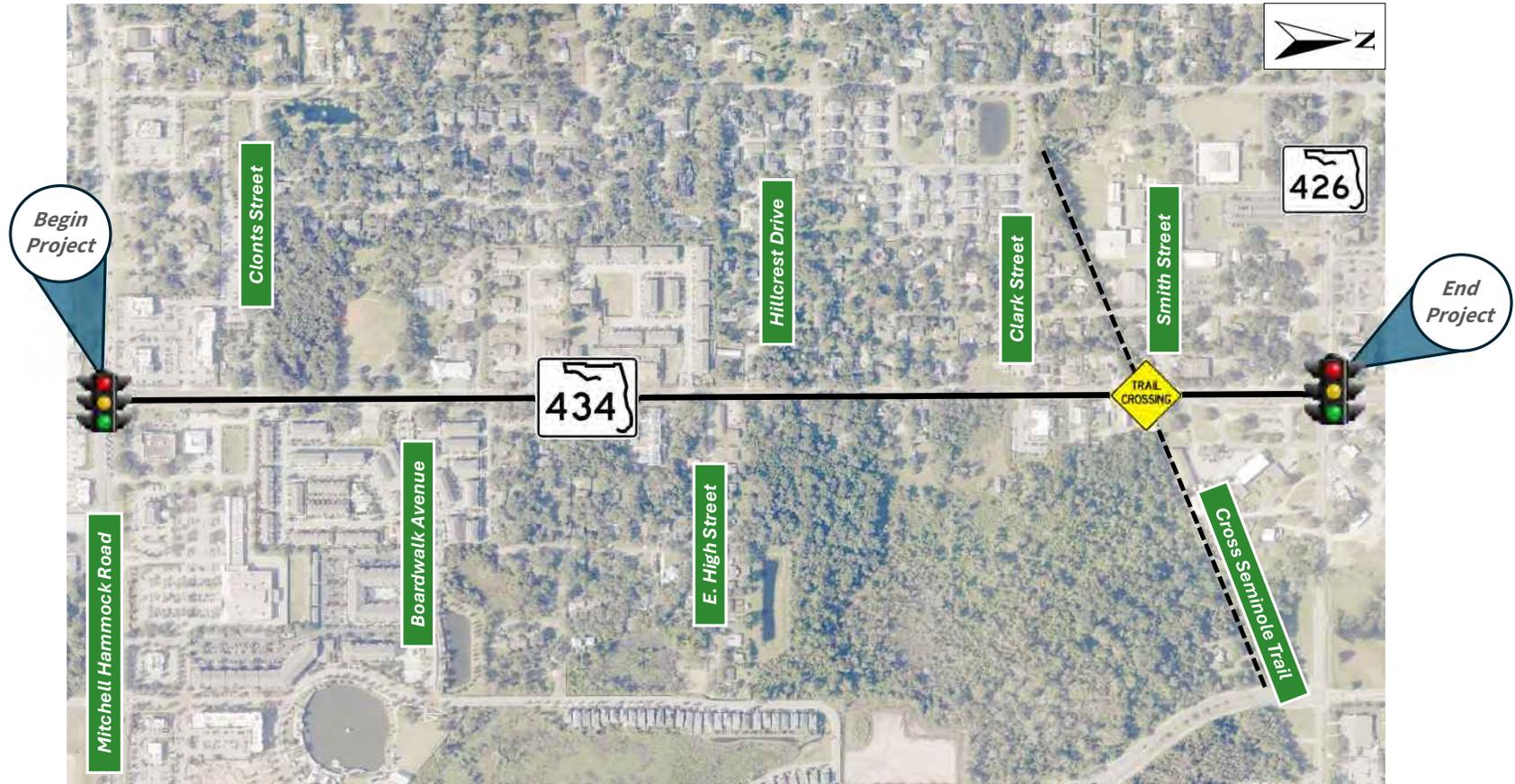
04

Recommendations





Corridor Details





Project Goals



Safety



**Speed
Management**



**Pedestrian &
Bicycle Mobility**



Data-Driven

- Understand and balance surrounding land uses
- Determine best way to meet current and future needs of roadway users
- Evaluate impacts





Existing Operations



- Traffic:
 - Currently at LOS D w/ small portion at LOS E
 - In 2045, predicted at LOS E w/ small portion at LOS F
 - Queueing at Signalized Intersections
- Safety:
 - No fatal crashes within last 5 years
 - Most frequent crash type: rear-end



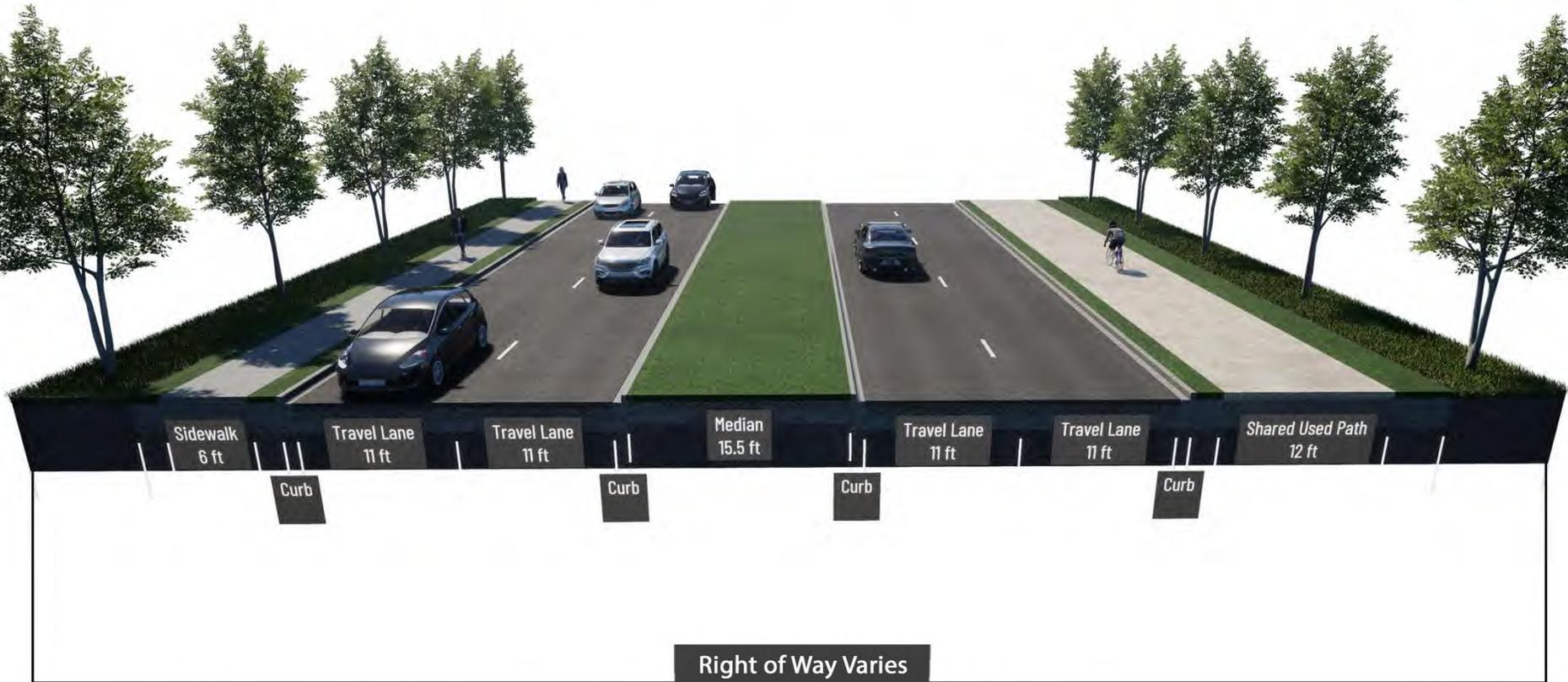


2-Lane, Divided Typical Section





4-Lane, Divided Typical Section





Pedestrian Hybrid Beacon (PHB)



EXISTING



PROPOSED





Anticipated Impacts

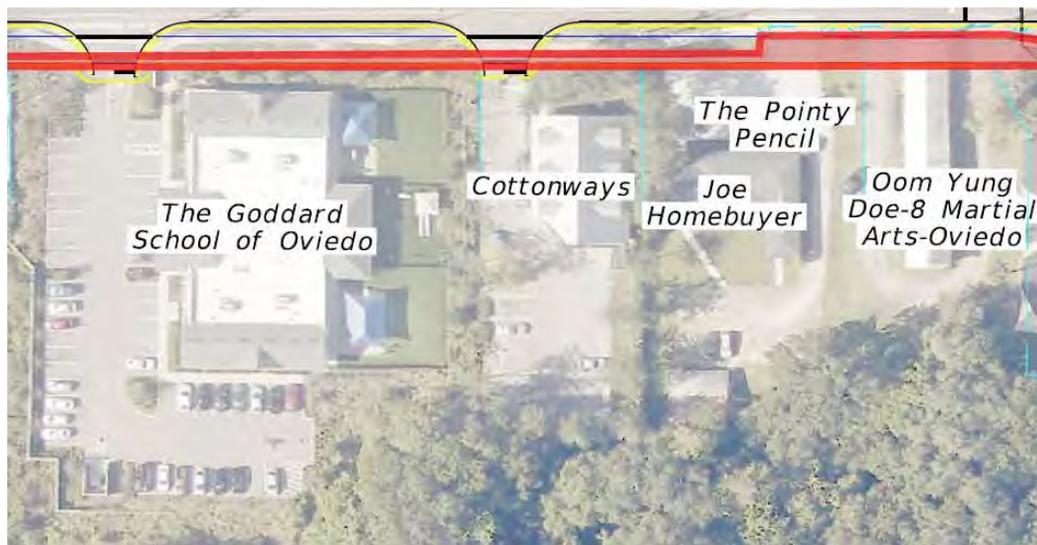


Environmental:

- Potential impacts to Historical Buildings:
 - 1 on the National Registry
 - Multiple building prior to 1970
- Potential impacts to Boston Hill Park, Church of God of Prophecy, and Local Businesses
- Potential contamination sites
 - Residential septic tanks

Right of Way:

- A minimum of 44 Parcels Impacted
- A minimum of 3 Buildings Impacted
- Likely need for Retention Pond





Community Engagement



Timeline





Community Event Feedback



- Event was well attended:
 - 34 general public
 - 20 comments received
- Support for each alternative, as well as no build
- Requests for Additional Crosswalks and Turn Lanes
- Concerns with speed and safety





Community Event Feedback



2-Lane Support

COMMENTS:

Any design that encourages people to exit 417 to save 25¢ and have overpass serve as a cut-through should be avoided
2-lane max - I bike here all the time

COMMENTS:

- ① keep 2 lane
- ② ~~add~~ add turn lanes by hillcrest, Clark, Wood
- ③ crosswalk by High St. there is a bus stop there, plus everyday high school kids cross there on their way home from school.

4-Lane Support

COMMENTS:

434 NEEDS TO BE 4 LANES. EVEN WITH WIDENING TURN LANE, IT CANNOT HANDLE THE POTENTIAL TRAFFIC AFTER THE 5 STORY APARTMENTS ARE BUILT AT 417/426 & 434.
TURN LANE MAY ACCOMMODATE ONE DIRECTION OF TRAFFIC, BUT NOT BOTH.

The four-lane alternative is the only logical approach to resolving the traffic load in Oviedo so we are in agreement with the plan for this alternative. Although not mentioned in this study, Central Blvd should also be expanded to four lanes from Broadway St to where the road becomes four lane again near the Toll Way 417 overpass, Then SR434 will be at least four lanes through Oviedo. Stop lights should be installed at the corner of Central Blvd and Artesia St. rather than a traffic circle which is being considered now. When one of the intersecting roads has heavy traffic, such as SR 434 does, and the side street has much lighter traffic, a circle will not work well because the busy road will dominate the traffic pattern and the side street will have a very difficult time trying to merge in. A traffic circle is okay when both intersecting roads do not have real heavy traffic

Support No Change

A/H: No change Light At Clark St
A/H +1: Pedestrian flyover A Seminole Trail

Consider intermediate widening for left turn lane at select locations only.

we support improvements to Central Ave / 434 that decrease overall speed and do not increase the current Right of Way.



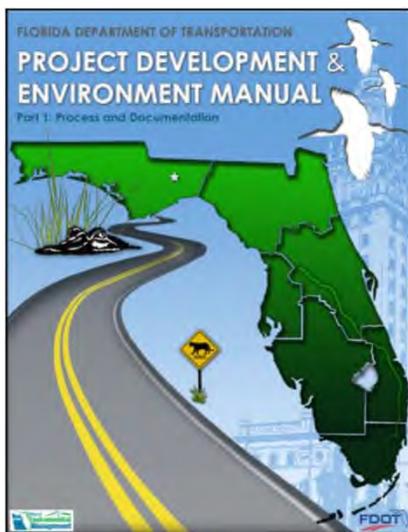


Recommendations



Long-Term Improvements:

- PD&E Study
- Identify preferred alternative
- Evaluate and identify corresponding impacts



Funding:

- No additional funding programmed for future phases.
- Anticipated Construction Cost
 - 2-Lane Divided w/ Trail Alternative: \$11 M
 - 4-Lane Divided w/ Trail Alternative: \$15 M
(high-level estimate, subject to change)
- Anticipated ROW Cost
 - 2-Lane Divided w/ Trail Alternative: \$5.4 M
 - 4-Lane Divided w/ Trail Alternative: \$19.7 M
(high-level estimate, subject to change)
(does not include ROW cost for stormwater pond)
(does not include business damage and relocation costs)

Consistent Findings:

- Similar findings to County study in 2019





Interim Improvements



Interim Pedestrian Spot Improvements:

These improvements could be further evaluated and considered for implementation in a future resurfacing project, via permit, or future maintenance efforts.

Pedestrian Improvements				
Improvement	Location	ROW Needed?	Construction Cost	Feasibility (Easy, Medium, Hard)
Midblock Crosswalk at Boston Hill Park	Boardwalk Avenue	No	City of Oviedo has Plans, Waiting on Funding	
Provide PHB and Raised Crossing at Cross Seminole Trail	Cross Seminole Trail	No	\$397,000	Easy
Restripe Crosswalks Along Minor Street Approaches	18 Approaches	No	\$23,000	Easy
Sidewalk Maintenance/Spot Improvements	Reconstruct East Sidewalk	No	\$500,000	Medium
Consider Lane Narrowing (restriping) for Wider Bike Lane	Lindsay to Smith	No	\$148,000	Medium
RRFB Midblock Crosswalk	High Street	No	\$71,000	Medium
PHB Midblock Crosswalk	Clark Street	No	\$357,000	Medium

Notes:

High-level cost estimates, subject to change.

Proposed PHB Mast Arms can be constructed within existing ROW

To avoid ROW acquisition, RRFB could be constructed at High Street





Interim Improvements



Interim Vehicular Operational Improvements:

These improvements **require right of way** and could be implemented via permit or in a future Department safety or operations project.

Operational Improvements				
Improvement	Location	ROW Needed?	ROW + Construction Cost	Feasibility (Easy, Medium, Hard)
Addition of Right-Turn Lane	SB at Smith or Garden Street	Yes, 1 Parcel	\$2,217,000	Hard
	SB at Hillcrest Drive	Yes, 3 Parcels	\$2,395,000	Hard
Addition of Left-Turn Lane	SB at Wood	Yes, 4 Parcels	\$3,779,000	Hard
	SB at Lawn St.	Yes, 4 Parcels	\$2,261,000	Hard
	NB at Hillcrest	Completed by Developer of Townhomes		
	NB at Clark	Yes, 4 Parcels	\$3,604,000	Hard

Note: High-level cost estimates, subject to change.



Stay Involved!

Erika Miller

Planning Project Manager
Planning & Environmental Management (PLEMO)
FDOT District Five
Erika.Miller@dot.state.fl.us
(386) 943 – 5378

Todd Davis

Consultant Project Manager
Protean Design Group
TDavis@proteandg.com
(407) 246 – 0044 (x122)

PROJECT WEBSITE: <https://www.cflroads.com/project/962200-4>



Appendix E

Community Event Flyer



S.R. 434 CORRIDOR STUDY

Financial Project Identification Number (FPID): 962200-4

Community Event

Wednesday, May 15, 2024: 5:30 p.m. - 7:00 p.m.

Oviedo Amphitheater & Cultural Center

357 Center Lake Lane, Oviedo, FL 32765



PROJECT OVERVIEW: The Florida Department of Transportation (FDOT) is conducting a corridor study to evaluate proposed transportation solutions for a segment of State Road (S.R.) 434 (Central Avenue) between Mitchell Hammock Road and State Road (S.R.) 426 (Broadway Street / County Road (C.R.) 419).

The study is considering safety, speed management, network connectivity, and operations for all roadway users along the corridor. The improvements will be considered in the context of surrounding land use, future growth, and potential impacts to the study area.



Safety



Speed Management



Network Connectivity



Traffic Operations



S.R. 434 NORTHBOUND

YOU'RE INVITED: During this community event, solutions will be presented for public feedback. Your participation and involvement is a critical component of the study process. We invite you to attend this community event and stay involved in the process.

WAYS TO PARTICIPATE: FDOT is offering the community event in a hybrid format to provide more ways to participate. All participants, regardless of which platform they choose, will receive the same information.

IN-PERSON OPEN HOUSE: Oviedo Amphitheater & Cultural Center, 357 Center Lake Lane, Oviedo, FL 32765, anytime between 5:30 p.m. and 7 p.m. to view project displays, speak with project team members, and submit comments or questions. A prerecorded presentation will begin at 6 p.m. Free bicycle and vehicle parking available on site.



VIRTUAL: Interested persons may join from a computer, tablet, or mobile device at 5:30 p.m. For this option, advance registration is required by visiting bit.ly/SR434OviedoStudy. Once registered, participants will receive a confirmation email containing information about joining the community event online. If using a mobile device, use the GoToWebinar app to be able to view the presentation and submit comments. If joining online, please allow adequate time to log in to view the presentation in its entirety.



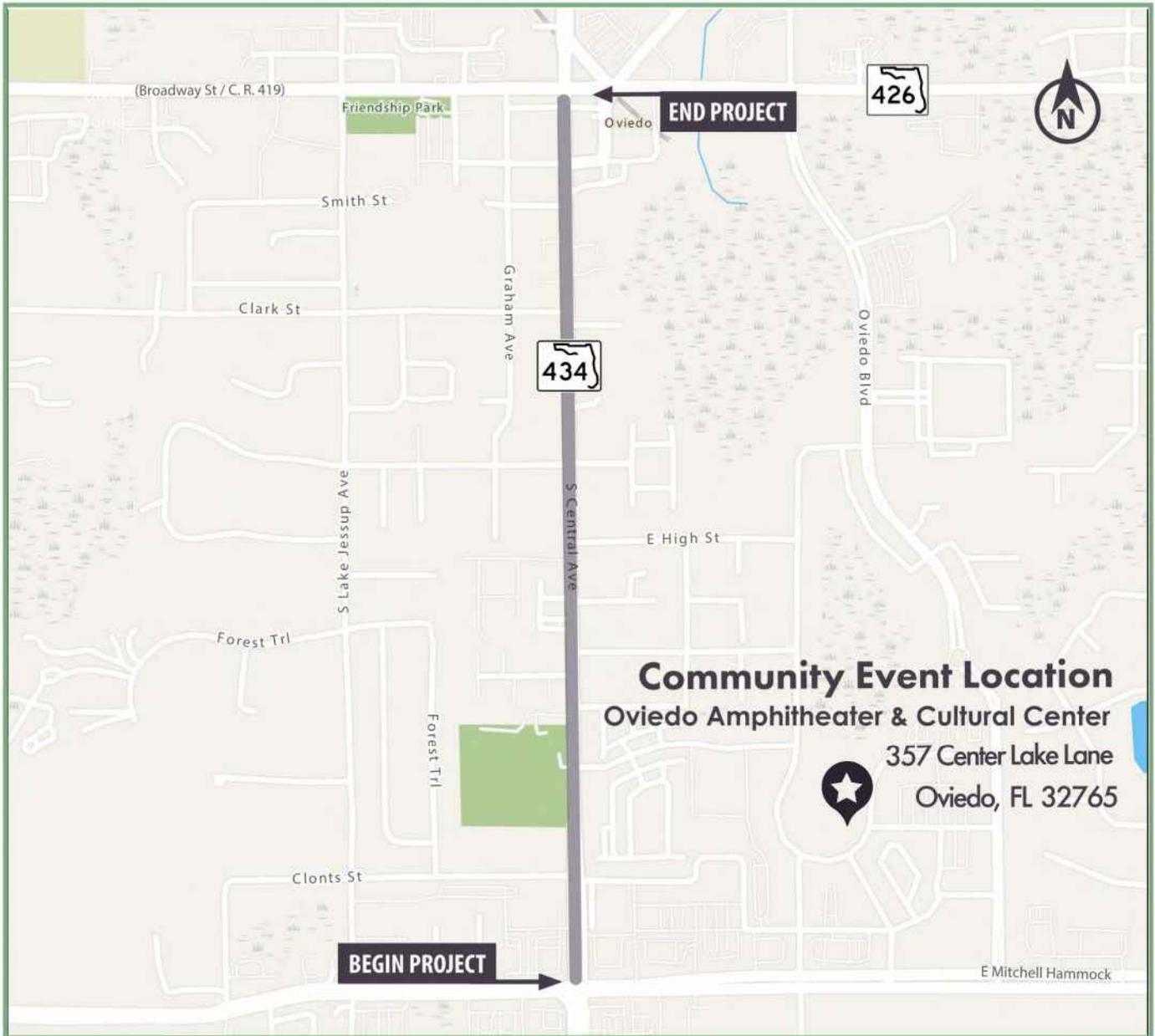
PROJECT CONTACT:

Erika Shellenberger
FDOT Project Manager
719 S Woodland Blvd.,
DeLand, FL 32720
386-943-5378

Erika.Shellenberger@dot.state.fl.us



All meeting materials, including the presentation, will be available on the project website at: <https://www.CFLroads.com/project/962200-4>.



ADA Accommodations: Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation services (free of charge) should contact Erika Shellenberger, using the information provided on this flyer at least seven (7) days prior to the meeting. If you are hearing or speech impaired, please contact us by using the Florida Relay Service, 1-800-955-8771 (TDD) or 1-800-955-8770 (Voice).

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons wishing to express their concerns relative to FDOT compliance with Title VI may do so by contacting Melissa McKinney, FDOT District Five Title VI Coordinator, at Melissa.McKinney@dot.state.fl.us.

Appendix F

Community Event Summary



COMMUNITY EVENT PLAN SUMMARY

S.R. 434 – OVIEDO CORRIDOR STUDY

May 15, 2024

EVENT:	Hybrid Community Event
PROJECT NUMBER:	Financial Project Identification (FPID) No.: 962200-4
PROJECT DESCRIPTION:	<p>The Florida Department of Transportation (FDOT) is conducting a corridor study for State Road (S.R.) 434 (Central Avenue) in the City of Oviedo. The study limits are from south of Mitchell Hammock Road to north of S.R. 426 (Broadway Street/County Road 419), which is approximately 1.2 miles in length. The purpose of the study is to evaluate alternatives which enhance safety, speed management, network connectivity, and operations for all roadway users. The Corridor Study will consider surrounding land use, future growth, and potential impacts to the study area to determine the location and conceptual design of preferred roadway improvements.</p>
IMPACT:	<p>A study was previously performed by Kittelson & Associates on behalf of Seminole County. The corridor has limited right of way available and local stakeholders anticipate that any recommendation will have right of way implications. This would require additional funds for right of way purchases, as well as impact future project schedules. There is a question of whether a 4-lane corridor is necessary or if a median, with exclusive left turn lanes, are needed for safety. Commissioner Dallari has -maintained close communication with the Department regarding this corridor study and future needs.</p> <p>S.R. 426, located on the north end of the study limits, is currently under construction and anticipated to complete the 4-lane widening by summer 2024.</p>
DEMOGRAPHICS:	<p>Over 55% of residents within vicinity of the corridor identify as White Alone and close to 9% identify as Black or African American Alone. Over 16% of residents claimed to be of Hispanic or Latino ethnicity. The minority population is over 63.6%. Less than 0.5% of residents claim to not speak English well, or not at all, age 5 and over. Therefore, it is not anticipated that bilingual information will be necessary. However, consultant staff will be available to provide support in other languages.</p>
EVENT DATE & TIME:	Wednesday, May 15, 2024, 5:30 p.m. to 7:00 p.m.
IN-PERSON LOCATION:	<p>Oviedo Amphitheatre & Cultural Center 357 Center Lake Lane, Oviedo, FL 32765 Located east of the corridor's location.</p>
VIRTUAL PLATFORM:	<p>GoTo Webinar bit.ly/SR434OviedoStudy</p>

PROJECTED IN-PERSON ATTENDANCE:

Protean team will provide event flyers to local businesses in the area 3 weeks in advance of the community event. Additional handouts will be provided to PVT members (i.e., HOAs, City of Oviedo, and Seminole County) for distribution. Anticipated attendance could range between 10 and 50 people, but we will know more after receiving input from the Project Visioning Team (PVT) meeting that will be scheduled for April 24, 2024.

It is anticipated City of Oviedo staff (Bobby Wyatt and Paul Yeargain), Seminole County staff, and Commissioner Bob Dallari will be in attendance.

VENUE TECHNOLOGY:

The venue is equipped with internet access and outlets. For the presentation, the conference room has multiple TVs on the wall, with audio speakers in the open ceiling. A HDMI cord is accessible for the consultant team to connect the computer to and play a prerecorded presentation on all TV screens, with audio. The TVs will be utilized during the 6 p.m. presentation with the speakers in the venue (with a backup speaker available). After the 6 p.m. presentation, the presentation will be put on a loop with minimal sound levels on a few select TVs located away from the board / roll plot locations.

The venue has access to 245 black banquet chairs and 32 rectangular tables, available for placement and use by the project team. The center includes multiple chairs set up for stadium sitting, with a raised platform for in the upper center of the room, accessible by stairs on the left or a ramp to the right.

The consultant project team will provide a computer, projector, and large projector screen for the prerecorded presentation. Seats will be rearranged for stadium seating on the right side of the room. The consultant team will include additional seating along the exterior walls for those needed to sit, as well as near the comment table to allow attendees to sit along the parameter.

The consultant will provide a backup for all audio and visual systems in addition to what the venue offers. All wires from the projector setup should be covered using a tablecloth. The consultant will provide tablecloths to cover all tables used during the community event. Under no circumstances will cardboard be allowed to prop up projectors.

VENUE ROOM LAYOUT:

See attached room layout and pictures.

Sanitation stations are currently available outside the bathrooms. Additional sanitizer will be available at all tables (i.e., reception table, comment table, and roll plots). Displays will be spaced based on the number of boards provided, attempting to meet the six feet separation. Bathrooms are ADA accessible and located towards the left side of the raised platform (south side of building). Room details are as follows:

- Reception / Welcome area: Sign-in table will be located at the entry way of the middle doors. An A-frame, with a welcome sign, will be placed at the entrance to the building to identify the entrance location. The doors to the left and right will be closed. Display boards (including Title VI will be

located next to the reception table. As each attendee signs in, each will receive a packet with the project brochure, comment card, and pen.

- Room (*trapezoid shape: W1 – 67.5-ft, W2 - ~78-ft, L - ~34-ft*): Set up with approximately 6 to 9 display boards (to be finalized based on number of alternatives and based on coordination with project team) and 1 roll plot (per alternative) on tables. A table where attendees can sit and fill out their comment cards will also be available along the parameter of the room.

Set up for viewing looping presentation with 55 chairs (5 rows, 11 chairs across) and project screen. More can be set up based on the number of attendees. Additional 48" TV, with hookup to laptop will be available within room to zoom in/out of google maps to discuss with attendees. Drop boxes will be available in near the front of the room, near the reception area for attendees to return their comment cards.

- Handicap access is provided via the south side of the building (left of the entrance). Designated handicap parking spots are provided along Center Lake Lane, as well as in the parking lot. The reception area and rooms are located in the center, which is only 1-story.
- There are a total of approximately 235 parking spaces within the parking lot, with additional diagonal parking along Center Lake Drive, with a drop off circle located at the building front and loading docks available on the north side of the building. Additional diagonal parking is located along Center Lake Lane.
- Directional parking signs will direct people to park in the parking lot to the north. An additional a-frame sign will be located at the passenger drop-off circle to identify the event entrance. The outside parking signs will be posted in the ground via wire h-stakes.

VENUE SAFETY PLAN:

All staff and attendees will comply with current safety protocols. Wearing masks will be optional; however, study team members will be encouraged to wear them. Masks will be available for attendees who do not have one. Hand sanitizing stations will be provided when entering the venue.

COMMUNITY EVENT FORMAT: The in-person and virtual community events will occur concurrently.

For the in-person component:

- The in-person location will operate as a traditional open house, where attendees may arrive anytime between 5:30 p.m. and 7:00 p.m. to view project exhibits, speak with project team members, and provide comment.
- A prerecorded presentation will be shown at 6:00 p.m. in the same event space. This presentation will be shown once to those attending in-person. However, a looping presentation will be played on a side TV after the 6:00 p.m. showing, in case anyone arrives after it. The sound will be kept to a minimum, enough to be heard within a few feet, but not to disturb the rest of the room.
- The FDOT PM will attend the in-person community event.
- Consultant staff will arrive at 2:00 p.m. to set up for the in-person community event.
- There will be no formal public comment period.
- The in-person portion of the event will end at 7:00 p.m.

For the virtual component:

- Hosted by FDOT at FDOT District 5 District Office – 719 S. Woodland Boulevard, DeLand, FL 32720
- Phone number for technical difficulties will be provided and monitored in a separate room at FDOT by Department staff
- The virtual community event connection will be via hardwired internet at the FDOT District 5 district office
- FDOT staff will pre-record an introduction video of Erika Shellenberger (FDOT PM) that will welcome virtual attendees. This short video will be embedded in a presentation slide.
- Advance registration will be required to participate virtually.
- The GoTo Webinar will be hosted by Department staff and will begin at 5:30 p.m.
- Staff will open the virtual webinar in practice mode at approximately 5:00 p.m.
- The presentation for the virtual community event will be played once. However, the virtual community event organizers may play the presentation a second time if they notice a significant number of attendees joined late.
- For the virtual component, comments or questions can be entered into the questions pane. Simple questions (such as what’s the schedule? where can I find documents? etc.) may be answered only to the person who made the comment. Responses to other questions will be provided in writing at a later date. If providing a general acknowledgement response (such as ‘thank you for your comment/question’), the person providing the response should be connected via hardwired internet.
- The chat feature will be turned off.
- The following information will be available on the Control Panel as Handouts for the virtual community event:
 - Display boards
 - Project Information Brochure
 - Presentation slides
 - Comment Form (fillable PDF)
- The virtual portion of the community event will end after the project presentation.

For both the in-person and virtual components:

- No formal public comment period will be held for the in-person or virtual component of the community event.
- Comments may be submitted after the community event by in-person or virtual attendees via comment form, telephone, mail and/or email.
- Responses to questions will be provided in writing at a later date.

CONTACT:

FDOT PM Erika Shellenberger
Consultant (Kimley-Horn)
(386) 943-5378
Erika.Shellenberger@dot.state.fl.us

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NOTIFICATION MATERIALS:

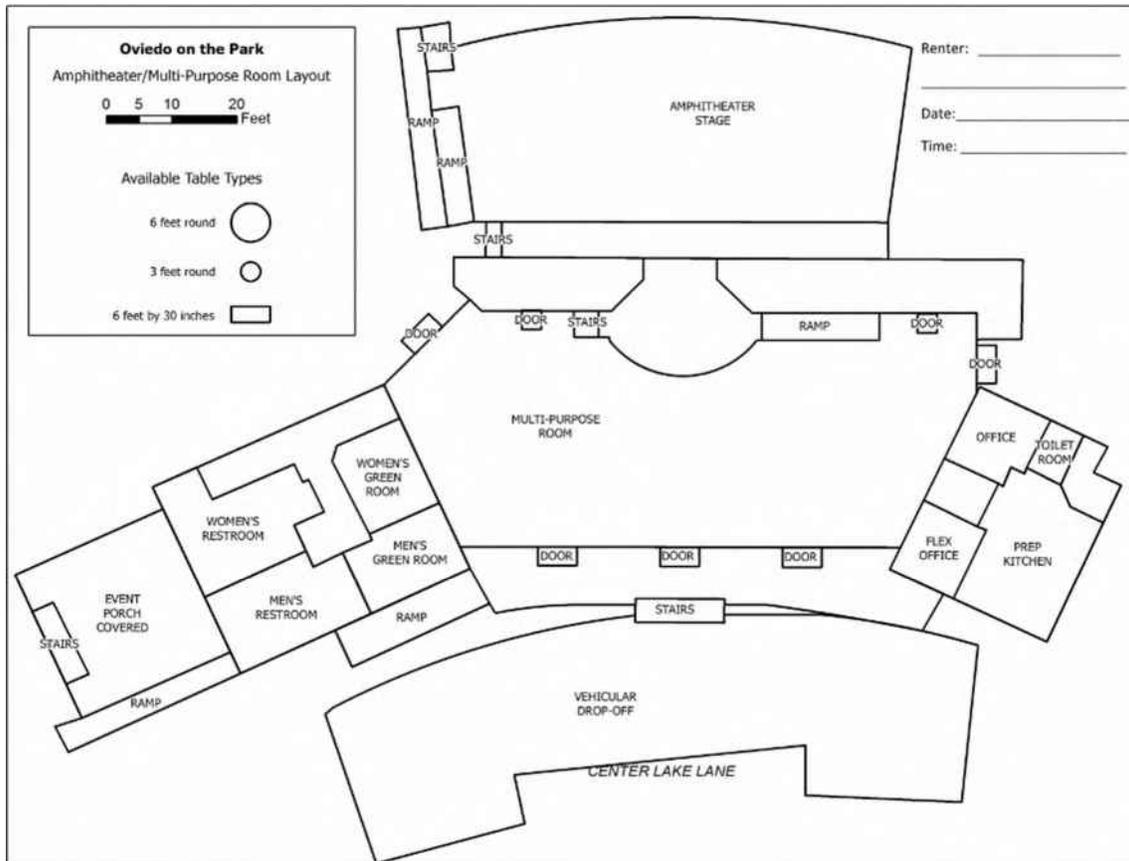
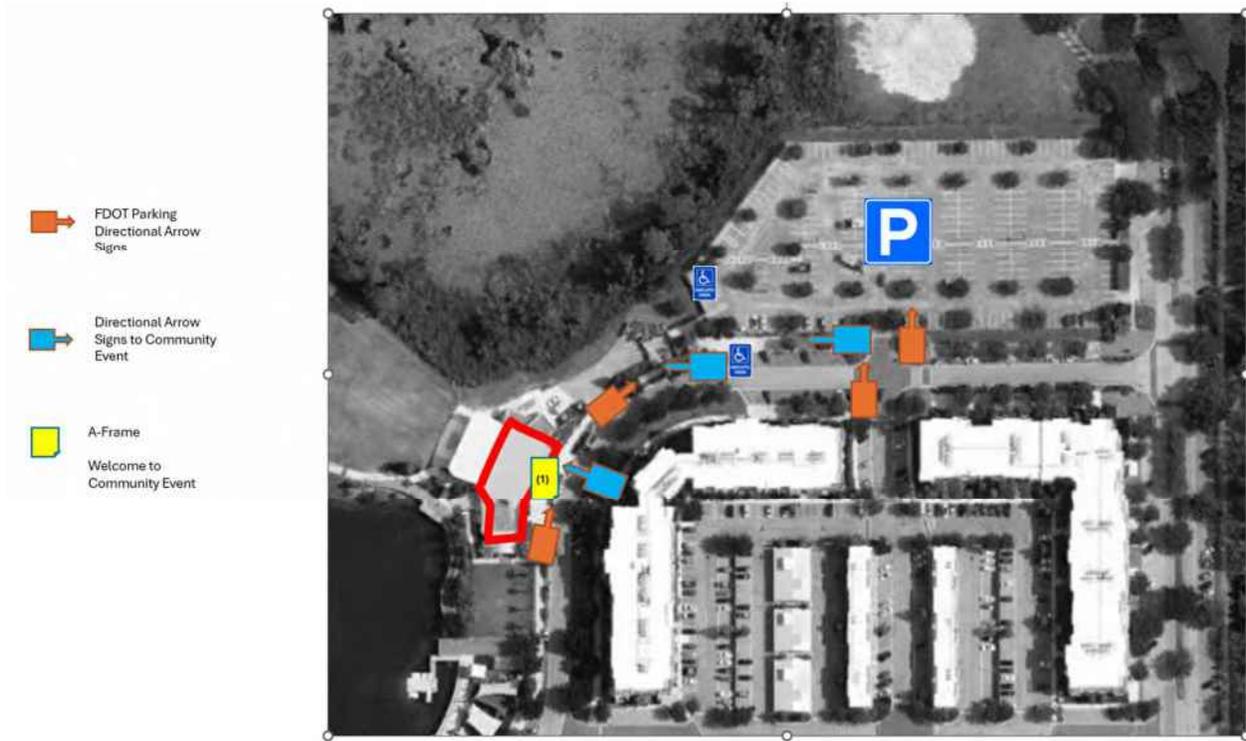
- Project Information Flyer (for grassroots approach)
- Project Information Brochure (handed out at Community Event)
- Newspaper ad – Ad published once on May 6, 2024
- FAR ad – once on May 6, 2024
- Press Release
- Notice on fdot.gov – Public Notices Page for D5
- <https://www.cflroads.com/project/962200-4>

STAFF ASSIGNMENTS:

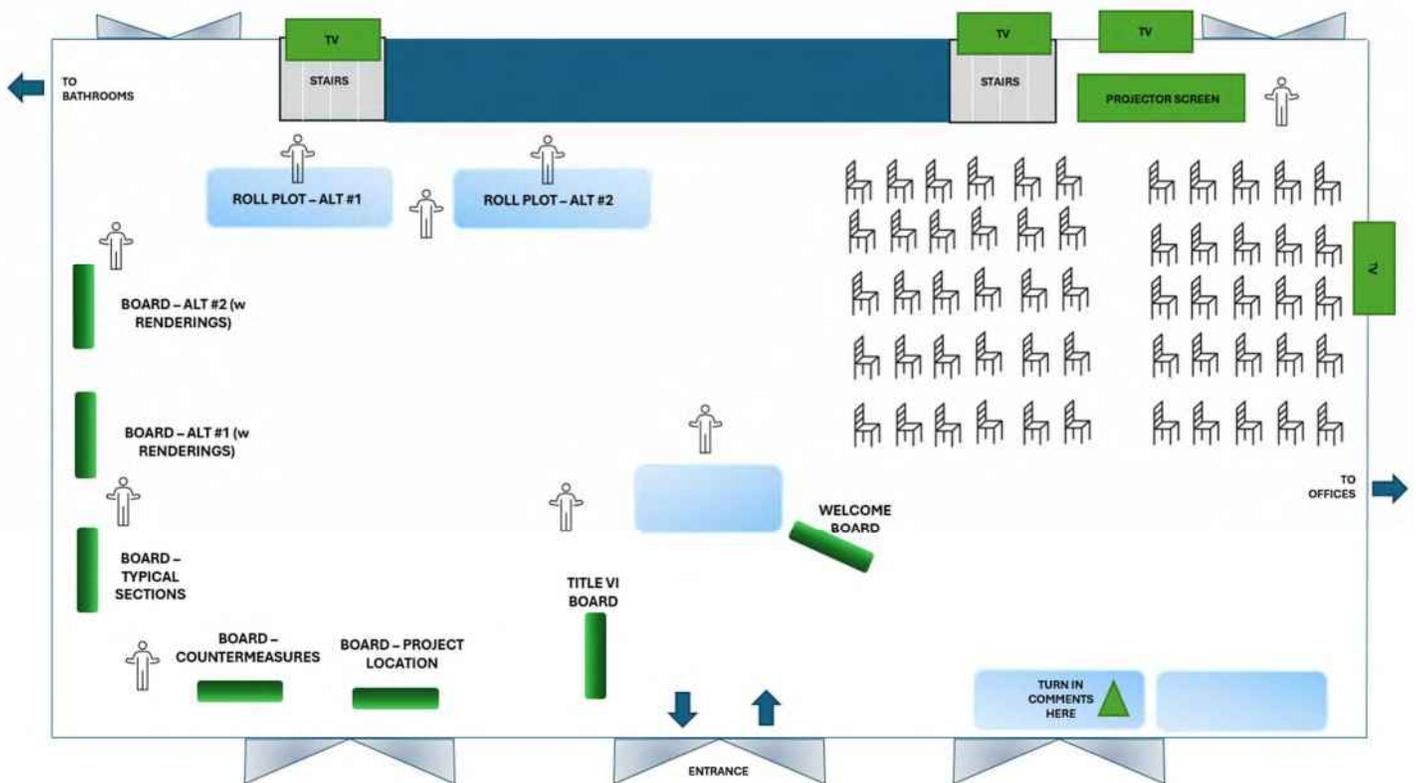
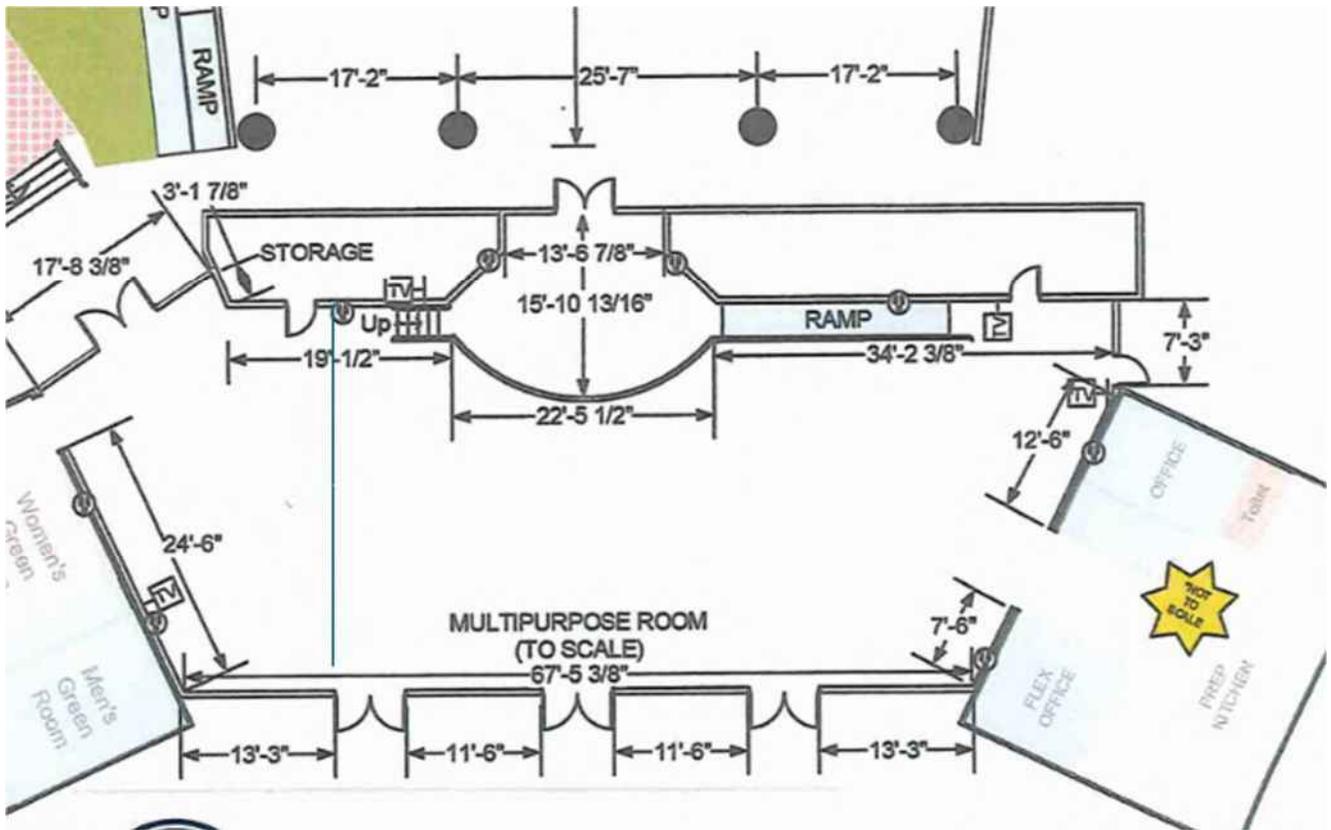
- Onsite Team:
 - Protean Design Group (8, including PM)
 - InNovo Partners (2)
 - FDOT PM and additional FDOT staff (as needed)
- Virtual Component Team: Protean Design Group (1); FDOT (1)

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VENUE LAYOUT:



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OUTSIDE



Room



Appendix G

Community Event Talking Points





**FPID #962200-4 S.R. 434 Corridor Study
Community Event Talking Points
May 15, 2024**

Project Overview - The S.R. 434 Corridor Study is located in the City of Oviedo (Seminole County). The study is evaluating the 1.02-mile stretch of from Mitchell Hammock Road to S.R. 426 / C.R. 419 / Broadway Street.

Seminole County previously studied S.R. 434 in YR 2019, however included a bigger study area.

- SOUTH (this section) – between Mitchell Hammock Road and Smith Street
- NORTH – between Franklin Street and S.R. 417

As such, this study is being conducted to complement the previous study, zero in on the southern section of the state roadway (S.R. 434), and gain consensus on a preferred alternative. The no build alternative is still being considered as any improvements would significantly impact right of way. No future phases and/or funding has been identified at this time. The City of Oviedo was involved in the previous study and this study. Note, the City did request the removal of on-street bike lanes, with a shared-use path along the east side of the roadway.

In addition, cutouts of potential exclusive left-turn lanes will be available to event attendees. Due to the amount of closely spaced driveways and off-set intersections, the team wants to obtain input from the public on where potential median openings/turn lanes should be considered. The intent of these cutouts is to graphically show where turn lanes/median openings could be located throughout the corridor and due to the FDM requirements, show the public how the required turn lane length could impact how close median openings could be located. We will need to further evaluate where the median openings are located; however, want to get as much input as we can, without making any promises.

Corridor Details:

- The corridor currently has a posted speed limit of:
 - Mitchell Hammock Road to north of Clark Street: 35 mph.
 - North of Clark Street to S.R. 426: 30 mph
- The travel lanes are 12-feet (with an 11-foot center two-way left turn lane, where available) with a 4-foot on-street bike lane and varying sidewalk widths between 6-foot and 5-feet.
- The right of way varies throughout the study limits, between 52.75-feet and 65-feet, wide enough to service the existing roadway only.
- There are numerous driveways throughout the corridor, servicing both residential and commercial uses. This is a grid network for most; however, some neighborhoods along the west have no alternate access.
- Drainage is a concern along the corridor in the middle of the study area, as there is no curb, gutter, or swale (between Lindsay Lane and Hillcrest Drive). Due to the flat area and no drainage system, the water has no where to go.
- There are limited left-turn lanes, due to the right-of-way constraints.

- Left-turn lanes are under construction at Hillcrest Drive currently, in association with the Oviedo Square development (fka Chelonian)
- No bus stops within the corridor. LYNX offers a local neighborhood connector by request.
- Signals at study limits (Mitchell Hammock Road and S.R. 426). No additional signals in between. Rapid Rectangular Flashing Beacon (RRFB) with marked crosswalk crossing at Cross Seminole Trail, south of Smith Street.
 - **DISCLAIMER:** Note, the alternatives proposed do not effect the signalized intersections, as the typical section is already 4-lanes.
- Beyond completion of this study, future phases have not been identified. No funding has been identified beyond this study. Results of this study to identify if preferred alternative is identified for future discussions and identification of next steps.
- Info about Engagement (PVT, Coordination with City/County/MPO, etc.)
 - Continuous coordination with City of Oviedo and Seminole County
 - Initial Meeting with City of Oviedo (January 8)
 - Preferred 2-lane, divided option with shared-use path.
 - Community Redevelopment proposed NE corner of 434 and 426
 - PVT Meeting #1 (March 28)
 - Prefer to keep existing speed limit
 - If widening, avoid wider/faster roadway
 - Confirmed queuing at locations where no left turns are present.
 - Desire to keep other modes separate from travel lanes
 - Internal Review Meeting (April 8)
 - PVT Meeting #2 (April 24)
 - Will analyze potential for u-turns and additional needs based on preferred alternative and location of median openings.

Board 3 – Project Location (Assigned – TBD)

- S.R. 434, also known as Central Avenue
- Limits, Mitchell Hammock Road to S.R. 426/C.R. 419/Broadway Street
- 2-lane section, Clonts Street to Smith Street



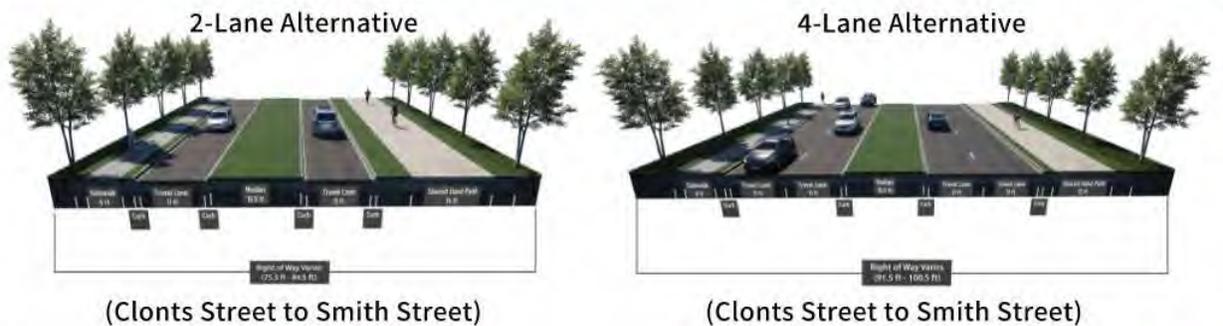
Board 4 – Typical Sections (Assigned – TBD)

- 4-lanes between Mitchell Hammock Road and Clonts Street (to the south) and Smith Street and S.R. 426 (to the north).
- Typical Sections shown are for the 2-lane sections (Clonts Street to Smith Street)
 - Clonts Street to Lindsay Lane includes a center two-way left turn lane
 - 12-foot travel lanes
 - 11-foot two-way left turn lane
 - 4-foot on-street bike lanes
 - 5-6 feet sidewalks, with a landscaped buffer on the west side
 - 65-feet of ROW
 - Lindsay Lane to Smith Street
 - 12-foot travel lanes
 - Left turn lane only being constructed at Hillcrest Drive
 - 4-foot on-street bike lanes
 - 2-foot shoulder, when no curb/gutter is present
 - 5-foot sidewalks, with a landscaped buffer
 - 52.75-feet of ROW
- Proposed Typicals
 - 2-lane, divided
 - 11-foot travel lanes

- 15.5-foot landscaped median
 - with left turn lanes/median openings – TBD
 - Required to meet FDM
 - U-turn aprons and/or use grid network TBD, when median openings determined
- EAST side: 6-foot landscaped buffer, with 14-foot shared use path
- WEST side: keep 6-foot sidewalk, with landscaped buffer
- Curb/gutter throughout corridor. New drainage structures
- 4-lane, divided
 - 11-foot travel lanes (widening to include additional travel lanes)
 - 15.5-foot landscaped median
 - with left turn lanes/median openings – TBD
 - Required to meet FDM
 - EAST side: 2-foot landscaped buffer, with 12-foot shared use path
 - WEST side: 6-foot sidewalk, with landscaped buffer
 - Curb/gutter throughout corridor. New drainage structures



TYPICAL SECTIONS



S.R. 434 Corridor Study
FPID: 962200-4



Board 5 – Proposed Improvements / Renderings (Assigned – TBD)

- Compares a picture of the existing corridor with renderings of the 2 proposed alternatives (2-lane, divided and 4-lane, divided)
- 3 locations
 - (1) At Boardwalk Street / Boston Hill Park
 - Proposed 2LD:
 - Center landscaped median, with NB left turn lane into Boston Hill Park
 - 14-foot shared used path, with 6-foot buffer on east side of the road (left side of the picture)
 - Used to accommodate both cyclists and pedestrians.
 - Proposed 4LD:
 - Center landscaped median, with NB left turn lane into Boston Hill Park
 - 12-foot shared used path, with 2-foot buffer on east side of the road (left side of the picture)
 - Used to accommodate both cyclists and pedestrians.
 - (2) South of Clark Street
 - Proposed 2LD:
 - 15.5-foot center landscaped median (due to no side streets and limited driveway connection, rendering shows no median openings at this location).
 - As you can see in the rendering, the roadway has a western alignment (right side) and would impact the ROW by 13 feet.
 - Proposed 4LD:
 - 15.5-foot Center landscaped median (due to no side streets and limited driveway connection, rendering shows no median openings at this location).
 - As you can see in the rendering, the roadway has a western alignment (right side) and would impact the ROW by 30 feet.
 - (3) South of Smith Street (at the Cross Seminole Trail)
 - BOTH 2LD and 4LD options:
 - Improved Pedestrian Crossing:
 - Pedestrian Hybrid Beacon (PHB) signal
 - Median opening for crossing
 - Pedestrian pavement markings
 - Pedestrian Signal heads
 - Median refuge
 - It was observed and noted by residents that several vehicles were not stopping when the RRFB was flashing. In addition, there were 22 rear-end collisions at this location over the past 5 years.
 - The beacon head consists of two red lenses above a single yellow lens. The lenses remain “dark” until a pedestrian desiring to cross

the street pushes the call button to activate the beacon, which then initiates a yellow to red lighting sequence consisting of flashing and steady lights that directs motorists to slow and come to a stop, and provides the right-of-way to the pedestrian to safely cross the roadway before going dark again.

- This includes 2 sets of signal heads on a mast arm, which provides a visual RED light directly in the driver’s view. Whereas the RRFB is a flashing yellow row of lights, located along the side of the roadway.
- This allows a “safer” pedestrian crossing for those using the Cross Seminole Trail.

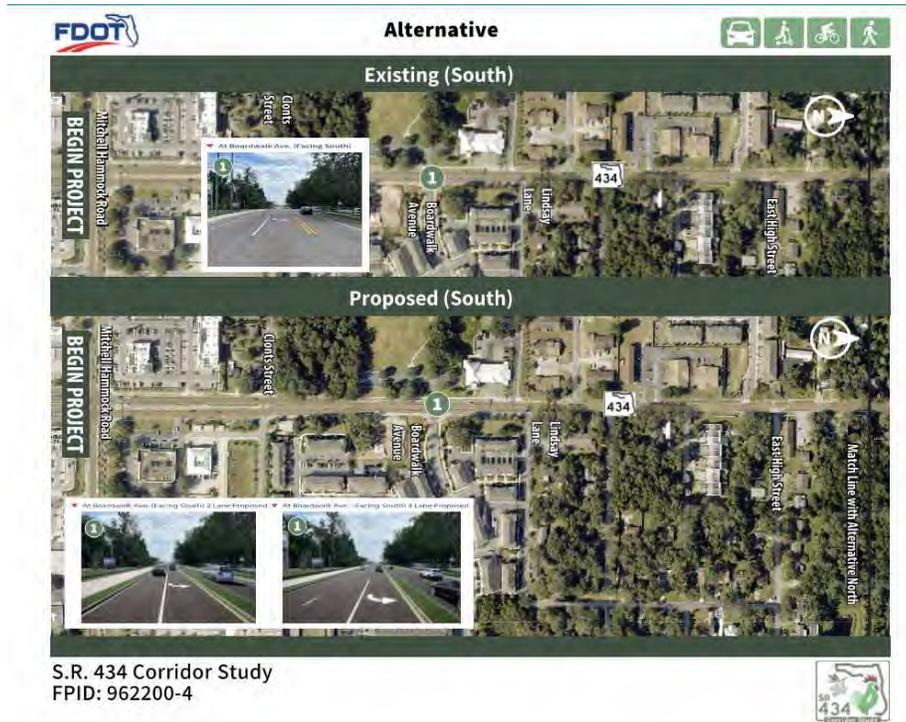
Renderings

	Existing	2-Lane Alternative	4-Lane Alternative
<p>Project Location Map</p>	<p>At Boardwalk Avenue</p>	<p>At Boardwalk Avenue Proposed 2-Lane Alternative</p>	<p>At Boardwalk Avenue Proposed 4-Lane Alternative</p>
	<p>At Clark Street</p>	<p>At Clark Street Proposed 2-Lane Alternative</p>	<p>At Clark Street Proposed 4-Lane Alternative</p>
	<p>At Smith Street</p>	<p>At Smith Street Proposed 2-Lane Alternative</p>	<p>At Smith Street Proposed 4-Lane Alternative</p>

S.R. 434 Corridor Study
FPID: 962200-4

Board 6 – Alternatives - South (Assigned – TBD)

- See notes above (rendering for #1)



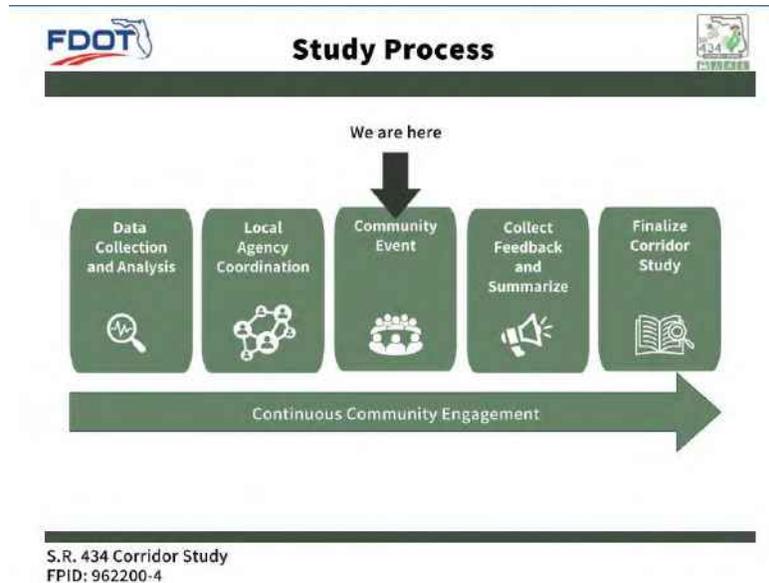
Board 7 – Alternatives - North (Assigned – TBD)

- See notes above (renderings for #2 and #3)



Board 8 – Schedule Board (Assigned – TBD)

- The alternatives presented today are based on data collection, preliminary analysis, and coordination with the local agencies.
- Event today to collect input and identify preferred alternative and identify potential median opening recommendations.
- The corridor study is anticipated to be completed in Fall 2024.
- At this time, funding for any future phases of the corridor study is unknown.



Boards 9 – Comparison Board (Assigned – TBD)

- Compares the existing corridor conditions and the proposed alternatives.
- Difference between alternatives:
 - 2-lane, divided vs. 4-lane, divided
 - 14-foot share-use path vs 12-foot shared use path
 - Both alternatives:
 - Remove on-street bike lane
 - Add curb/gutter
 - Include a 15.5-foot landscaped, center median with left-turn lanes.
- Under existing conditions, left turning vehicles must wait for a gap in opposing traffic either on the travel way or on the two-way left turn lanes. The proposed alternatives provide exclusive left turn lanes within a landscaped median.
 - Due to the amount of closely spaced driveways and off-set intersections, the team wants to obtain input from the public on where potential median openings could be proposed.
 - The intent of the provided cutouts is to graphically show where turn lanes/median openings could be located throughout the corridor and due to the

- FDM requirements, show the public how the required turn lane length could impact how close median openings could be located.
- We want to obtain input from the public on any preference and/or request on potential median opening locations.
 - We will need to further evaluate where the median openings are located; however, want to get as much input as we can, without making any promises.
 - The majority of the existing corridor has no curb and gutter or drainage facilities. The proposed alternatives will improve the drainage with the inclusion of curb and gutter throughout the corridor.
 - The existing trail crossing is controlled by rectangular rapid flashing beacons. Under the proposed alternatives, the trail crossing would be controlled by a pedestrian hybrid beacon.
 - The beacon head consists of two red lenses above a single yellow lens. The lenses remain “dark” until a pedestrian desiring to cross the street pushes the call button to activate the beacon, which then initiates a yellow to red lighting sequence consisting of flashing and steady lights that directs motorists to slow and come to a stop, and provides the right-of-way to the pedestrian to safely cross the roadway before going dark again.
 - This includes 2 sets of signal heads on a mast arm, which provides a visual RED light directly in the driver’s view. Whereas the RRFB is a flashing yellow row of lights, located along the side of the roadway.
 - This allows a “safer” pedestrian crossing for those using the Cross Seminole Trail.
 - Similar improvement included in SR 426 widening project (FPID 415030-6).
 - The corridor currently has narrow on-street bike lanes and sidewalks on both sides of roadway. The proposed alternatives remove the bikes from the road by constructing a shared-use path along the east side of the roadway that would provide mobility for both pedestrians and bicyclists,
 - Right of way availability is limited and the existing features utilize the entire existing right of way. Therefore, any improvement along S.R. 434 in the study area would impact right of way.
 - Additional turn lane being constructed at Hillcrest Drive today, in support of the Oviedo Square development, which has begun construction along the east side of S.R. 434 at Hillcrest Drive.
 - Both proposed alternatives require additional right of way, varying from a maximum of 21 feet for the 2-lane divided alternative to a maximum of 37 feet for the 4-lane divided alternative.

Other Boards

- Welcome
- Title VI
- Thank you

FAQs

- Is ROW needed for these improvements?
 - Yes, both alternatives (or any other improvement) would require ROW.
 - The amount of ROW would vary based on the proposed alignment and/or preferred alternative.
 - The alignment shown is NOT the final alignment. They are showing the potential impacts and needed room to support each alternative.
- Are these improvements funded?
 - No future phase has been determined and/or funded at this time.
 - The intent of today's meeting is to discuss the impacts of each alternative and obtain feedback on which option is preferred, if any, due to the impacts to ROW.
- When will construction start?
 - No future phase has been funded at this time.
 - If either alternative is preferred, a ROW phase would be required. Therefore, construction is not known at this time.
 - Today's meeting will be the first step in determining what is needed as the next steps.
- What are the benefits of having a median in this corridor?
 - Queuing / stacking occurring along S.R. 434 (for example, at Hillcrest Drive, at Clark Street, and at Smith Street)
 - due to the left turn vehicles waiting to turn, with no exclusive turn lane.
 - Vehicles observed to use on-street bike lane to by-pass vehicle waiting to turn left.
 - Median would allow landscaped area for beautification, grass.
 - Median would limit locations where vehicles could make left hand turns (access management).
- Why are the limits of the study what they are?
 - Mitchell Hammock Road to the south
 - S.R. 426 / C.R. 419 / Broadway Street to the north
 - Currently under construction to be widened to 4-lanes (completion anticipated in summer 2024)
 - Due to the limited ROW of this section of S.R. 434, the team will need input from the public regarding the preferred alternative.
 - This section connects the City of Oviedo's "old" downtown (north of S.R. 426 intersection) and the City's "new" downtown (Oviedo on the Park).

- Who has been involved with this project, as part of the Project Visioning Team?
 - o FDOT, District 5
 - o The City of Oviedo (public works, planning, operations, park & rec, police, fire)
 - o City of Oviedo CRA Board
 - o City of Oviedo Chamber of Commerce
 - o Seminole County (public works, planning, operations)
 - o Lynx
 - o MetroPlan Orlando Metropolitan Planning Organization (MPO)
 - o Local HOAs
 - Central Park, Sandalwood, Preserve of Oviedo on the Park, Oviedo Park Terrace
 - o Local schools
 - Seminole County Public Schools, Goddard School of Oviedo

- Will this impact access to the existing business/homes?
 - o Yes, both alternatives would impact businesses and/or homes.
 - o Alignment shown is NOT final.
 - Some locations (i.e. the eastern alignment at the Boston Hill Park) are the only option; however, other locations will be finalized based on design (based on horizontal and vertical curvatures, drainage, etc.) of the roadway.

- What will happen with my comment about a turn lane location?
 - o The team will evaluate median opening locations based on engineering standards while considering public input.
 - o If 2-lane alternative preferred, consideration to u-turn aprons and grid network to be considered as part of evaluation.

Appendix H

Community Event Presentation





Welcome

S.R. 434 Corridor Study

Mitchell Hammock Road to S.R. 426 (C.R. 419)

Financial Project Identification (FPID) No.: 962200-4

Community Event

May 15, 2024



The presentation will begin promptly at 6:00 p.m.



INTRODUCTION

Erika Shellenberger
FDOT Project Manager
719 S. Woodland Blvd., MS 501
DeLand, FL 32720-6834
Erika.Shellenberger@dot.state.fl.us
386-943-5378



ABOUT THIS EVENT



www.CFLRoads.com/project/962200-4

Hybrid Format

- In-Person
- GoToWebinar



TITLE VI COMPLIANCE

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons wishing to express concerns relative to FDOT compliance with Title VI may do so by contacting:

Melissa McKinney
District Five Title VI Coordinator
719 S. Woodland Blvd., MS 501
DeLand, FL 32720-6834
386-943-5077
Melissa.McKinney@dot.state.fl.us

Stefan Kulakowski
State Title VI Coordinator
605 Suwannee St., MS 65
Tallahassee, FL 32399-0450
850-414-4742
Stefan.Kulakowski@dot.state.fl.us

All inquiries will be handled in a prompt and courteous manner.



PUBLIC NOTICE



Florida Administrative Register



FDOT/project website



Newspaper



Hand deliver



Email to project contacts list

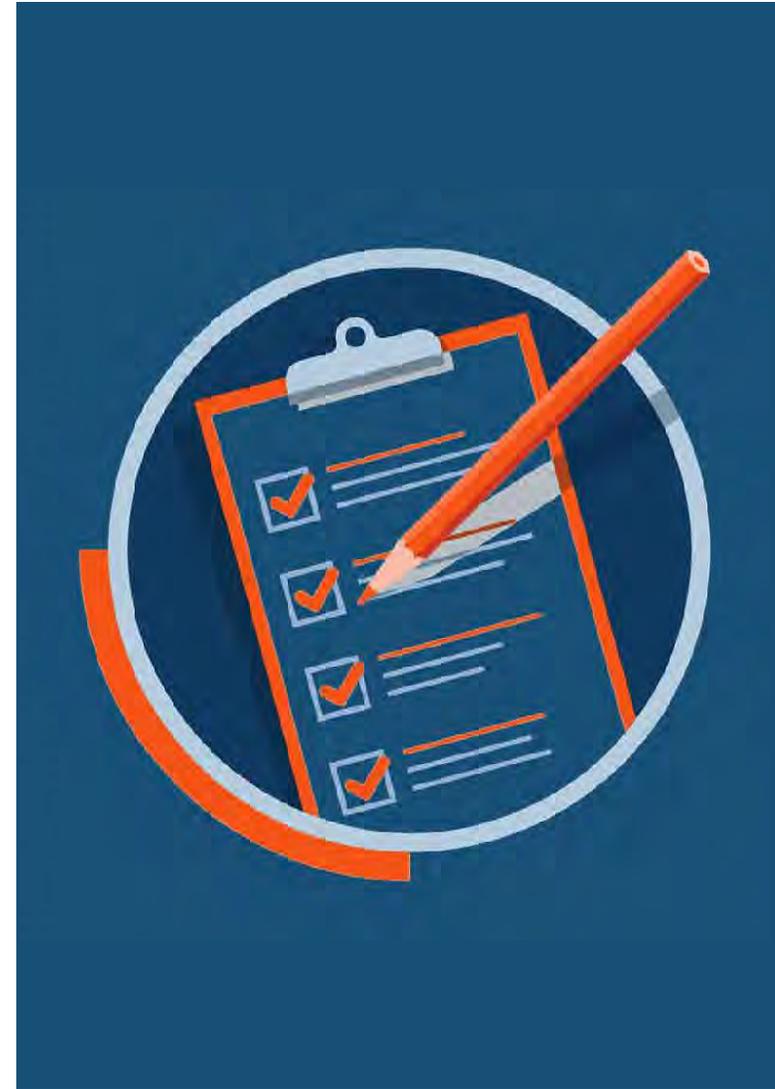


Social media

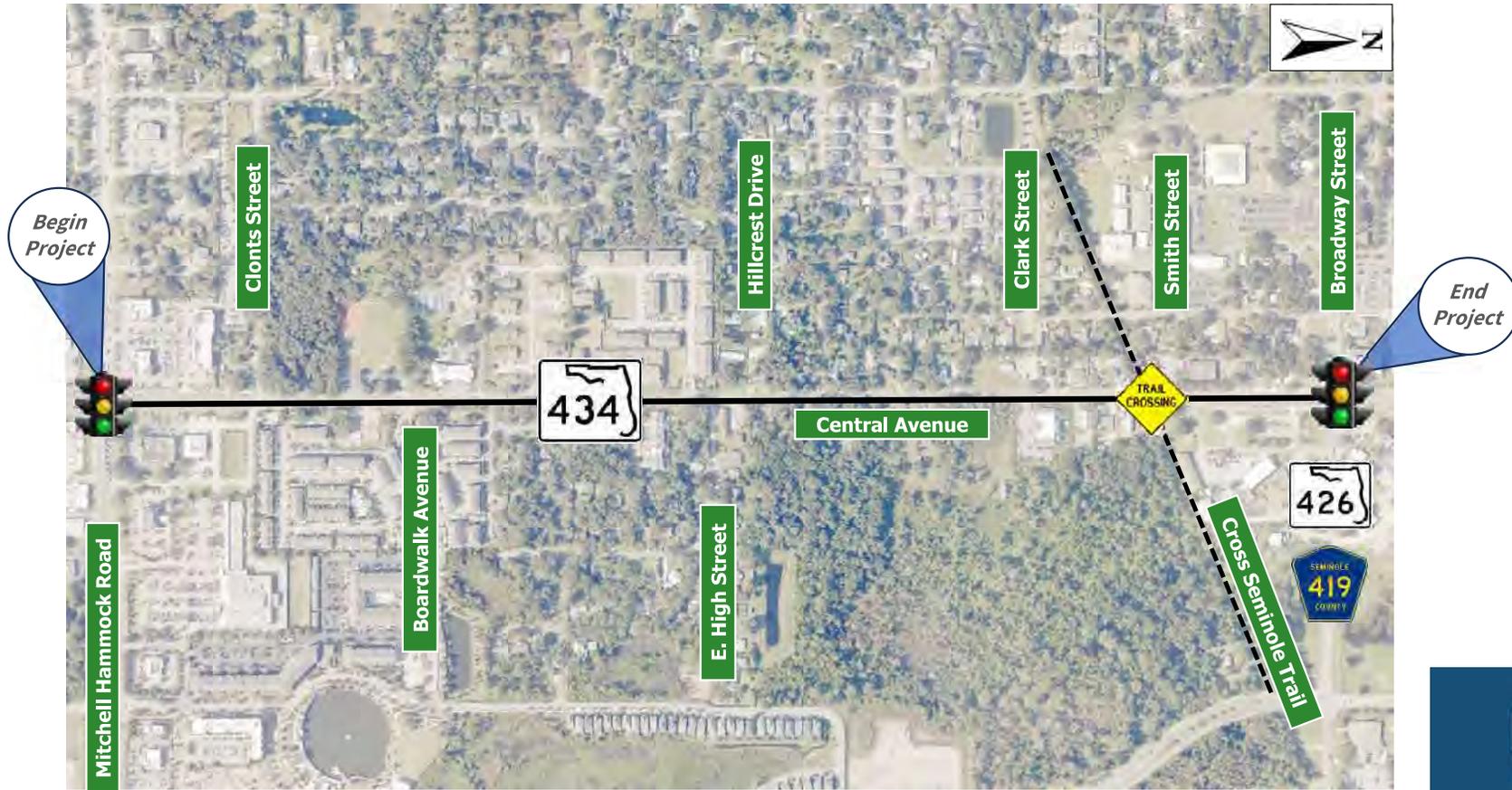


AGENDA

- Explain the project goals
- Present proposed improvements
- Obtain your feedback

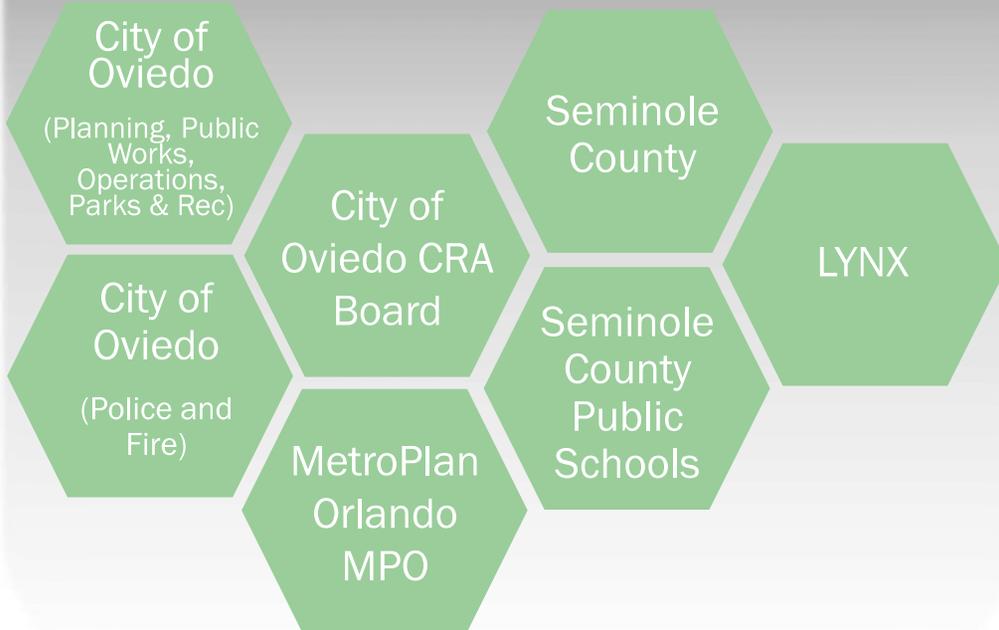


CORRIDOR STUDY LIMITS



STUDY PARTNERS

Government Partners



Community Partners



STUDY GOALS



Safety



Speed
Management



Pedestrian &
Bicycle Mobility

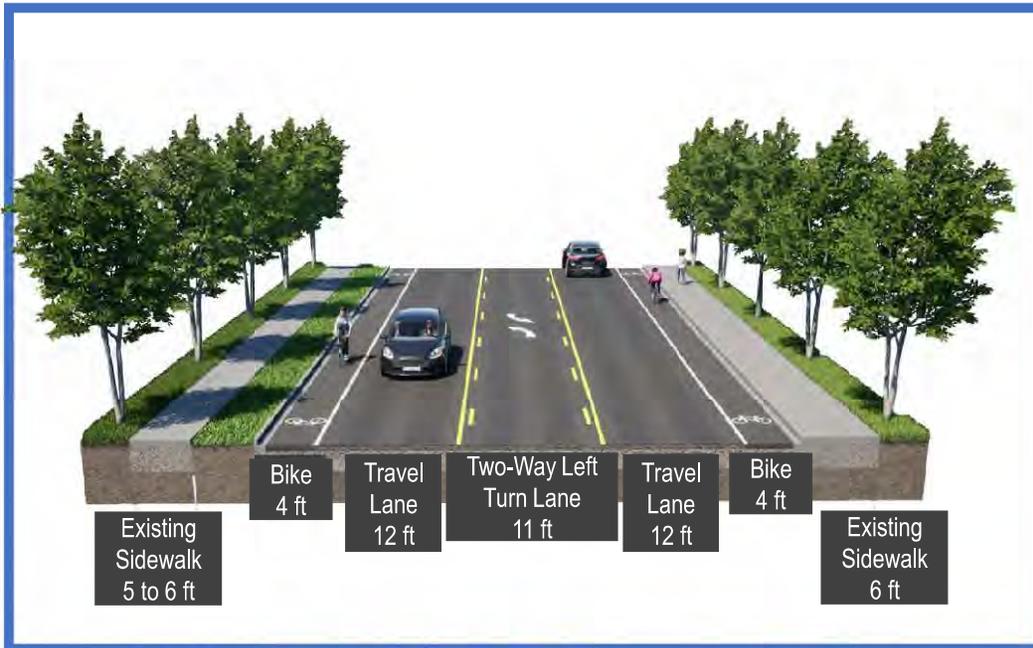


Data-Driven

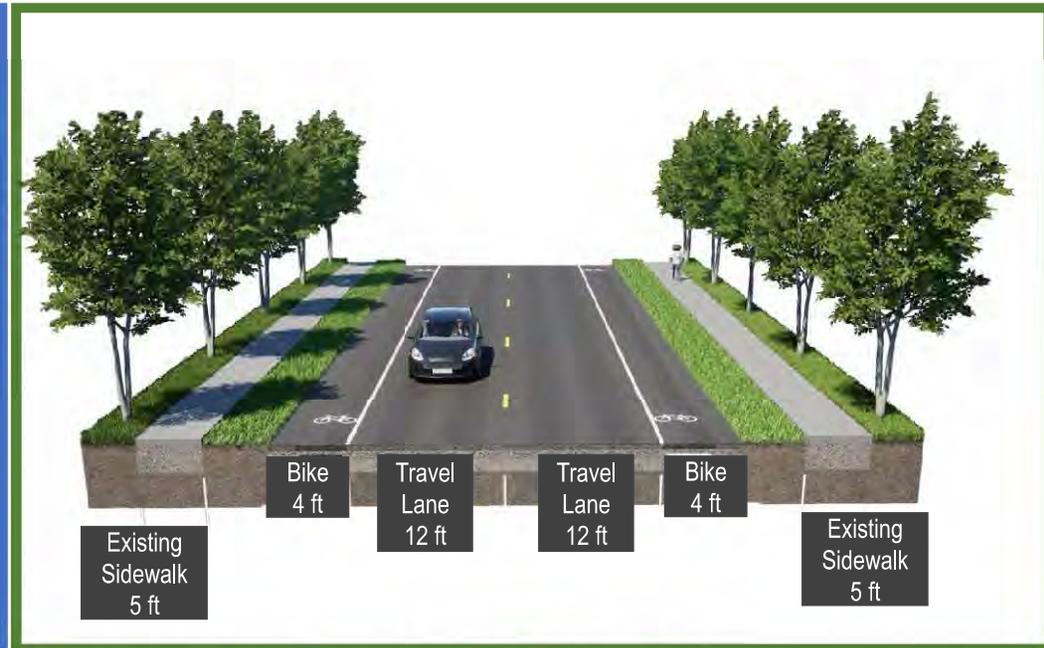
- Understand and balance surrounding land uses
- Determine best way to meet current and future needs of roadway users
- Evaluate impacts

EXISTING TYPICAL SECTION

Clonts Street to North of Lindsay Lane



North of Lindsay Lane to Smith Street



EXISTING CONDITIONS



Limited Exclusive Turn Lanes



Right of Way



Drainage



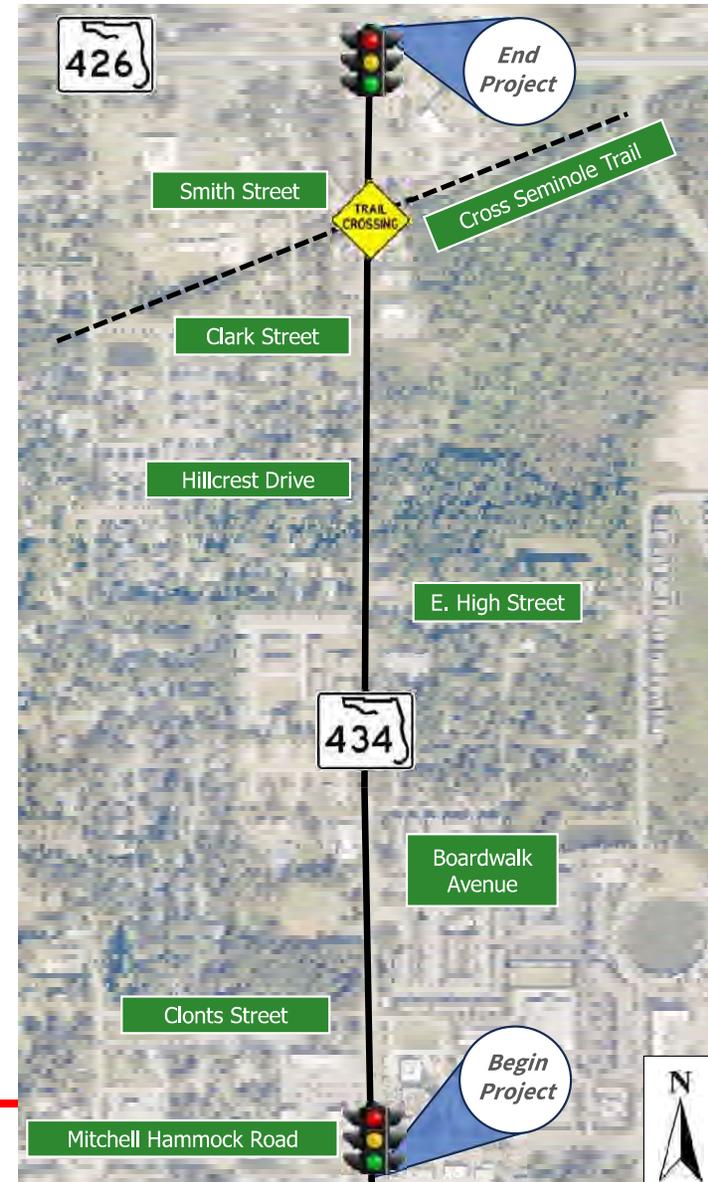
On-shoulder Bicycle Lanes



Lack of Pedestrian Crossings



Trail Crossing Operations



Proposed Alternatives



2-LANE, DIVIDED TYPICAL SECTION



4-LANE, DIVIDED TYPICAL SECTION



PROPOSED RENDERINGS – BOARDWALK AVENUE

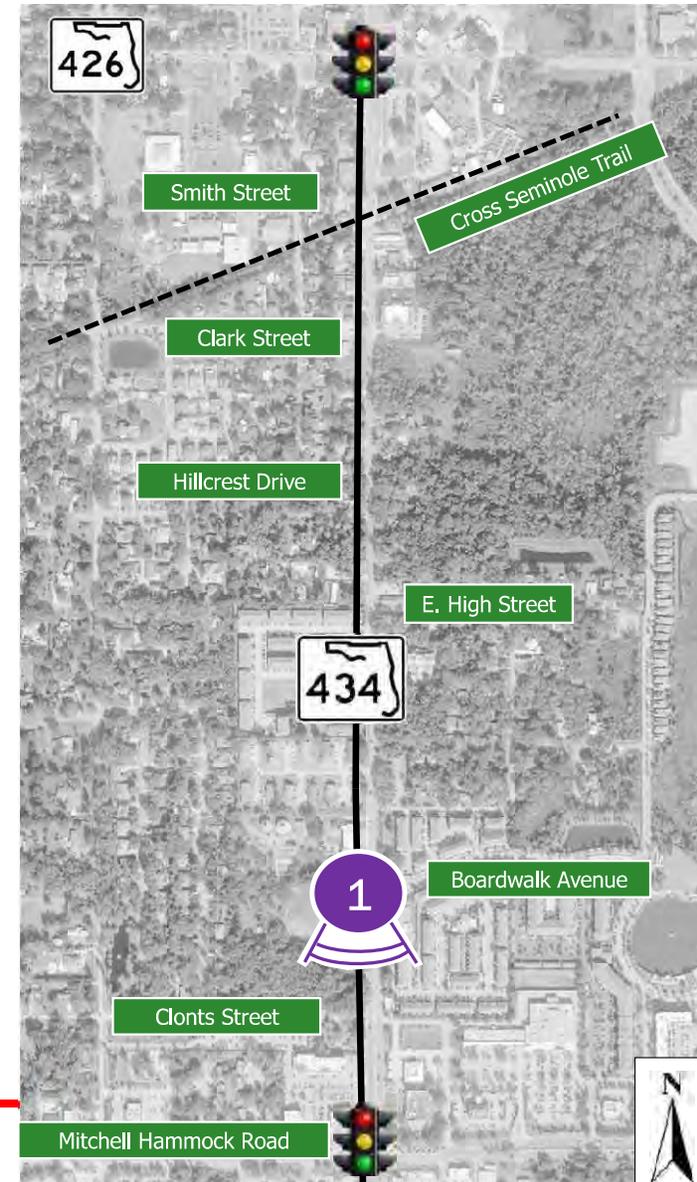
2-lane, Divided



4-lane, Divided



FPID No. 962200-4



PROPOSED RENDERINGS – SOUTH OF CLARK STREET

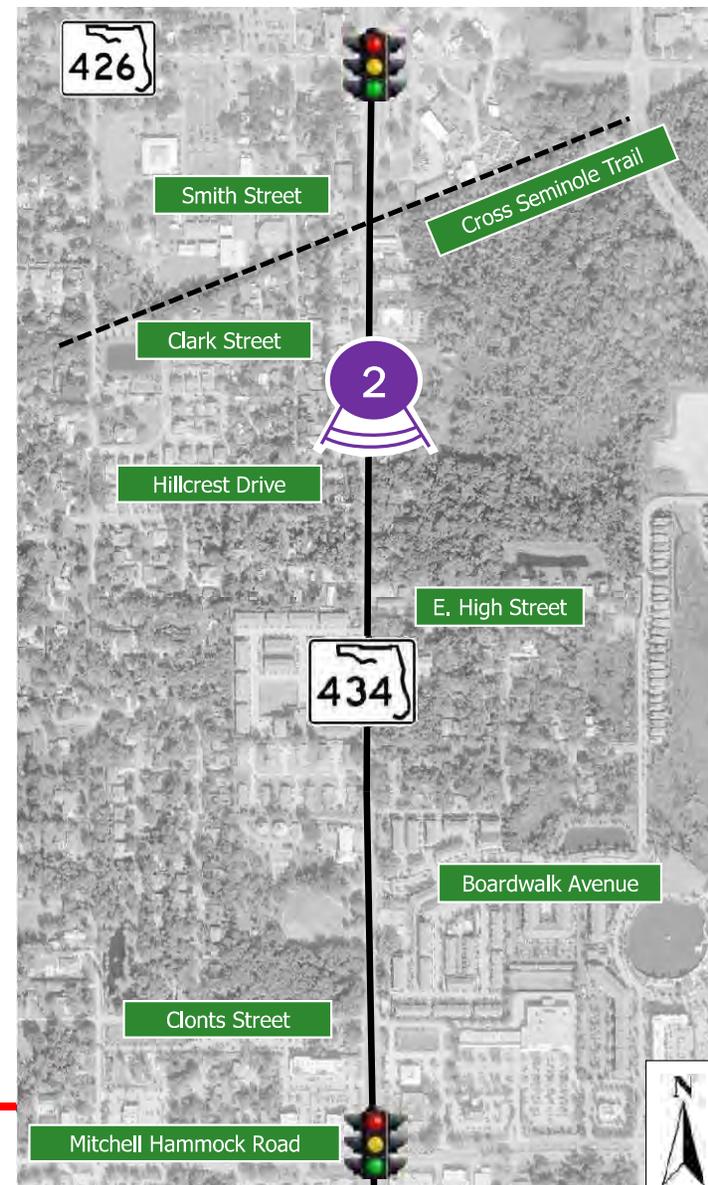
2-lane, Divided



4-lane, Divided



FPID No. 962200-4



PEDESTRIAN HYBRID BEACON (PHB)



EXISTING



PROPOSED



COMPARISON



Existing



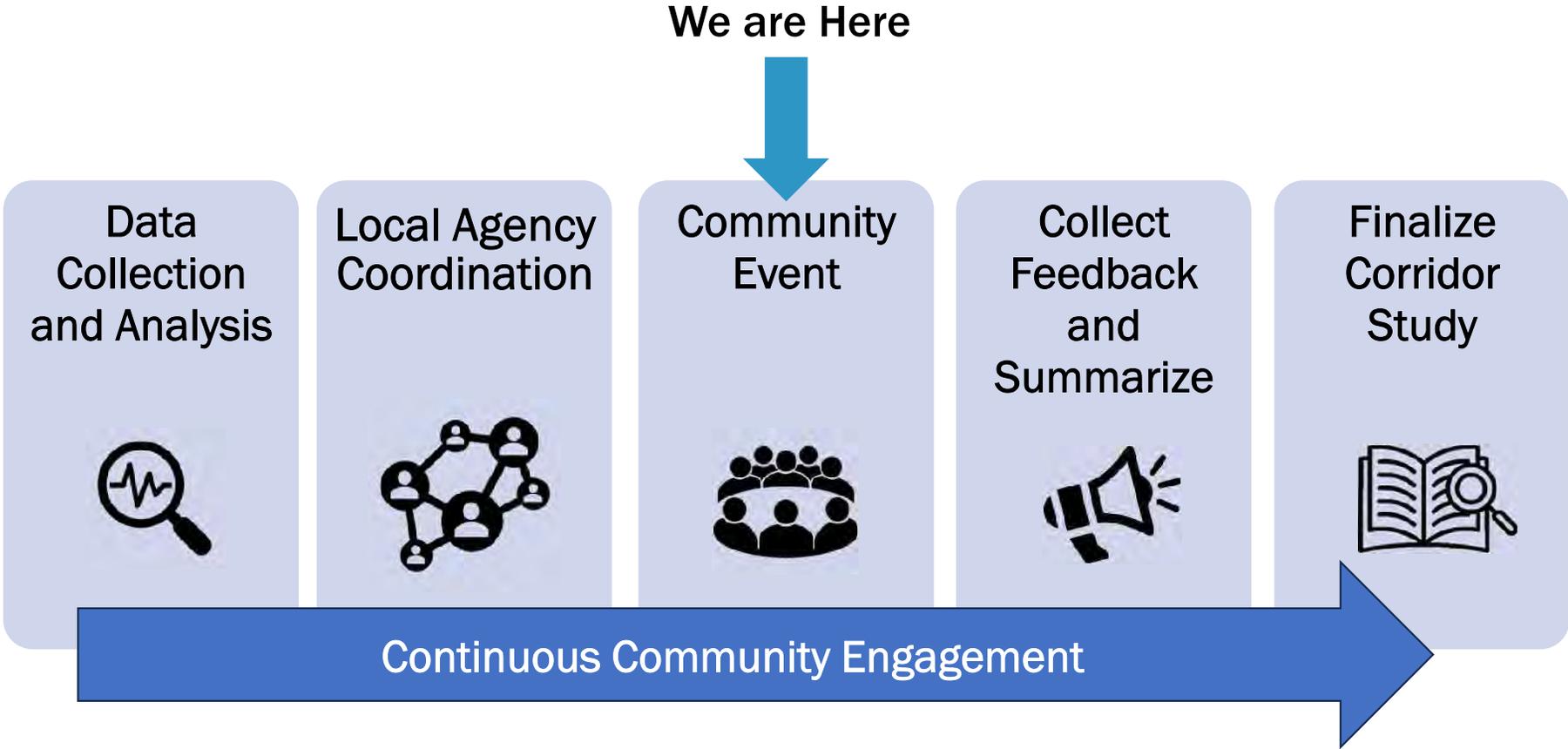
2-lane, Divided



4-lane, Divided

Left Turn from Travel Way or Two-Way Turn Lane	<input checked="" type="checkbox"/>		
Provides Exclusive Left Turn Lane		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Landscaped Median		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Extends Curb & Gutter		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Flashing Beacon at Trail Crossing	<input checked="" type="checkbox"/>		
Pedestrian Hybrid Beacon at Trail Crossing		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
On-Street Bike Facility	<input checked="" type="checkbox"/>		
Off-Street Bicycle and Pedestrian Facilities		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Additional Right of Way Need	None	Maximum of 24 Feet	Maximum of 37 Feet

SCHEDULE AND NEXT STEPS





STAY INVOLVED



In Person



Virtually



Email or Mail



Telephone



Speak with project team members

Submit a written comment for the project record



CONTACT THE PROJECT MANAGER



Erika.Shellenberger@dot.state.fl.us



719 S. Woodland Blvd., MS 501
DeLand, FL 32720-6834



386-943-5378



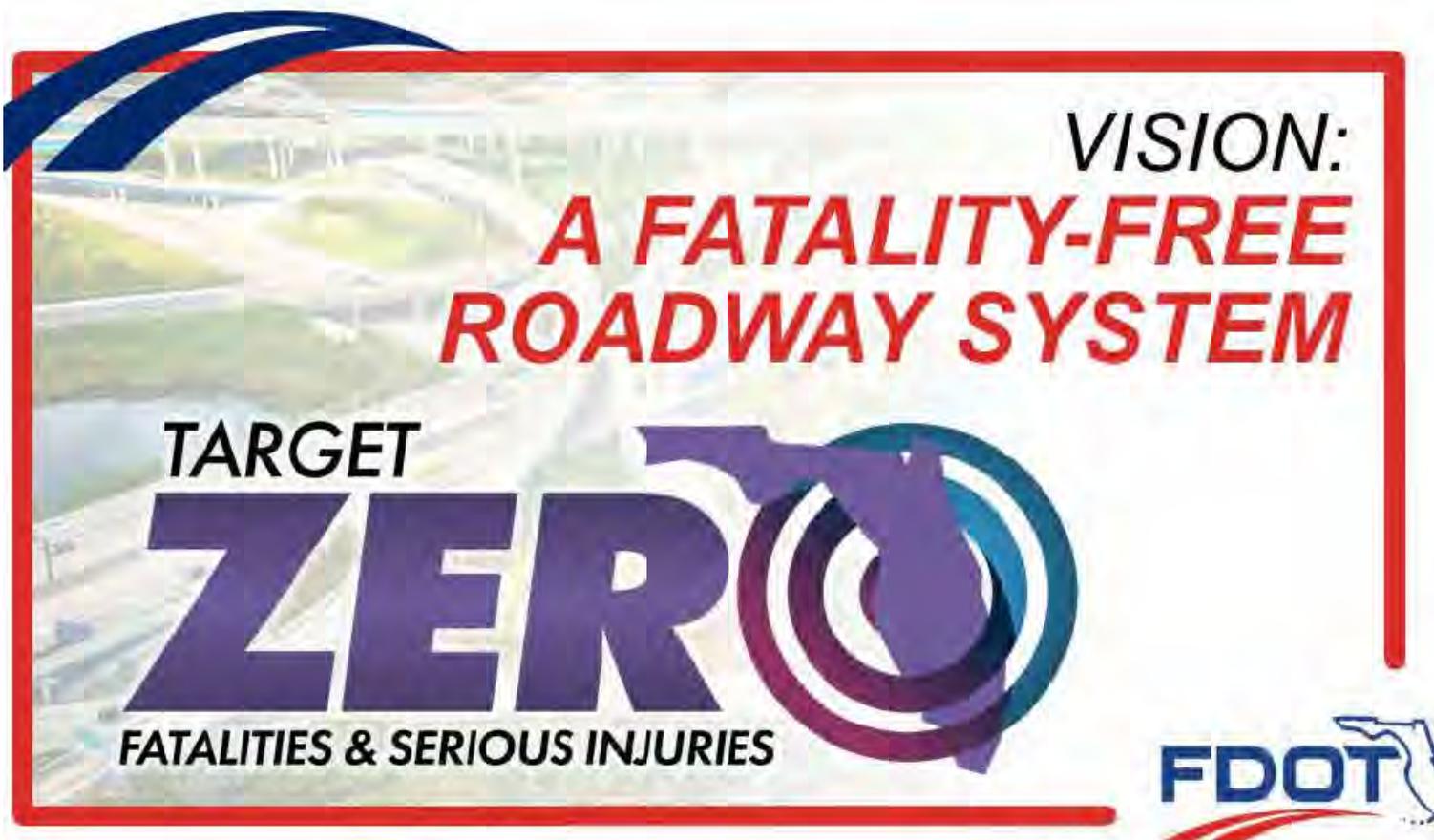
STAY INFORMED



The screenshot shows the FDOT website interface. At the top, there is a navigation menu with links for 'CFLRoads Home', 'Lane Closures', 'Construction Projects', 'Future Projects', 'News', and 'Resources'. A search bar labeled 'Search CFLRoads' with a 'Go' button is on the right. Below the menu, a welcome message reads 'Welcome to FDOT's District Five Projects Website'. A large image of a roundabout is featured, with a text box below it titled 'Roundabouts 101: Learn about the benefits and proper use of roundabouts'. To the right of the roundabout image is a map of District Five counties: Marion, Flagler, Volusia, Lake, Seminole, Orange, Osceola, and Brevard. An orange callout bubble points to the search bar with the text 'Type the project number (962200-4) here'. Another orange callout bubble points to the 'Go' button with the text 'Click "Go"'.

www.CFLRoads.com





THANK YOU FOR PARTICIPATING

Please submit any questions or comment by May 25, 2024

THANK YOU

This concludes the presentation.

We invite you to view the event materials and exhibits and talk to the project team.



Appendix I

Community Event Boards and Roll Plots



PROJECT LOCATION MAP



S.R. 434 Corridor Study
FPID: 962200-4

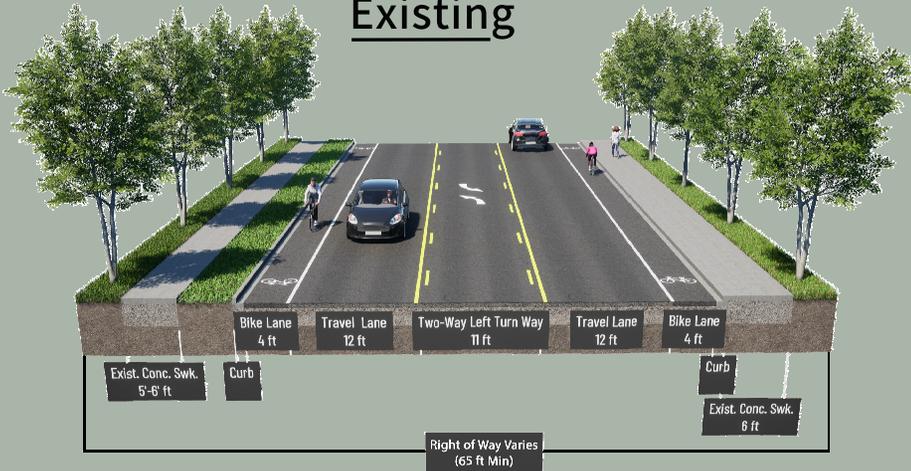


We are here



TYPICAL SECTIONS

Existing



(Clonts Street to north of Lindsay Lane)

Existing



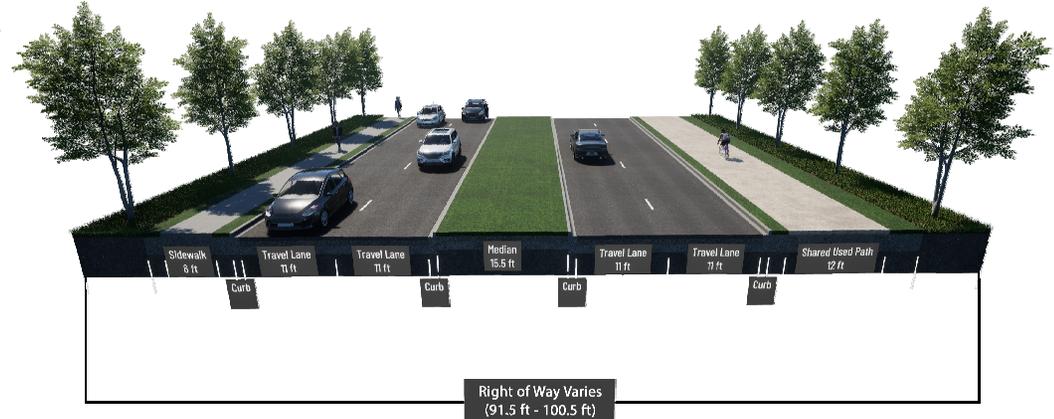
(north of Lindsay Lane to Smith Street)

2-Lane Alternative



(Clonts Street to Smith Street)

4-Lane Alternative



(Clonts Street to Smith Street)



Existing



2-Lane, Divided



4-Lane, Divided

	Existing	2-Lane, Divided	4-Lane, Divided
Left Turn from Travel Way or Two-Way Turn Lane	<input checked="" type="checkbox"/>		
Provides Exclusive Left-Turn Lane		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Landscape Median		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Extends Curb & Gutter		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Flashing Beacon at Trail Crossing	<input checked="" type="checkbox"/>		
Pedestrian Hybrid Beacon at Trail Crossing		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
On-Street Bike Lane Facility	<input checked="" type="checkbox"/>		
Off-Street Bicycle and Pedestrian Facilities		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Additional Right Of Way Need	None	Maximum of 24 Feet	Maximum of 37 Feet

Existing

2-Lane Alternative

4-Lane Alternative

Project Location Map



At Boardwalk Avenue



At Boardwalk Avenue
Proposed 2-Lane Alternative



At Boardwalk Avenue
Proposed 4-Lane Alternative



At Clark Street



At Clark Street
Proposed 2-Lane Alternative



At Clark Street
Proposed 4-Lane Alternative



At Smith Street



At Smith Street
Proposed 2-Lane Alternative



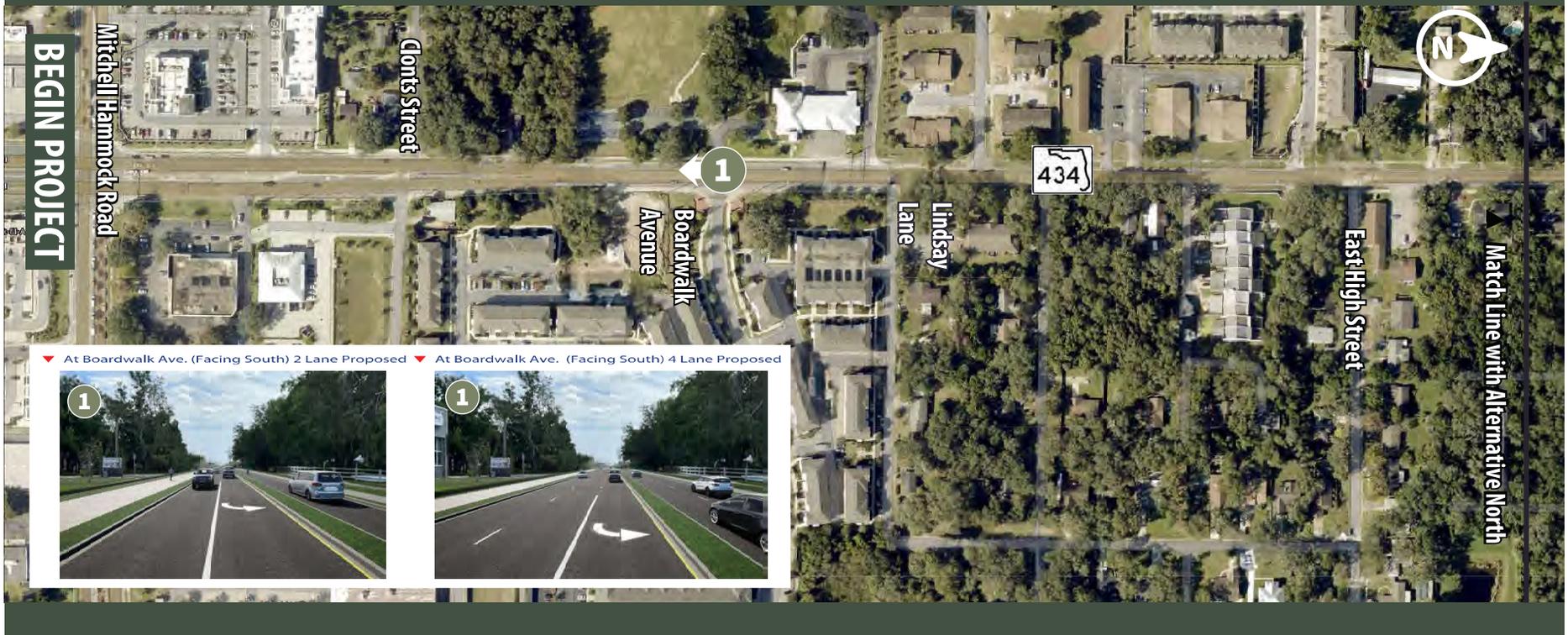
At Smith Street
Proposed 4-Lane Alternative



Existing (South)



Proposed (South)



Existing (North)



Proposed (North)



4-Lane, Divided Alternative



Existing



Proposed



LEGEND

- EXISTING RIGHT OF WAY
- PROPOSED RIGHT OF WAY
- PROPOSED ROADWAY IMPROVEMENTS
- PROPOSED SHARED USER ZONE/BIKEWAY



Appendix J

Community Event Attendee Summary



Name	Address	Subdivision / Business / Agency	Email	Representing
Paul Yeargain	400 Alexandria Blvd, Oviedo	City of Oviedo	pyeargain@cityofoviedo.net	Agency
Arturo Perez	100 E. 1st Street, Sanford	Seminole County	aperez@seminolecountyfl.gov	Agency
Taylor Laurent		MetroPlan Orlando MPO	Taylor.Laurent@MetroPlanOrlando.gov	Agency
Bryan Cobb	400 Alexandria Blvd, Oviedo	City of Oviedo	bcobb@cityofoviedo.net	Agency
Megan Sladek	6007 Lake Charm Circle, Oviedo	Gwynn's Survey Of Lake Charm	megansladek@gmail.com	Agency/Public
Matthew Richardson		FDOT	matthew.richardson@dot.state.fl.us	FDOT
Michele Verner		FDOT	michele.verner@dot.state.fl.us	FDOT
Jack Adkins		FDOT	jack.adkins@dot.state.fl.us	FDOT
Dean Tasman	784 S. Central Avenue, Oviedo	Pharmacy on the Park	cecgrouppinc@aol.com	Public
Lori Hood	1063 Abell Circle, Oviedo	Alafaya Woods	LoriHood407@gmail.com	Public
Derek Cattin	561 Piazza Point, Oviedo	Oviedo Park Terrace	walstib1961@gmail.com	Public
J Nurk	Stonebriar Lane, Stanford	---	parlay25@aol.com	Public
Sandra Erny	473 S Central Avenue, Oviedo	n/a	sderney@aol.com	Public
Dennis Erney	473 S Central Avenue, Oviedo	n/a	sderney@aol.com	Public
Mark Tillman	17 Lawn Street, Oviedo	n/a	jmtill23@gmail.com	Public
Kathie Smith	2476 Mills Creek Road, Chuluota	Lake Pickett Estates	KathieSmithARNP@hotmail.com	Public
Sam Nasser	773 S. Lake Jessup Avenue, Oviedo	n/a	sam@bstncapital.com	Public
Jake and Tracy Vigliotti	90 E High Street, Oviedo	Oviedo Heights		Public
Alan Ashlock	200 S. Central Avenue, Oviedo	Cotton ways Retail	alan@ashlockdecatour.com	Public
Marien Lugo	530 La June Avenue, Oviedo	Oviedo Heights	marienlugo402@gmail.com	Public
Kylie Casazza	150 Boardwalk Avenue Apt 311, Oviedo	Oviedo on the Park	KylieCarazza@rocketmail.com	Public
Pam Knapp	788 Rich Drive, Oviedo	Richfield	pamisioux2@aol.com	Public
Lawrence Ewaldt	185 S. Central Avenue, Oviedo	TSA, Inc.	Lewaldt@TSAFL.com	Public
Barbara Barbour	3 Lawn Street, Oviedo	n/a	burd51@aol.com	Public
Marianne O'Brien	1013 Willa Lake Circle, Oviedo	Willa Lake	marianne.s.obrien@gmail.com	Public
Ken Ray	1006 Quaker Ridge Court, Oviedo	Twin Rivers	chidracy@gmail.com	Public
Pearl Sullivan	41 E. High Street, Oviedo	Church of God of Prophecy	redirected2glory@gmail.com	Public
Bill Graf	2476 Mills Creek Road, Chuluota	Lake Pickett Estates	wagraf86@gmail.com	Public
Jeff Boddiford	3504 Wild Eagle Run, Oviedo	Live Oak Reserve	jboddiford@csc.rr.com	Public
Alex Moore	28 Hillcrest Drive, Oviedo	n/a	moorea08@gmail.com	Public
Aaron Shafer	13 Lawn Street, Oviedo	n/a	aaron.shafer@msn.com	Public
Alex Fiset	150 Boardwalk Avenue Apt 311, Oviedo	Oviedo on the Park	afiset@bellsouth.net	Public

LEGEND
Agency
Residence
Business
unable to confirm

REGISTERED AND ATTENDED

First Name	Last Name	Email	Representing
Kelly	Patrick	pkelly@cityofoviedo.net	Agency
Nelson	Tony	anelson@seminolecountyfl.gov	Agency
Bauer	Ronald	rbauer@cfl.rr.com	Public
Calderella	James	jcalderella@gmail.com	Public
Dalusung	Omar	odalusung@ennov8dg.com	Public
Green	Aguya	ad87green.ag@gmail.com	Public
Huth	Robert	huthr477@gmail.com	Public
Kesler	Robin	rkesler@mac.com	Public
Quinones	Maria	mlq9093@gmail.com	Public
Rashedi	Kristina	krashedi_32765@yahoo.com	Public
Smith	Lynne	cloudrock@bellsouth.net	Public
Zambrano-Marin	Luisa Fernanda	rojolufechristian@gmail.com	Public

REGISTERED but did NOT Attend

First Name	Last Name	Email	Notes
Gifford	Elizabeth	elizabeth.gifford@dot.state.fl.us	Agency
Ashlock	Alan	alan@ashlockdecalur.com	Attended In Person
Lugo	Marien	marienlugo402@gmail.com	Attended In Person
Moore	Alex	moorea08@gmail.com	Attended In Person
Tasman	Dean	cecgrouppinc@aol.com	Attended In Person
Agnew	Janan	jagnew@hotmail.com	
Benson	Tyler	tylerbenson@gmail.com	
Caceres	Tanya	tanya_caceres@hotmail.com	
Caro	Kenneth	kenneth.caro68@gmail.com	
Dziedzic	Bill	NIKABILL@COMCAST.NET	
Gillen	Diane	boozieding@yahoo.com	
Goldstein	Joel	joel@joelgoldstein.com	
Hopkins	Patty	hopkinspatriciar@gmail.com	
Houston	Kylie	kyliehouston4@yahoo.com	
Janisz	Craig	cjanisz@hotmail.com	
Johnson	Randall	randyjpe@gmail.com	
Keller	Richard	kellerr@bellsouth.net	
Montesdeoca	Xavier	montexav@gmail.com	
Oneal	Jason	jasononeal.email@gmail.com	
Ray	Lorraine	lorraineraya@mailfoundery.com	
STEINER	MARGARET	funhouses@gmail.com	
Stokes	Megan	contact@oviedocommunitynews.org	
Trinh	Truong	ttrinh@ennov8dg.com	
Wahrenberger	Brian	brianwahrenberger@yahoo.com	
Whipple	Richard	whiplerichard@yahoo.com	

Agency - City of Oviedo	3
Agency - Seminole County	1
Agency - MetroPlan Orlando MPO	1
FDOT	3
Public	24
	32

Agency - City of Oviedo	1
Agency - Seminole County	1
Public	10
	12

Team Members (in-person)	
Erika Shellenberger	FDOT
Joseph Fontanelli	FDOT
Todd Davis	Consultant
Alex Hinkle	Consultant
Karen Van den Avont	Consultant
Nicole Henry	Consultant
Brittany Nesbitt	Consultant
Jhoanna Pantoja	Consultant
Sonya Howard	Consultant
Melissa Gross	Consultant

Team Members (virtual)	
Carolyn Fitzwilliam	FDOT
Mario Texeira	Consultant

Appendix K

Summary of Community Event Comments Received



Attended Event?	Name	Address	Phone	Resident / Business	Email	Comment Received From / Platform	Comment / Question	Date Received (if not during Event)
No	Ian Tasman	784 S. Central Avenue, Oviedo, FL 32765	407-733-0494	Pharmacy on the Park	wwp14@gmail.com	Email	<p>It was nice getting to speak to you today. We are the owners of Pharmacy on the Park and the adjoining lot at 784 S Central Ave, Oviedo, FL 32765.</p> <p>During construction and development of our property we paid for engineering studies to confirm and finally approve the viability of a turning lane into our properties, something we view as essential to our operational success.</p> <p>Please keep this in mind and on file.</p> <p>Best, Ian Tasman</p>	4/26/2024
Yes	Lawrence Ewaldt	185 S. Central Avenue, Oviedo, FL 32765	407-365-2233	TSA, Inc.	Lewaldt@TSAFL.com	In-person Meeting	<p>434 needs to be 4 laned. Even with widening and turn lanes, it cannot handle the potential traffic after the 5-story apartments are built at 419/426 and 434.</p> <p>Turn lanes may alleviate one direction of traffic, but not both.</p>	
Yes	Lawrence Ewaldt	185 S. Central Avenue, Oviedo, FL 32765	407-365-2233 (w) 407-902-1194 (cell)	TSA, Inc.	Lewaldt@TSAFL.com	Email	<p>Erika,</p> <p>Good morning, congratulations on your event yesterday.</p> <p>Since these proposals will affect me the most and my business as I am 15ft away from 434 presently. If you could please save my info and let me know how this progresses, that would be perfect.</p> <p>Thanks.</p> <p>Lawrence Ewaldt Technical Sales & Applications, Inc.</p>	5/16/2024
---	n/a					In-person Meeting	If not already, look into pedestrian crossing options in the area by Boston Hill Park. Many pedestrians already attempt to make this crossing, even during busy peak hours.	
---	n/a					In-person Meeting	<p>Please better implement the merge that occurs northbound when turning from Mitchell Hammock, if 2-lanes are chosen.</p> <p>Please consider a PHB at Boardwalk Avenue connecting to Boston Hill Park. May be able to utilize the emergency signal at the fire station. More physical separation between SUP and roadway.</p>	
Yes	Megan Sladek			City of Oviedo/Resident		In-person Meeting	Any design that encourages people to exit 417 to save 25¢ and have Oviedo serve as a cut-through should be avoided. 2-lane max - I bike here all the time.	

Attended Event?	Name	Address	Phone	Resident / Business	Email	Comment Received From / Platform	Comment / Question	Date Received (if not during Event)
Yes	Alan Ashlock	200 S. Central Avenue, Oviedo, FL 32765	407-808-9413	Cotton ways Retail	alan@ashlockdecalur.com	In-person Meeting	We have a retail business at this location. Business name is "Cotton ways" so whatever you do, we need a median cut so our patrons can go N or S.	
Yes	Pearl or Van Sullivan	496 S. Central Avenue, Oviedo, FL 32765	321-245-3224 407-221-7085	Church of God of Prophecy	redirected2glory@gmail.com	In-person Meeting	I was informed that places of worship fall under a different set of rules when it comes to losing your property. How can I find out what our rights are?	
Yes	Jake Vigliotti	90 E High Street, Oviedo, FL 32765	407-694-5070	Resident		In-person Meeting	If you turn 434 into a 4-lane road, cars will travel in excess of 60 mph and it will only be a matter of time when someone is killed at the Smith Street walkway (former RR tracks). Alt: No change, light at Clark Street Alt +1: Pedestrian flyover at Seminole Trail	

Attended Event?	Name	Address	Phone	Resident / Business	Email	Comment Received From / Platform	Comment / Question	Date Received (if not during Event)
Yes	Sandra and Dennis Erny	473 S Central Avenue, Oviedo, FL 32765	407-399-5883	Resident	sderney@aol.com	In-person Meeting	(1) keep 2-lane (2) add turn lanes by Hillcrest, Clark, Wood (3) crosswalk by High Street. There is a bus stop there, plus everyday high school kids cross there on their way home from school.	
Yes	Barbara Barbour	3 Lawn Street, Oviedo, FL 32765	407-782-0256	n/a	hurd51@aol.com	In-person Meeting	<p>Something definitely needs to be done. I also own the "Graceland Square" townhome wall and end grass parcel. The parcel has a well and the irrigation system for the entire townhouse unit. These are important to the units so that potable water is not used for irrigation. We are on septic in the rear of the units and would like for sewer to be installed as the current code requires us to obtain a variance to repair our current systems. Only one company is willing to go through the variance process. If the "wall" is taken for 3 lanes I would request a low wall with iron fencing between the sidewalk and end unit.</p> <p>If the end unit is taken I would then own the end unit and would want the wall doubled and finished. There are numerous accidents between Wood and High Street and at Clark Street. At best there needs to be turn lanes.</p> <p>Power poles are too close to the current street and in the new Pulte compiles some are in the sidewalk.</p> <p>Crosswalks are also desperately needed and possible a lower speed limits.</p>	
Yes	Alex and Hillary Moore	28 Hillcrest Drive, Oviedo, FL 32765	(904) 687-9736	Resident	moorea08@gmail.com	Email	<p>Hi Erica, my wife and I own the property at 28 Hillcrest Dr, Oviedo 32765. We have reviewed the proposals regarding the 434 Corridor study and have questions regarding the project area and it's impact in regards to the proposed RoW widening.</p> <p>Are there any current plans to alter the Hillcrest Dr / 434 intersection? As you may know, we have recently had this intersection widened to accommodate what appears to be a center turn lane for the new Pulte development. Our concern is further widening would almost certainly result in an eminent domain taking from our property. Our home and our neighbor's home predates the founding of Oviedo, so the current setbacks from 434 are already quite narrow.</p> <p>Happy to discuss this via phone or in person tomorrow, if you're planning to attend the community meeting.</p>	5/14/2024
Yes	Alex and Hillary Moore	28 Hillcrest Drive, Oviedo, FL 32765	(904) 687-9736	Resident	moorea08@gmail.com	Email	<p>Hi Erica, no worries at all. My formal public comment is below the break. Please confirm receipt and submission when able.</p> <p>After attending the informational meeting and speaking with FDOT designers, we vigorously oppose any proposal that results in a Right of Way extension. In spaces where existing Right of Way can be maintained, a center turn lane as at the intersection of Central and Hillcrest would be appropriate.</p> <p>Central Ave / 434 southbound traffic is due to inefficient and undersized capacity at the Mitchell Hammock intersection. The accidents are due to drivers who are traveling too fast and unprepared to stop. Adding capacity or turn lanes does not solve this issue. Reducing the overall speed on Central or placing speed tables would be a better solution.</p>	5/24/2024

Attended Event?	Name	Address	Phone	Resident / Business	Email	Comment Received From / Platform	Comment / Question	Date Received (if not during Event)
Yes (Virtual)	Luisa Fernanda		787-310-5165	Resident	mjplufchristian@gmail.com	GoTo Webinar	<p>Good Morning Ms. Carolyn!</p> <p>Thank you so much for your message.</p> <p>My question is regarding crosswalks and school bus stops.</p> <p>At the intersection of Central ave with Boardwalk ave (across the park, by the firehouse) there is a school bus stop that about 30 students from the community use. It would be extremely useful to have a cross walk since the students board the bus on the park side in the mornings. It would also help improve the access to the park.</p> <p>A plus would be to have a canopy or something to provide shade/shelter from rain on that corner! It's very hot when we wait in the afternoon for the kids' arrival.</p> <p>The idea on the map :)</p> <p>Thanks so much!</p>	5/16/2024
Yes (Virtual)	Bob Huth		407-365-3915	Resident	huthr477@gmail.com	Email	<p>Ms. Fitzwilliam,</p> <p>Thank you for the opportunity to comment. My wife and I have lived in Oviedo since 1993 and the steady increase in traffic has grown horrific over these years.</p> <p>The four-lane alternative is the only logical approach to resolving the traffic load in Oviedo so we are in agreement with the plan for this alternative.. Although not mentioned in this study, Central Blvd should also be expanded to four lanes from Broadway St to where the road becomes four lane again near the Toll Way 417 overpass, Then SR 434 will be at least four lanes through Oviedo. Stop lights should be installed at the corner of Central Blvd and Artesia St. rather than a traffic circle which is being considered now. When one of the intersecting roads has heavy traffic, such as SR 434 does, and the side street has much lighter traffic, a circle will not work well because the busy road will dominate the traffic pattern and the side street will have a very difficult time trying to merge in. A traffic circle is okay when both intersecting roads do not have real heavy traffic</p> <p>Thank you in advance for allowing me to comment on the current study and that I hope you will also consider the alternatives for SR 434 that I presented beyond the scope of the current study..</p> <p>Regards,</p>	5/17/2024
No	Beatriz Ortiz		786•385•3602	Resident	ocp.hoa.president@gmail.com	Email	<p>Hello,</p> <p>One of the board members of Oviedo Central Park HOA forwarded your presentation to me and pointed out how our HOA is listed as a community partner. I have reached out to our property manager and he nor I have been in contact with anyone from your office.</p> <p>I am inquiring about why we are listed as a community partner and what that means.</p> <p>Thank you for your time,</p> <p>Beatriz Ortiz Oviedo Central Park HOA President -- Beatriz Ortiz President Oviedo Central Park HOA C: 786•385•3602</p>	5/16/2024

Attended Event?	Name	Address	Phone	Resident / Business	Email	Comment Received From / Platform	Comment / Question	Date Received (if not during Event)
Yes	Pam Knapp		407-359-2313	Resident	pamisioux2@aol.com	Email	<p>Thank you Joseph for your time talking with me yesterday. I have taken a bit of time to format my thoughts on this project. As an avid walker in this area I'm quite concerned about the pedestrian safety offered by your options as well as the optimal traffic flow that needs to continue.</p> <p>I used to walk along this stretch of 434 routinely as I would go to Club Pilates in the Publix shopping center, Tires Plus for car care, Walgreens and Panera. I have had to deviate from this roadway as the vehicles are too close to the sidewalks and drive too erratically to be safe (note: I do not use headphones or play music - God protect those that do). Instead, I have opted to walk down Lake Jessup, crossing Mitchell-Hammock at that intersection and cutting through the Sprouts shopping center to get to Club Pilates or Tires Plus.</p> <p>I have been in touch with several members of the City of Oviedo with an update on the pedestrian crosswalks that the installed on Pine and I offer you this insight from my email to them.</p> <p>"Pam Knapp From:pamisioux2@aol.com To:Wyatt, Bobby,Cobb, Bryan,Stewart, Alexis,Mayor Sladek Cc:Pardo, David,Yeargain, Paul,Joseph.Fontanelli@dot.state.fl.us</p>	5/16/2024
No	Tyler Benson			Resident	jslandguyhym@gmail.com	Email	<p>Can you tell me more about what will happen with the intersections with both Clark and Hillcrest? In one of the pictures from your slides, it seemed to suggest that a left turn from Hillcrest onto 434 would be prevented due to a divided median.</p> <p>With these changes, would the speed limit be increased or stay at 35?</p> <p>Is there a more detailed view available showing how things will be expanded in places where there isn't much room? (What is getting squeezed out?)</p> <p>I'm also very curious what the response from the community was like at the meeting last week.</p>	5/20/2024

2-lane, Divided

Location	Comment
at Mitchell Hammock Road	Left turn lane into Chick Fil A green time too short (~5 secs)
at Mitchell Hammock Road	Mitchell Hammock EB left to Oviedo Place - not enough green time
south of Clonts Street	NB motorists speeding past in right turn lane. Anything to lower speed? Signal?
at Pharmacy on the Park	Left turn lane request. (Pharmacy on the Park)
at Oviedo Fire Station	Queuing backs up to here in SB direction at PM peak. 4-lane to Broadway
at Lawn Street	Lawn Street Townhomes request full median opening with left turn lane (3 residents requested)
at E. High Street	Desire for pedestrian crossing.
NW corner at E. High Street	Consider intermediate widening for left turn lane at select locations only.
NW corner at E. High Street	Resident wants no-build, no ROW take, and no widening
south of Hillcrest Drive	"Accident Alley". 3 crashes since March (rear ends, run off road)
SW corner of SR 434 and Hillcrest Drive	various (5 total) historic homes close to SR 434. Possible acquisition issues.
North of Hillcrest Drive	Bicyclist - prefers bike lane as compared to shared use path.
at TSA, Inc building	Needs to be 4-lane. Congestion from merge / crosswalk
at Seminole Trail Crossing	Needs to be a pedestrian overpass.

4-lane, Divided

Location	Comment
General Comment	Likes 2-lane because buffers are not big enough with 4-lane
General Comment	Speed is a concern. Lack of bike paths. Make road one-way.
at Clonts Street	right turn lane to Clonts Street, needed due to new townhomes
at Pharmacy on the Park	upset about rendering showing turn lane, but no turn lanes shown on plan view roll plot
at Boardwalk Avenue	Traffic backs up, need turn lane, not opposed to improveemnts.
at Fire Station	Questioned why fire station doesn't need traffic study for median opening.
NE corner at Lawn Street	well for irrigation system for complex
north of Lawn Street	rear ends at Park Place townhomes
at High Street	Generally concerns about crossing when riding bike on shared use path - visibility.
at Hillcrest Drive	right turn lane to Hillcrest Drive, pedestrian crossing
at TSA, Inc building	Tech Sales and Applications - 15' off road. Okay with buying him out / purchasing property.
at Seminole Trail Crossing	Concerned about traffic not stopping for PHB -- Tuskawilla / Aloma example.
at Seminole Trail Crossing	Likes crossing because state law posted sign. Signal is not continuous flashing, which is good because traffic gets immune.
north of Smith Street	Right turn lanes in high traffic areas, such as elementary school.
SE corner at SR 426/CR 419	Apartments coming - traffic a concern.