

ADMINISTRATIVE ACTION
TYPE 2 CATEGORICAL EXCLUSION

Florida Department of Transportation

SR 5/US 1 OVER PELLICER CREEK BRIDGE REPLACEMENT

District: FDOT District 5

County: Flagler County

ETDM Number: N/A

Financial Management Number: 447118-1-32-01

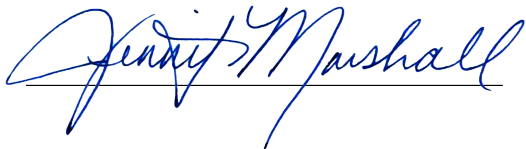
Federal-Aid Project Number: D521-060-B

Project Manager: Jeanette Maldonado-Ambler

The Environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

This action has been determined to be a Categorical Exclusion, which meets the definition contained in 40 CFR 1508.4, and based on past experience with similar actions and supported by this analysis, does not involve significant environmental impacts.

Signature below constitutes Location and Design Concept Acceptance:

A handwritten signature in blue ink that reads "Jenny Marshall". The signature is written in a cursive style and is positioned above a horizontal line.

February 18, 2025

Director Office of Environmental Management
Florida Department of Transportation

For additional information, contact:

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This document was prepared in accordance with the FDOT PD&E Manual.

This project has been developed without regard to race, color or national origin, age, sex, religion, disability or family status (Title VI of the Civil Rights Act of 1964, as amended).

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1. Project Information

1.1 Project Description

The Florida Department of Transportation (FDOT) District Five is conducting a Project Development and Environment (PD&E) Study at State Road (SR) 5 / US 1 over Pellicer Creek. The project consists of replacing the parallel bridges at SR 5 / US1 over Pellicer Creek (Existing Bridge Nos. 730008 and 730045) in Flagler (City of Palm Coast) and St. Johns Counties, Florida. SR 5 / US 1 is a rural principle arterial on the National Highway System that runs north south with a posted speed of 65 mph. The existing bridges consist of two-lanes in each direction within approximately 200 feet of right-of-way. At the bridge crossing, Pellicer Creek is not a navigational waterway. Based on the most recent bridge inspection reports, Bridge No. 730008 was constructed in 1927 and has a sufficiency rating of 62.2 and Bridge No. 730045 was constructed in 1957 and has a sufficiency rating of 48.9. Both bridges are rated functionally obsolete due to the traffic barrier type and substandard shoulder widths.

The preferred alternative replaces both bridges and includes the necessary roadway and drainage elements to provide transitions to the existing roadway and drainage system. The preferred alternative will provide a typical section meeting current design criteria and includes two 12-ft wide travel lanes in each direction with 6-ft inside and 10-ft outside shoulder widths and will be constructed within the existing right-of-way. The preferred alternative will increase the bridge spans from approximately 220 feet to 240 feet and increase the vertical and horizontal clearances. The vertical clearance above mean high-water elevation will increase from 6.96 feet to 7.52 feet and the horizontal clearance will be increased from 18 feet in width to 80 feet in width. Stormwater run-off will be collected and conveyed to roadway drainage structures and no longer discharged directly to Pellicer Creek.

Figure 1 shows the project location.



Figure 1. Project Location Map

Coastal Zone Consistency

The State of Florida has determined that this project is consistent with the Florida Coastal Zone Management Program through the issuance of St. Johns River Water Management District (SJRWMD) general permit # 214136-1 on February 21, 2024.

1.2 Purpose and Need

The purpose of this project is to correct an existing roadway deficiency and is based on the following needs:

Existing Roadway Deficiency

Based on the most recent bridge inspection reports dated December 2021, Bridge No. 730008 was constructed in 1927 and Bridge No. 730045 was constructed in 1957. Both bridges are rated functionally obsolete and have substandard 2-ft inside and outside shoulder widths. Both bridges have vertical face bridge railings which do not meet current design criteria due to substandard re-directive capabilities for vehicles traveling SR 5 / US 1.

Project Status

The project is within the jurisdiction of River to Sea Transportation Planning Organization (TPO). The project is consistent with River to Sea TPO Goals Objectives Plan (GOP) Objective 1.6. The Pellicer Creek bridge replacement is funded for design, right-of-way and construction in the FDOT 2024-2029 Five-Year Work Program and River to Sea TPO 2025-2030 Transportation Improvement Program (TIP).

1.3 Planning Consistency

Currently Adopted LRTP-CFP	COMMENTS			
Yes	The project is within the jurisdiction of River to Sea Transportation Planning Organization (TPO). Project is consistent with River to Sea TPO Goals Objectives Plan (GOP) Objective 1.6 in the Long Range Transportation Plan (LRTP) goals and strategies.			
	Currently Approved	\$	FY	COMMENTS
PE (Final Design)				
TIP	Y	2,083,634	>2025	PE Programmed prior to FY 2025
STIP	Y	1,881,764 36,780	>2025 2025	PE Programmed prior to FY 2025
R/W				
TIP	Y	183,375 25,000	>2025 2025	ROW costs >2025 included in Roll Forward Report
STIP	Y	165,000	2025	
Construction				
TIP	Y	13,412,509	2025	
STIP	Y	13,841,958	2026	

2. Environmental Analysis Summary

Issues/Resources	Significant Impacts?*			
	Yes	No	Enhance	NoInv
3. Social and Economic				
1. Social	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Economic	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Land Use Changes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Mobility	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Aesthetic Effects	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. Relocation Potential	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. Farmland Resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Cultural Resources				
1. Section 106 of the National Historic Preservation Act	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Section 4(f) of the USDOT Act of 1966, as amended	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Section 6(f) of the Land and Water Conservation Fund	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Recreational Areas and Protected Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. Natural Resources				
1. Protected Species and Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Wetlands and Other Surface Waters	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Essential Fish Habitat (EFH)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Floodplains	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Sole Source Aquifer	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Water Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. Aquatic Preserves	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. Outstanding Florida Waters	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. Wild and Scenic Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. Coastal Barrier Resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Physical Resources				
1. Highway Traffic Noise	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Air Quality	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Contamination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Utilities and Railroads	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Construction	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

USCG Permit

- A USCG Permit IS NOT required.
- A USCG Permit IS required.

* **Impact Determination:** Yes = Significant; No = No Significant Impact; Enhance = Enhancement; NoInv = Issue absent, no involvement. Basis of decision is documented in the following sections.

3. Social and Economic

The project will not have significant social and economic impacts. Below is a summary of the evaluation performed.

3.1 Social

Demographics

An analysis of minority and low-income populations was conducted through a review of census data. The study area for reviewing the demographics included a one-mile buffer surrounding the project area. A one-mile buffer was used due to the rural nature of the study area. The Sociocultural Data Report is located in the project file. Per the 2017-2021 American Community Survey (ACS) census data (Table 1), the residential population in the study area is approximately 29.21% minorities compared to Flagler County (26.22%) and St. Johns County (19.07%). The ACS defines minority population percentage as "the percent of individuals in a block group who list their racial status as a race other than white alone and/or list their ethnicity as Hispanic or Latino. That is, all people other than non-Hispanic white-alone individuals. The word 'alone' in this case indicates that the person is of a single race, not multiracial." This bridge replacement project will enhance safety, therefore all populations including minorities will benefit from this project.

TABLE 1 - Sociocultural Data			
Evaluation Criteria	Flagler County	St. Johns County	Project Study Area
Total population	113,888	265,724	178
Percent of the population that is White	79.12%	84.87%	79.78%
Percent of the population that is Black	9.76%	5.35%	8.99%
Percent of the population that is Hispanic*	10.94%	7.63%	14.61%
Percent of the population that is Asian	2.38%	3.03%	2.25%
Percent of the population that is Other**	8.74%	6.74%	7.87%
Percent of the population that is considered 'Minority'	26.22%	19.07%	29.21%
Median population age	52	43.8	47
Percent of the population that is above 65 years old	30.41%	20.10%	8.99%
Source: Efficient Transportation Decision Making (ETDM) Environmental Screening Tool (EST) 1990-2021; ACS 2017-2021			
*Hispanic includes persons of any race with Hispanic or Latino family heritage.			
**Other persons include: American Indian/Alaskan Native, Asian, Native Hawaiian, some other race alone, and two or more races.			

In addition to race/ethnicity and household income, the ACS five-year estimates were reviewed to evaluate the percentage of households with one or more persons 65 years or older and the percentage of persons with limited English proficiency. The percentage of elderly within the study area is 8.99% percent.

Within the study area, the percentage of persons that Speak English Less than Very Well is 9.88%, and the percentage of persons that Speak English Not Well or Not at All is 2.33%.

Community Facilities

Within the one-mile buffer of the project area, there is only one community facility: the Florida Agricultural Museum. This Museum is located at 7900 Old Kings Road North, Palm Coast, Florida 32137. The features provided at this museum include a restored 1890s pioneer homestead, early 20th century dry goods store, five restored buildings from a 1930's Great Depression-era citrus operation and a dairy barn. Additionally, horseback riding tours are also offered. No right-of-way will be required from this facility and there will be no negative impacts to this facility from the proposed project. Since the bridges will be built in phases, it is anticipated that there will not be significant issues to the traveling public or emergency response teams.

Community Cohesion

Community cohesion is the degree residents have a sense of belonging to their community. Community cohesion may also include the degree in which neighbors interact and cooperate with one another, the level of attachment felt between residents and institutions in the community, and/or a sense of common belonging, cultural similarity or "togetherness" experienced by the population. This project is the reconstruction of an already existing bridge; therefore, community cohesion is not expected to be negatively impacted. The preferred alternative was developed within existing right-of-way. As such, the preferred alternative does not divide existing neighborhoods or result in a significant division between the neighborhoods and places of interest that may be frequented by the residents. The proposed project is occurring within the existing FDOT right-of-way, therefore, no relocations or right-of-way acquisitions are proposed.

3.2 Economic

Table 2 illustrates the differences in *Household Income Characteristics* summarized from the 2017-2021 ACS five-year estimates, between the project study area, St. Johns County, and Flagler County. ACS estimates indicate that the median household income of the study area is approximately \$41,447, with approximately 20.37% of households having incomes below the federal poverty level.

TABLE 2 Project Area Household Income Characteristics			
Geography	Flagler County	St. Johns County	Project Study Area
Median Household Income	\$62,305	\$88,794	\$41,447
Households below Poverty Level	9.46%	7.38%	20.37%
Source: ETDM EST 1990-2021; ACS 2017-2021			

The project is not located within a Rural Area of Opportunity. The project improvements are expected to maintain access to businesses (Shantytown Shopping Village) and between communities on either side of the bridge. While the project will be developed primarily utilizing the right-of-way of the existing bridge and roadways, access to adjacent businesses, residences, and recreational areas could potentially be affected during project construction.

There are no proposed relocations and no right-of-way acquisition required from the adjacent businesses or lands. Therefore, there is no loss of tax base from a full business relocation. Since the bridges are being built in phases, it is anticipated that there will not be significant issues to the traveling public.

3.3 Land Use Changes

The surrounding land use in the study area is composed of a majority of Agricultural (23.98%), Acreage Not Zoned For Agriculture (15.20%), Public/Semi Public (17.86%), and Recreational (16.71%). The replacement of the existing bridges is not expected to change land use or induce secondary development in the area. Land use map included in project file.

3.4 Mobility

The SR 5 / US 1 bridges are north of the City of Bunnell and near the City of Palm Coast. The bridges provide a means of connecting the traveling public in those cities to Interstate 95. The bridges also provide a connection between the nearby cities and conserved areas including Washington Oaks Gardens State Park, Faver-Dykes State Park, and the Matanzas State Forest. Since the bridges will be replaced within the existing right-of-way and will be built in phases, it is anticipated that there will not be significant issues to the traveling public.

3.5 Aesthetic Effects

Per the Bridge Development Report (BDR), a Level One Aesthetics is recommended since the bridge is located in a rural area away from residences and local businesses. This consists of cosmetic improvements to conventional bridge types, such as the use of color pigments in the concrete, texturing the surfaces, modifications to fascia walls, beams, and surfaces, or more pleasing shapes for columns and caps. Additionally, the existing bridges do not have lighting present on either the begin or end approaches and no lighting is proposed as part of this project.

3.6 Relocation Potential

The proposed project is occurring within the existing FDOT right-of-way, therefore, no relocations or right-of-way acquisitions will be needed.

The proposed project, as presently conceived, will not displace any residences or businesses within the community. Should this change over the course of the project, a Right of Way and Relocation Assistance Program will be carried out in accordance with Section 421.55, Florida Statutes, Relocation of displaced persons, and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17).

3.7 Farmland Resources

Through coordination with the Natural Resources Conservation Service, it has been determined that no farmlands as defined by 7 CFR Part 658 are located in the project vicinity.

4. Cultural Resources

The project will not have significant impacts to cultural resources. Below is a summary of the evaluation performed.

4.1 Section 106 of the National Historic Preservation Act

The proposed project will result in unavoidable adverse effects to the resource(s) listed below, which is eligible for listing in the National Register of Historic Places (NRHP). FDOT and the State Historic Preservation Officer (SHPO) have executed a Memorandum of Agreement (MOA), which outlined conditions to minimize and mitigate the adverse effects resulting from the project. Consequently, FDOT commits to the stipulations provided below as outlined in the MOA.

The project proposes replacing FDOT Bridge No. 730008 (8FL01008/8SJ08262) over Pellicer Creek and the construction of a new bridge in its place due to multiple factors related to the age of the structure, the structural capacity and condition of the bridge, and the substandard roadway geometry compared to current FDOT standards. The APE for the project was defined as the existing right of way from approximately 1,100 feet south of the Flagler and St. Johns County Line to 617 feet north of the line extending east and west to the back or side property lines adjacent to the right of way or no more than 330 feet from the right of way. FDOT undertook a cultural resources survey in June 2022 and determined that FDOT Bridge #730008 (8FL01008/8SJ08262), carrying SR 5 / US 1 southbound over Pellicer Creek, was eligible for listing in the NRHP under Criteria A and C for its role in Florida's transportation history. Additionally, it is an early and intact example of a T-beam bridge from the Florida boom period of the 1920s. SHPO concurred with the determination on July 6, 2022.

The Florida SHPO provided concurrence with the findings of the Cultural Resource Assessment Survey (CRAS) on July 6, 2022. This concurrence is provided as an attachment. The CRAS is located in the project file as a technical material. The CRAS was also provided to the Muscogee (Creek) Nation (MCN), and the submission correspondence is located in the project file. No response was received.

At present, FDOT Bridge No. 730008 is 96 years old, which is well beyond the 75-year design life for the bridge. While previous repairs and preventative measures have occurred, the most recent (2021) bridge inspections determined the bridge to be functionally obsolete, which means it does not meet current roadway design standards.

The three alternatives evaluated were rehabilitation, replacement, and no-build. Rehabilitation in the form of repairs or pile jacket replacement would remain in a bridge that does not meet the current design standards. Neither the rehabilitation nor the no-build alternative meet the purpose and need of correcting an existing roadway deficiency and were removed from further consideration. Therefore, replacement is the preferred alternative.

Because rehabilitation and continued use of this historic property are not feasible given its current condition, the project-related effects posed by the build alternatives were evaluated. All of the build alternatives involve removal and replacement of 8FL01008/8SJ08262, resulting in a total loss of historic fabric. The implementation of any of the build alternatives will result in an adverse effect to NRHP-eligible FDOT Bridge No. 730008 (8FL01008/8SJ08262).

A Section 106 Case Study Report was completed for this project and is located in the project file as technical material. This report concluded that the project will result in an adverse effect to FDOT Bridge No. 730008 (8FL01008/8SJ08262).

The Florida SHPO provided concurrence with the findings of the Section 106 Case Study Report on October 25, 2023. This concurrence is provided as an attachment. The Case Study Report was also provided to the MCN. No responses were received for either document. The MCN submission correspondence is included in the project file.

Through the affected parties consultation process, FDOT proposed to prepare state equivalent Level III Historic American Engineering Record documentation for 8FL01008/8SJ08262 as mitigation for the loss of this historic property. The Section 106 Memorandum of Agreement (MOA) between the SHPO and FDOT stipulating the Level III mitigation for adverse effects to the bridge was executed on October 30, 2024.

4.2 Section 4(f) of the USDOT Act of 1966, as amended

The following evaluation was conducted pursuant to Section 4(f) of the U.S. Department of Transportation Act of 1966, as amended, and 23 CFR Part 774.

The project area contains one Section 4(f) protected resource. Bridge #730008 is a significant historic site that carries the southbound lanes of SR 5/US 1 over Pellicer Creek. The bridge is a significant example of tee-beam bridge engineering. It was originally constructed in 1927 and widened in 1948. On July 6, 2022, the Official with Jurisdiction, the State Historic Preservation Officer (SHPO), concurred that the bridge is eligible for listing in the NRHP. The project proposes to construct new bridges over Pellicer Creek which will necessitate the removal of Bridge #730008.

The bridge is structurally deficient and functionally obsolete. The proposed replacement of the bridge would result in the Section 4(f) use of Bridge #730008. FDOT utilized the Programmatic Section 4(f) Evaluation and Approval for Federal Highway Administration (FHWA) Projects that Necessitate the Use of Historic Bridges. The documentation of the programmatic evaluation includes a detailed description of the use of the bridge and findings of alternatives including No Build, Build on New Location without Using the Old Bridge, Rehabilitation without Affecting the Historic Integrity of the Bridge, and Replacement. The documentation also includes measures to minimize harm and a summary of the opportunities for public comment.

OEM concurrence of the Section 4(f) occurred on January 16, 2025, and documentation is attached.

4.3 Section 6(f) of the Land and Water Conservation Fund Act of 1965

There are no properties in the project area that are protected pursuant to Section 6(f) of the Land and Water Conservation Fund of 1965.

4.4 Recreational Areas and Protected Lands

There are no other protected public lands in the project area.

Recreational areas and protected lands are located adjacent to the project limits but no impacts to these areas are anticipated. Figure 2 shows the location of the limits of the recreational areas and conservation lands in relation to the project limits.

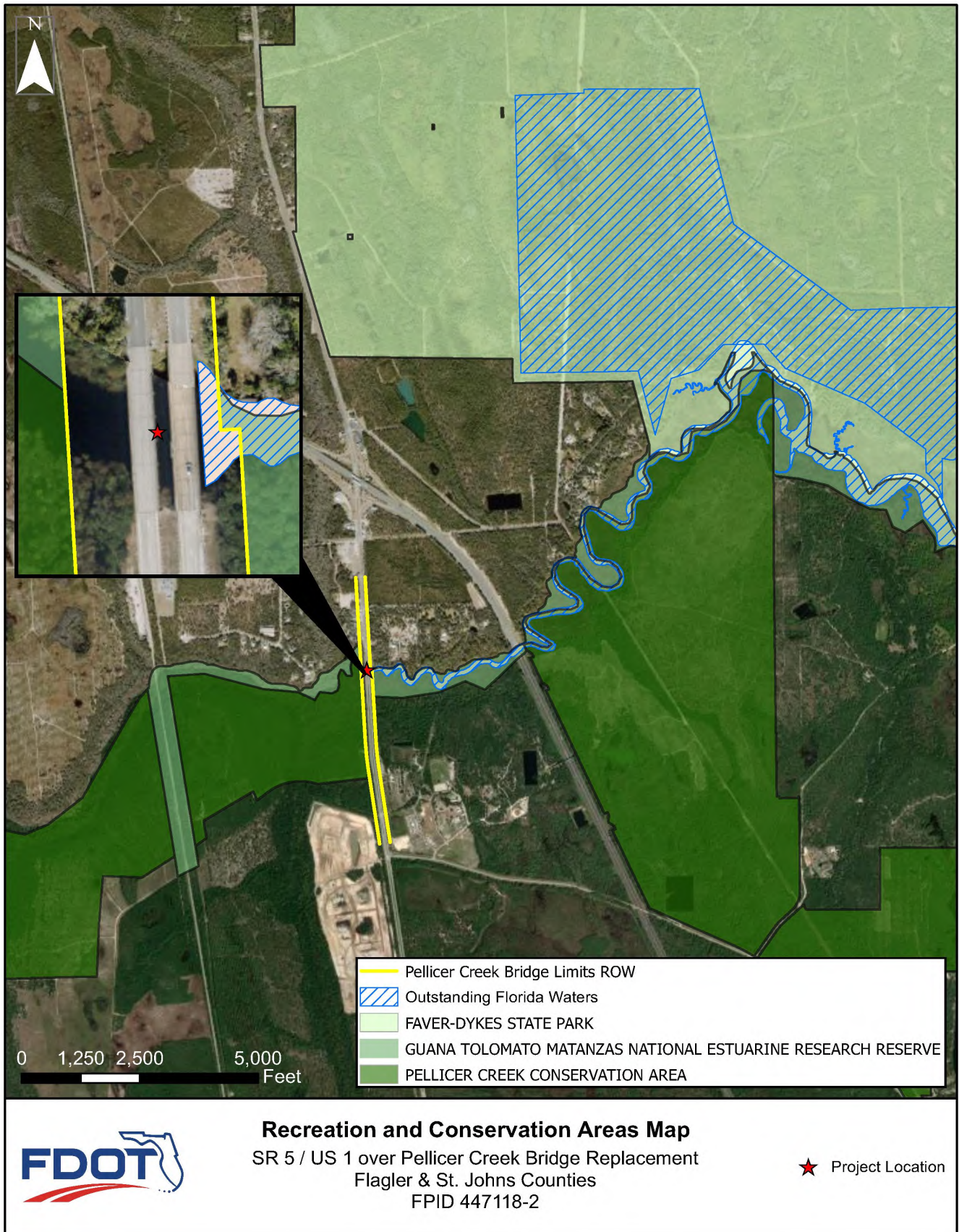


Figure 2. Recreation and Conservation Areas Map

5. Natural Resources

The project will not have significant impacts to natural resources. Below is a summary of the evaluation performed:

5.1 Protected Species and Habitat

The following evaluation was conducted pursuant to Section 7 of the Endangered Species Act of 1973 as amended as well as other applicable federal and state laws protecting wildlife and habitat.

A protected species and habitat assessment was conducted and the results are summarized in the Project Environmental Narrative document provided as technical material in the file. Tables 3A and 3B below indicate the effect determinations for both federal and state listed species. No protected species were observed during the field review.

Table 3A Effect Determinations for Federally Listed Species

Effect Determination	Scientific Name	Common Name	Federal Status	State Status
"No effect"	<i>Aphelcoma coerulescens</i>	Florida scrub-jay	T	T
"May affect, not likely to adversely affect"	<i>Mycteria americana</i>	Wood stork	T	T
	<i>Drymarchon corais couperi</i>	Eastern indigo snake	T	T

Status: T = threatened

Table 3B Effect Determinations for State Listed Species

Effect Determination	Scientific Name	Common Name	State Status
"No adverse effect"	<i>Antigone canadensis pratensis</i>	Florida sandhill crane	T
	<i>Egretta caerulea</i>	Little blue heron	T
	<i>Egretta rufescens</i>	Reddish egret	T
	<i>Egretta tricolor</i>	Tricolored heron	T
	<i>Gopherus polyphemus</i>	Gopher tortoise	T

Status: T=threatened

Based on a review of the data and habitats on the project site, 11 listed or protected animal species have the potential to occur in the area and are discussed below. The project site is located entirely within the U.S. Fish and Wildlife Service (USFWS) Consultation Area for the Florida scrub-jay and core foraging area for one wood stork rookery (No. 606109). No consultation areas are present for the eastern black rail or the tricolored bat at this time and suitable habitat for both species is not present within the preferred alternative. The tricolored bat is currently proposed endangered with USFWS, if the listing status is elevated to threatened or endangered, FDOT commits to re-initiating consultation with USFWS to determine appropriate avoidance and minimization measures for protection of the newly listed species. There are at least 27 listed plant species in either county but none occur in the habitat types within the project site.

The eastern indigo snake is threatened due to over-collecting for the pet trade and habitat loss and fragmentation. The indigo snake is widely distributed throughout central and south Florida, occurring in a range of habitats from upland scrub

to mangrove swamps. Indigo snakes are strongly associated with gopher tortoises, whose burrows provide refugia from cold or desiccating conditions. According to the USFWS' 2017 Consultation Key for the Eastern Indigo Snake, the sequential effect determination of $A > B > C > D =$ "**not likely to adversely affect**" the eastern indigo snake.

The Florida scrub-jay is classified as threatened due to habitat loss, degradation, and fragmentation. The species occurs on ancient dune systems and scrub habitats in peninsular Florida. Three types of scrub habitat were defined for scrub-jays. Type I habitat includes any upland plant community where scrub oak species (e.g., myrtle oak, sand live oak) 15% cover. Type II habitat is any plant community where one or more scrub oak species is present but < 15% cover. Type III habitat includes upland or seasonally dry wetland habitats within 400 meters (0.25 miles) of any Type I or II habitat. Florida Fish and Wildlife Conservation Commission (FWC) (1994) observed Florida scrub-jays in coastal dune scrub approximately 4.5 miles from the project site. There is no scrub-jay habitat within the project site and the project will have "**no effect**" on Florida scrub-jays.

The wood stork is threatened due to habitat loss and degradation; however, USFWS proposed delisting the southeast distinct population from the Endangered Species Act (ESA) list in February 2023. Wood storks are large, long-legged wading birds that breed in colonies, typically nesting with conspecifics and other wading bird species within a landscape containing sufficient wetland foraging habitats. Suitable foraging habitat consists of wetlands that generally contain aquatic prey which is concentrated by decreasing water levels (e.g., tidal creeks at low tide, etc.). The project is within the core foraging area for one wood stork rookery (No. 606109). According to the USFWS' 2008 Wood Stork Effect Determination Key for Central and North Peninsular Florida, the sequential effect determination of $A > B > C =$ "**not likely to adversely affect**" the wood stork. This determination was based on the fact that the impacts to wetlands will be < 0.50 acres.

The Florida sandhill crane is a state threatened, non-migratory subspecies due to habitat loss and degradation to nesting and foraging habitat. The species is widely distributed throughout peninsular Florida, relying on shallow marshes for roosting and nesting and open upland and wetland habitats for foraging. The project site contains suitable foraging habitat for Florida sandhill cranes but the proposed impacts will be mostly restricted to existing FDOT right-of-way with minimal impact to Pellicer Creek. Based on the proposed minor impacts, "**no adverse effect is anticipated**" for the Florida sandhill crane.

The gopher tortoise is a state threatened species due to over-harvesting, habitat loss, and the practice of "gassing" tortoise burrows to flush out and capture eastern diamondback rattlesnakes. They require well-drained, sandy soils for burrowing and nest construction, with a generally open canopy and abundance of herbaceous groundcover. Based on the lack of suitable habitat in the project site, "**no adverse effect is anticipated**" for the gopher tortoise.

Florida Fish and Wildlife Conservation Commission (FWC) listed four imperiled wading birds as threatened due to habitat loss and degradation, mostly stemming from hydrologic alterations to foraging areas. These four species are the little blue heron, reddish egret, roseate spoonbill and tricolored heron. The roseate spoonbill is not known to occur in the area. The project site contains potential foraging habitat for the other imperiled wading bird species but the proposed impacts will be mostly restricted to existing FDOT right-of-way with minimal impact to Pellicer Creek. A "**no adverse effect is anticipated**" for imperiled wading bird species.

The bald eagle was removed from the ESA in 2007 and from Florida's Endangered and Threatened Species list in 2008. The species remains protected under the Bald and Golden Eagle Protection Act and the Migratory Bird Treaty Act. They nest in the tops of very tall trees that provide unobstructed views to nearby habitats, particularly lakes and other open waters. Public data from the FWC and Audubon Florida indicate the nearest bald eagle nest is more than 2 miles from the project site. Based on the project information, the bald eagle is not anticipated to be impacted.

The Florida black bear was removed from Florida's Endangered and Threatened Species list in 2012. However, the species remains protected under 68A-4.009, F.A.C. The black bear requires large amounts of space for its home range and a variety of forested habitats, including flatwoods, swamps, scrub oak ridges, bayheads, and hammocks. There have not been documented black bear roadkill in the project site. Black bear nuisance has been reported in the project site as recently as 2020. Based on the project information, the black bear is not anticipated to be impacted.

The project site and surrounding area is not designated critical habitat for any federally listed species. However, the project area is located adjacent to conservation areas. The proposed project will not impact conservation areas since they occur outside of the FDOT right-of-way.

On July 31, 2024 USFWS provided concurrence on the effect determination for the eastern indigo snake, Florida scrub-jay, and wood stork. The USFWS species concurrence letter is attached.

Coordination with NMFS was conducted on May 23, 2024 regarding concurrence for ESA-listed species under NMFS' purview. NMFS provided concurrence on August 16, 2024 that the proposed action is not likely to adversely affect NMFS ESA-listed species or designated critical habitat. The NMFS ESA-Listed Species are as follows:

A. Smalltooth Sawfish

Smalltooth sawfish live in tropical seas and estuaries of the Atlantic Ocean. They are typically observed in shallow, coastal waters and less commonly documented in the lower reaches of freshwater river systems. According to the NMFS website, smalltooth sawfish are most often found off the southwest coast of Florida, from Charlotte Harbor through the Everglades and Florida Keys region. Their critical habitat is located in southwest Florida, with exclusion zones near the mouth of the Caloosahatchee River.

B. Atlantic Sturgeon

Atlantic sturgeon live in rivers and coastal waters from Canada to Florida. Due to overfishing, the abundance of natal populations is much less than historical levels, but breeding populations still exist in at least 22 U.S. rivers from Maine to Georgia. Most juveniles remain in their river of birth for at least several months before migrating out to the ocean. The South Atlantic distinct population segment is considered endangered, with the only critical habitat in Florida designated in the St. Mary's River.

C. Sea Turtles

Sea turtles are marine reptiles that are well adapted to life in the ocean, returning to lay their eggs on land. Green sea turtles are found worldwide and nest in over 80 countries. In the U.S., green sea turtles are part of the North Atlantic distinct population segment. Important feeding areas in Florida for green sea turtles include the Indian River Lagoon, the Florida Keys, Florida Bay, the Dry Tortugas, Homosassa, Crystal River, Cedar Key, and St. Joseph Bay.

Kemp's Ridley turtles are primarily found in the Gulf of Mexico, but juveniles are also found in the Atlantic Ocean as far north as Nova Scotia. The majority of Kemp's Ridley nesting occurs on the beaches of the western Gulf of Mexico, and 95% of worldwide Kemp's Ridley nesting occurs in the state of Tamaulipas, Mexico. They have been observed nesting in Florida, mostly on beaches along the Gulf of Mexico. Critical habitat has not been designated.

Loggerhead turtles have nine distinct population segments worldwide, including the Northwest Atlantic Ocean covering Florida, Georgia, South Carolina, and North Carolina. Many adults who nest on U.S. beaches migrate from neighboring nations like the Bahamas, Cuba, and Mexico. Loggerhead turtle nearshore reproductive critical habitat is located along the

Atlantic Coast, including the Marineland beaches approximately five miles from the project.

FDOT has determined the project is "not likely to adversely affect" the smalltooth sawfish, Atlantic sturgeon, or sea turtles (green, Kemp's Ridley, or loggerhead) because these species rarely if ever occur in this stretch of Pellicer Creek. The waters are not located near known sea turtle strandings or nesting beaches. Further, the waters are not known to support sawfish or sturgeon. The post-construction condition is expected to improve the hydraulic condition and water quality for aquatic resources. FDOT will adhere to the BMPs and conservation measures noted to minimize potential impacts during construction.

The project will have no impact on designated critical habitat. Table 4 indicates the effect determinations for the ESA-listed species in the action area.

Table 4. NMFS ESA-listed Species in the Action Area and Effect Determination(s)

Species	ESA Listing Status	Effect Determination (Species)
Green (North Atlantic [NA] distinct population segment [DPS])	T	Not Likely to Adverse Affect (NLAA)
Kemp's Ridley	E	NLAA
Loggerhead (Northwest Atlantic [NWA] DPS)	T	NLAA
Smalltooth Sawfish	E	NLAA
Atlantic Sturgeon	E	NLAA

5.2 Wetlands and Other Surface Waters

The following evaluation was conducted pursuant to Presidential Executive Order 11990 of 1977 as amended, Protection of Wetlands and the USDOT Order 5660.1A, Preservation of the Nation's Wetlands.

A wetland delineation of the project site and surrounding area was completed in 2023. The wetland delineation relied on literature reviews, public data, and field surveys to determine the location and extent of wetlands in the area. During the field surveys, wetland plants, hydric soil indicators, and other components typically found in wetland areas were identified. The wetland delineation was consistent with the U.S. Army Corps of Engineer (USACE) 1987 Corps of Engineers Wetland Delineation Manual, the 2010 Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Atlantic and Gulf Coastal Plain Region, and 62-340, Florida Administrative Code (F.A.C.). The wetlands and surface waters in the project site are described according to Florida Land Use, Cover and Forms Classification System (FLUCFCS) and the National Wetlands Inventory (NWI) (Cowardin et al. 1979).

Wetland 1

Wetland 1 is classified as FLUCFCS 630 (Wetland Forest Mixed) and NWI PFO1/4C (Palustrine, Forested Broad-Leaved Deciduous/Needle-Leaved Evergreen, Seasonally Flooded) and abuts SR 5 / US 1 and Pellicer Creek, in the Southeast (SE) Quadrant of the project site. The canopy includes cabbage palm, black gum, loblolly bay, red maple and loblolly pine. The understory consists of wax myrtle in the shrub layer with lizard tail and cinnamon fern groundcover. The project will directly impact 0.03 acres of Wetland 1, mostly due to the change in side slopes and rubble riprap for a proposed outfall.

Wetland 2

Wetland 2 is classified as FLUCFCS 630 (Wetland Forest Mixed) and abuts SR 5 / US 1 and Pellicer Creek, in the NE Quadrant of the project site. The canopy includes cabbage palm, red maple, and hazel alder. The understory consists of a sugarberry midstory with maidencane groundcover. The project will directly impact 0.002 acres of Wetland 2, mostly due to the change in side slopes and rubble riprap for a proposed outfall.

Wetland 3

Wetland 3 is classified as FLUCFCS 617 (Mixed Wetland Hardwoods) and NWI PFO1/2F (Palustrine, Forested Broad-Leaved Deciduous/Needle-Leaved Deciduous, Semipermanently Flooded) and abuts SR 5 / US 1 and Pellicer Creek, in the NW Quadrant of the project site. The canopy includes cabbage palm, bald and pond cypress, red maple, American elm, and juniper. The understory consists of a wax myrtle shrub layer with royal fern groundcover. Grapevine grows in multiple strata. The project will directly impact 0.19 acres of Wetland 3, mostly due to the change in side slopes and rubble riprap for a proposed outfall.

Wetland 4

Wetland 4 is classified as FLUCFCS 642 (Saltwater Marshes) and NWI E2EM1N (Estuarine, Intertidal, Emergent, Persistent, Regularly Flooded) and abuts SR 5 / US 1 and Pellicer Creek, in the SW Quadrant of the project site. The canopy includes cabbage palm, bald and pond cypress, red maple, American elm, and cedar. The understory consists of a wax myrtle shrub layer with royal fern groundcover. Grapevine grows in multiple strata. The observed plant community appears closer to FLUCFCS 630 / NWI PFO1/2F rather than the Geographic Information System (GIS) data. The project will directly impact 0.22 acres of Wetland 4 due to the change in side slopes.

Surface Water 1

Surface Water 1 (Pellicer Creek) is classified as FLUCFCS 510 (Streams and Waterways) and NWI E1UBL (Estuarine, Subtidal, Unconsolidated Bottom, Subtidal) and flows under the existing bridges. The project will directly impact 0.04 acres of Surface Water 1. The impact will be the result of rubble riprap enhancement at the existing bridge abutments.

The project as been designed to avoid and minimize wetland and surface water impacts to the greatest extent practicable, including a net reduction in the number of piles and pile area in Pellicer Creek. However, unavoidable impacts will result in direct impacts of 0.44 acres of wetlands and 0.04 acres of surface waters for a total of 0.48 acres. The majority of the impacts will be the result of changes in side slopes and rubble riprap enhancement around the abutments. The project will result in several minor secondary impacts extending 25 feet from the limits of the proposed direct wetland impacts. The secondary impacts for Wetlands 1, 2, 3, and 4 will be 0.17 acres, 0.02 acres, 0.22 acres, and 0.37 acres, for a total of 0.78 acres.

Wetland impacts which will result from the construction of this project will be mitigated pursuant to Section 373.4137, F.S., to satisfy all mitigation requirements of Part IV of Chapter 373, F.S., and 33 U.S.C. 1344. The project will result in 0.48 acres of direct impacts and an additional 0.78 acres of secondary impacts, resulting in a Uniform Mitigation Assessment Method (UMAM) functional loss of 0.41 units. The project is located within the service area of Fish Tail and Brick Road Mitigation Banks and credits will be purchased from one of these federally-approved wetland mitigation banks to achieve no net loss of wetland function. The proposed wetland impacts will be mitigated at a federally-approved wetland mitigation bank within the same Hydrologic Unit Code. Therefore, cumulative impacts are not anticipated for the project.

Agency coordination with the St. Johns River Water Management District (SJRWMD) was held on June 1, 2023 to provide an overview of the project and discuss environmental permitting. Agency coordination with the US Army Corps of Engineers (USACE) was held on July 13, 2023 to provide an overview of the project and discuss environmental

permitting.

5.3 Essential Fish Habitat (EFH)

An Essential Fish Habitat (EFH) Assessment has been prepared and consultation has been completed in accordance with the Magnuson-Stevens Fishery Conservation and Management Act (MSFCMA). It has been determined that this project will not have adverse effects to EFH.

According to the National Marine Fisheries (NMFS) EFH Mapper, the project site does not contain EFH but the tool states that spatial data does not exist for some species. Public EFH data from the FWC, in collaboration with the NMFS, indicates that Pellicer Creek may provide EFH for Penaeid Shrimp and the Snapper/Grouper complex, within the project site. The project will have only minor impacts in Pellicer Creek for rubble riprap enhancement of the abutments. Additionally, the post-construction condition of the bridge will result in fewer piles in the water than exist currently, thus providing improved hydraulic conditions and **"no adverse affect"** on managed fisheries.

Based on coordination with the National Marine Fisheries Service to comply with Magnuson-Stevens Fishery Conservation and Management Act (MSFCMA), FDOT commits to reinitiate consultation and provide information necessary to complete consultation on the Penaeid Shrimp and the Snapper/Grouper prior to advancing the project to construction. FDOT's commitment is intended to provide reasonable assurance, per 23 CFR 771.133, that requirements of the MSFCMA are able to and will be met prior to construction and this approach is affirmed by the National Marine Fisheries Service. The status of this commitment will be updated in any subsequent project re-evaluations.

5.4 Floodplains

Floodplain impacts resulting from the project were evaluated pursuant to Executive Order 11988 of 1977, Floodplain Management.

A Bridge Hydraulic Report was completed on October 18, 2023 and is located in the project file as technical material.

The Pellicer Creek bridges span across two Flood Insurance Rate Maps (FIRM) panels (12109C0495J in St Johns County and 12035C0018E in Flagler County). The project limits are within Zone AE which have a 1% risk of flooding annually. The Federal Emergency Management Agency (FEMA) FIRMs show a 100-yr base flood elevation (BFE) of +6 ft NAVD88 in the vicinity of the bridges. The FEMA National Flood Hazard Layer FIRMette map is included in the Bridge Hydraulic Report that is part of the project file. All bridge crossings must comply with the National Flood Insurance Program. As shown on the FIRM, the bridges do not lie in a FEMA floodway. Therefore, the widening of the bridges does not require a No-Rise certification.

The proposed structure will perform hydraulically in a manner equal to or greater than the existing structure, and backwater surface elevations are not expected to increase. Thus, there will be no significant adverse impacts on natural and beneficial floodplain values. There will be no significant change in flood risk, and there will not be a significant change in the potential for interruption or termination of emergency service. Therefore, it has been determined that this encroachment is not significant.

5.5 Sole Source Aquifer

There is no Sole Source Aquifer associated with this project.

5.6 Water Resources

The SJRWMD General Permit 214136-1 was approved on February 21, 2024 for this project and is located within the project file.

Currently, the bridge is directly discharging into the Outstanding Florida Water (OFW). For the preferred alternative the deck drainage runoff will flow towards the ends of the bridge and into ditches on the sides of the road. Best Management Practices (BMPs) will provide treatment prior to entering the OFW in the post-construction. Erosion is not anticipated under the bridge as a result of the deck runoff as the abutments have rubble riprap protection.

A National Pollutant Discharge Elimination System (NPDES) permit will be obtained by the contractor. A Stormwater Runoff Control Concept Plan was developed by the design team and will be implemented by the contractor.

5.7 Aquatic Preserves

There are no aquatic preserves in the project area.

5.8 Outstanding Florida Waters

There are no Outstanding Florida Waters (OFW) in the project area.

5.9 Wild and Scenic Rivers

There are no designated Wild and Scenic Rivers or other protected rivers in the project area.

5.10 Coastal Barrier Resources

It has been determined that this project is neither in the vicinity of, nor leads directly to a designated coastal barrier resource unit pursuant to the Coastal Barrier Resources Act of 1982 (CBRA) and the Coastal Barrier Improvement Act of 1990 (CBIA).

This project is outside the limits of the Coastal Barrier Resource Unit - Matanzas River Unit P05A/P05AP.

6. Physical Resources

The project will not have significant impacts to physical resources. Below is a summary of the evaluation performed for these resources.

6.1 Highway Traffic Noise

This project is a Type III project according to the provisions of 23 CFR 772 and Section 335.17, F.S., therefore noise analysis or consideration of abatement measures is not required.

6.2 Air Quality

This project is not expected to create adverse impacts on air quality because the project area is in attainment for all National Ambient Air Quality Standards (NAAQS) and because the project is expected to not change the Level of Service (LOS) and not change delay and congestion on all facilities within the study area.

Construction activities may cause short-term air quality impacts in the form of dust from earthwork and unpaved roads. These impacts will be minimized by adherence to applicable state regulations and to applicable FDOT Standard Specifications for Road and Bridge Construction.

6.3 Contamination

An Asbestos Survey Report was completed for FDOT Bridge structures #730045 and #730008 on May 17, 2023 and is provided as an attachment and in the project file.

The Asbestos Containing Material (ACM) survey was conducted on February 13, 2023 with samples from the following materials collected: various concrete components, bearing pads, vibration pads, expansion joint compound, water drains, and surface coatings. The laboratory analyses of the collected samples did detect asbestos containing materials.

Asbestos was detected in drains located underneath and through the bridge deck edges.

A Florida licensed asbestos abatement contractor shall abate these materials before the subject work area is disturbed by any demolition or renovation activities. This licensed abatement contractor's scope of work shall include adherence to all regulations related to asbestos abatement in accordance with 40 CFR, Part 61, Sec. 61.145 (NESHAP) and 29 CFR 1926.1101 (OSHA Construction Asbestos Standard). The transportation and disposal of asbestos containing materials is also a regulated activity under 40 CFR, Subpart M, Part 61.150(a) and the licensed asbestos abatement contractor shall follow all regulations related to disposal and transportation as applicable.

Although ACM was identified in this survey, other unidentified, or unsampled bridge components may be encountered during renovation and/or demolition, and shall be surveyed to determine the presence, if any, of suspect ACM. It is best practice that the renovation and/or demolition contractor employs wet demolition methods to limit dust generation. In addition, Florida Department of Environmental Protection (FDEP) notification is required pursuant to the provisions of Rule 62-257.301, F.A.C. which states "The notice requirements...apply to each owner or operator of a renovation of a facility involving the removal of a threshold amount of regulated ACM or any demolition of a facility regardless of whether or not

asbestos is present" prior to any renovation and/or demolition activities.

Waste characterization and delineation of any waste streams resulting from the renovation and/or demolition of the subject structure is the responsibility of the renovation and/or demolition contractor.

6.4 Utilities and Railroads

There are existing utilities within the project limits, adjacent to or attached to the existing northbound bridge. The existing utility owners are AT&T Florida, Centurylink, Florida Power & Light (FP&L), TECO Peoples Gas and Uniti Fiber. Coordination with the utility owners is ongoing and a Utility Design Meeting is scheduled to occur on March 6, 2025. The existing utilities are generally located between the east edge of pavement of the northbound travel lanes and FDOT's eastern right-of-way line. All are buried with the exception of FP&L power lines which are located on poles adjacent to the right-of-way line. Uniti Fiber has a buried fiber optic cable which is connected to the underside of the northbound bridge. The proposed bridge design will require both Uniti Fiber and CenturyLink to relocate their facilities underground and under Pellicer Creek to avoid impacts during construction. The proposed relocation of the Uniti Fiber and CenturyLink facilities was obtained from the relocation markups provided to the design team by the utility owners and will be finalized in the Utility Work Schedules by each of the utility owners prior to the project letting to construction.

6.5 Construction

Based on the existing land uses within the limits of this project, the construction of the preferred alternative may include potential erosion and sedimentation, temporary noise and vibration impacts, visual impacts to natural setting of the roadway and disruption of traffic. Construction phase impacts are anticipated to be short-term in duration and localized around the site of construction. During the construction phase, the project will comply with the *FDOT Standard Specifications to Road and Bridge Construction* to avoid and minimize impacts.

Measures such as silt fences or silt stops will avoid erosion and sedimentation and A National Pollution Discharge Elimination System Permit will be required for construction.

Vibration-sensitive sites on the project include residences. During the construction phase of the Preferred Alternative, short-term noise and vibration may be generated by stationary and mobile construction equipment. The construction noise and vibration will be temporary at any location and controlled by adherence to the most recent edition of the *FDOT Standard Specifications for Road and Bridge Construction*.

Visual impacts associated with the storage of construction materials and establishment of temporary construction facilities will occur but are temporary and short term.

A maintenance of traffic plan has been prepared. Maintenance of traffic and sequence of construction will be planned and scheduled to minimize traffic delays during project construction. If a brief closure is needed, a detour route is available utilizing I-95 and Matanzas Woods Parkway. The detour is approximately 3 miles and would add about 3 minutes to the normal travel time. The detour route can accommodate oversized vehicles. Signs will be used as appropriate to provide notice of road closures and other pertinent information to the traveling public. The local news media will be notified in advance of road closings and other construction-related activities which could inconvenience the community so that

pedestrians, motorists, residents, and business persons can plan travel routes in advance.

7. Engineering Analysis Support

The engineering analysis supporting this environmental document is contained within the Bridge Development Report.

8. Permits

The following environmental permits are anticipated for this project:

Federal Permit(s)

USACE Section 10 or Section 404 Permit

Status

To be acquired

State Permit(s)

DEP or WMD Environmental Resource Permit (ERP)

DEP National Pollutant Discharge Elimination System Permit

Status

Permit received

To be acquired

9. Public Involvement

The following is a summary of public involvement activities conducted for this project:

Summary of Activities Other than the Public Hearing

A Public Engagement Plan/Community Awareness Plan, dated August 10, 2023 is attached and provided in the project file.

A stakeholders meeting was held on June 28, 2022 from 1:30 pm to 3:00 pm. A public stakeholders meeting was held on March 21, 2023 from 4:00 pm to 6:00 pm at the Flagler County building with invitations provided to the parcel owners adjacent to the project limits. The project information handout, presentation and meeting minutes are provided in the project file.

The 30% design plans were provided to local government stakeholders on June 6, 2023. The St. Johns County Certified Local Government (CLG) representative commented on the historic nature of the bridge. The CLG asked if it was feasible to save a bridge rail post with a date stamp. However, during the Section 10 process it was determined that the date is stamped on an element of the bridge constructed in 1948, which is outside the bridge's period of significance. Thus, the date stamp is not a significant feature of the bridge. FDOT provided this clarification to the CLG, and they subsequently stated they have no further concerns.

Coordination regarding the public engagement strategy in support of the PD&E Study was discussed between FDOT District Five and the Office of Environmental Management and agreed to on October 3, 2023. The approved PD&E engagement approach was to publish a notice of opportunity that provides the opportunity for the public to request a public hearing.

The notice of opportunity was published on Friday, August 16, 2024 and provided 15 days following the publication of the notice for the public to submit a written request to the FDOT Project Manager for a public hearing. The notice of opportunity period closed on Saturday, August 31, 2024. The notice was published in the Daytona Beach News Journal (the local newspaper), in the Florida Administrative Record, and on FDOT's public meeting notices website. It was also included on the project website at <https://www.cflroads.com/project/447118-1>. The notice of opportunity documentation is attached.

At the same time that the notice was published, the Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation were made available for public review on the project website. FDOT did not receive any requests for a public hearing nor inquiries about the project during the notice of opportunity period. Therefore, no public hearing is required. Documentation of the notice of opportunity publications is included as an attachment.

Date of Consultation with OEM: 10/03/2023

Date of opportunity for Public Hearing: 08/16/2024

10. Commitments Summary

1. FDOT will prepare a Level III Historic American Engineering Record documentation for 8FL01008/8SJ08262 as mitigation for the loss of this historic property per the Section 106 Memorandum of Agreement between SHPO and FDOT that was executed on October 30, 2024.
2. The most recent version of the USFWS' Standard Protection Measures for the Eastern Indigo Snake will be adhered to during construction.
3. The tricolored bat is currently proposed for listing under the Endangered Species Act, if the listing status is elevated to threatened or endangered, FDOT commits to re-initiating consultation with USFWS to determine appropriate avoidance and minimization measures for protection of the newly listed species.
4. Based on coordination with the National Marine Fisheries Service to comply with Magnuson-Stevens Fishery Conservation and Management Act (MSFCMA), FDOT commits to reinitiate consultation and provide information necessary to complete consultation on the Penaeid Shrimp and the Snapper/Grouper prior to advancing the project to construction.
5. If disturbance to materials containing asbestos is anticipated, a Florida licensed asbestos abatement contractor will abate these materials before the subject work area is disturbed by any demolition or renovation activities in adherence to all regulations related to asbestos abatement. The contractor will also follow all regulations related to disposal and transportation of the materials, as applicable.

11. Technical Materials

The following technical materials have been prepared to support this Environmental Document and are included in the Project File.

Sociocultural Data Report
Cultural Resources Assessment Survey (CRAS)
Section 106 Case Study Report
Bridge Hydraulic Report (BHR)
Environmental Narrative
Asbestos Survey Report Bridge #730008
Asbestos Survey Report Bridge #730045
Bridge Development Report
Pellicer Creek Bridge CAP

Attachments

Planning Consistency

River to Sea TPO LRTP (GOP)

Current STIP

TIP

TIP Roll Forward

Social and Economic

Land Use Map

NRCS Coordination Documentation

Cultural Resources

FDOT and SHPO MOA

SHPO Concurrence Letter

Section 106 Effects Case Study 2023-4979B SHPO Concurrence Letter

Section 4(f) Report

Natural Resources

Recreation and Conservation Areas Map

USFWS Species Concurrence Letter

Wetlands Map

Floodplains Map

Essential Fish Habitat Concurrence

Public Involvement

Notice of Opportunity

Planning Consistency Appendix

Contents:

River to Sea TPO LRTP (GOP)

Current STIP

TIP

TIP Roll Forward

Connect 2045 Objectives

Goal 1 - Develop and maintain a balanced and efficient multimodal transportation system

- Objective 1.1** Develop a multimodal transportation system that improves accessibility and mobility to economic centers for all users (including motor vehicle, bicycle, pedestrian, transit) as well as the movement of goods.
- Objective 1.2** Minimize congestion/delay and maintain travel time reliability on roadways and intersections through projects that improve capacity, provide for the more efficient use and operation of existing transportation facilities, and reduce transportation demand. [System Performance Measures (PM3) – See measures 1 and 2 on page 2-21]
- Objective 1.3** Provide public transit systems that serve diverse populations and deliver efficient and convenient transit service.
- Objective 1.4** Develop a plan that maximizes the use of all available existing and alternative revenue sources and is financially feasible.
- Objective 1.5** Incorporate measures that give priority to projects that provide high benefit-to-cost value.
- Objective 1.6** Adequately fund preservation of transportation assets (National Highway System Pavement Condition, Bridge Condition, and Transit Assets). [Pavement and Bridge Condition Performance Measures (PM2) – see measures 1 to 6 on page 2-17]
- Objective 1.7** Address incident management including improving response and mitigating impacts through development of alternative routes and other solutions.

Goal 2 - Support the economic development and growth of the TPO area and region

- Objective 2.1** Develop a transportation system that supports diverse economic growth, advances tourism, and improves the economic competitiveness of the region.
- Objective 2.2** Identify and support safe and efficient truck routes and other facilities that improve the movement of freight and goods. [System Performance Measures (PM3) – See measure 3 on page 2-21]
- Objective 2.3** Improve connectivity and access to rail, port, bus, and airport facilities.
- Objective 2.4** Support funding of transit service that improves access to employment activity centers.



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Web Application

Federal Aid Management Sabrina Aubery - Manager

STIP Project Detail and Summaries Online Report

**** Repayment Phases are not included in the Totals ****

Selection Criteria	
Current STIP	Detail
Financial Project: 447118 1	Related Items Shown
As Of: 1/1/2025	

HIGHWAYS							
Item Number: 447118 1	Project Description: SR 5/US 1 OVER PELLICER CREEK BRIDGE REPLACEMENT						
District: 05	County: FLAGLER	Type of Work: BRIDGE REPLACEMENT			Project Length: 0.231MI		
	Fiscal Year						
Phase / Responsible Agency	<2025	2025	2026	2027	2028	>2028	All Years
PRELIMINARY ENGINEERING / MANAGED BY FDOT							
Fund Code: ACBR-ADVANCE CONSTRUCTION (BRT)	248,189	24,585					272,774
DIH-STATE IN-HOUSE PRODUCT SUPPORT	71,296	12,195					83,491
DS-STATE PRIMARY HIGHWAYS & PTO	18,751						18,751
NHBR-NATIONAL HIGHWAYS BRIDGES	1,543,528						1,543,528
Phase: PRELIMINARY ENGINEERING Totals	1,881,764	36,780					1,918,544
RIGHT OF WAY / MANAGED BY FDOT							
Fund Code: DDR-DISTRICT DEDICATED REVENUE		115,000					115,000
DIH-STATE IN-HOUSE PRODUCT SUPPORT		50,000					50,000
Phase: RIGHT OF WAY Totals		165,000					165,000

CONSTRUCTION / MANAGED BY FDOT							
Fund Code:	ACBR-ADVANCE			13,841,958			13,841,958
	CONSTRUCTION (BRT)						
	DS-STATE PRIMARY HIGHWAYS & PTO	4,237					4,237
	Phase: CONSTRUCTION Totals	4,237		13,841,958			13,846,195
	Item: 447118 1 Totals	1,886,001	201,780	13,841,958			15,929,739
	Project Totals	1,886,001	201,780	13,841,958			15,929,739
	Grand Total	1,886,001	201,780	13,841,958			15,929,739

This site is maintained by the Office of Work Program and Budget, located at 605 Suwannee Street, MS 21, Tallahassee, Florida 32399.

For additional information please e-mail questions or comments to:
 Federal Aid Management
 Sabrina Aubery: Sabrina.Aubery@dot.state.fl.us Or call 850-414-4449
 Or
 Denise Strickland: Denise.Strickland@dot.state.fl.us Or call 850-414-4491

[Reload STIP Selection Page](#)

Office Home: [Office of Work Program](#)

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Consistent, Predictable, Repeatable

Section III - Bridge Projects

447118-1 SR 5/US 1 OVER PELLICER CREEK BRIDGE REPLACEMENT		Managed By: FDOT						Non-SIS	
Project Type: BRIDGE REPLACEMENT		Length: 0.231						County: FLAGLER	
		L RTP Ref: Pgs 2-3 to 2-5							
Fund	Phase	<2025	2025	2026	2027	2028	2029	>2029	Total
ACBR	ROW		\$ 25,000	\$ -	\$ -	\$ -	\$ -		\$ 25,000
ACBR	CST		\$ -	\$ 13,412,509	\$ -	\$ -	\$ -		\$ 13,412,509
TOTAL		\$ 2,083,634	\$ 25,000	\$ 13,412,509	\$ -	\$ -	\$ -	\$ -	\$ 15,521,143

447018-1 SAULS STREET BRIDGE REPLACEMENT FROM OAK GLEN DRIVE TO REED		Managed By: CITY OF SOUTH DAYTONA						Non-SIS	
Project Type: BRIDGE REPLACEMENT		Length: 0.02						County: VOLUSIA	
		L RTP Ref: Pgs 2-3 to 2-5							
Fund	Phase	<2025	2025	2026	2027	2028	2029	>2029	Total
LF	PE		\$ 48,832	\$ -	\$ -	\$ -	\$ -		\$ 48,832
SU	PE		\$ 439,484	\$ -	\$ -	\$ -	\$ -		\$ 439,484
TOTAL		\$ -	\$ 488,316	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 488,316

447117-1 SR 5/US 1 NB OVER TURNBULL CREEK BRIDGE REPLACEMENT		Managed By: FDOT						Non-SIS	
Project Type: BRIDGE REPLACEMENT		Length: 0.293						County: VOLUSIA	
		L RTP Ref: Pgs 2-3 to 2-5							
Fund	Phase	<2025	2025	2026	2027	2028	2029	>2029	Total
ACBR	CST		\$ 9,288,053	\$ -	\$ -	\$ -	\$ -		\$ 9,288,053
TOTAL		\$ 1,115,269	\$ 9,288,053	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 10,403,322

HIGHWAYS

ITEM NUMBER:445309 1 PROJECT DESCRIPTION:FLAGLER WEIGH STATION - SIGNING AND PAVEMENT MARKINGS *SIS*
 DISTRICT:05 COUNTY:FLAGLER TYPE OF WORK:MCCO WEIGH STATION STATIC/WIM
 ROADWAY ID:73001000 PROJECT LENGTH: 1.132MI LANES EXIST/IMPROVED/ADDED: 3/ 3/ 0

FUND CODE	LESS THAN 2025	2025	2026	2027	2028	2029	GREATER THAN 2029	ALL YEARS
PHASE: CONSTRUCTION / RESPONSIBLE AGENCY: MANAGED BY FDOT								
DIH	4,098	6,162	0	0	0	0	0	10,260
DS	759	0	0	0	0	0	0	759
DWS	504,556	0	0	0	0	0	0	504,556
TOTAL 445309 1	509,413	6,162	0	0	0	0	0	515,575

ITEM NUMBER:445309 2 PROJECT DESCRIPTION:FLAGLER WEIGH STATION - LIGHTING AND ELECTRICAL *SIS*
 DISTRICT:05 COUNTY:FLAGLER TYPE OF WORK:MCCO WEIGH STATION STATIC/WIM
 ROADWAY ID:73001000 PROJECT LENGTH: 1.132MI LANES EXIST/IMPROVED/ADDED: 3/ 3/ 0

FUND CODE	LESS THAN 2025	2025	2026	2027	2028	2029	GREATER THAN 2029	ALL YEARS
PHASE: CONSTRUCTION / RESPONSIBLE AGENCY: MANAGED BY FDOT								
DIH	5,149	5,111	0	0	0	0	0	10,260
DS	575	0	0	0	0	0	0	575
DWS	1,293,062	0	0	0	0	0	0	1,293,062
TOTAL 445309 2	1,298,786	5,111	0	0	0	0	0	1,303,897
TOTAL PROJECT:	1,808,199	11,273	0	0	0	0	0	1,819,472

ITEM NUMBER:447118 1 PROJECT DESCRIPTION:SR 5/US 1 OVER PELLICER CREEK BRIDGE REPLACEMENT *NON-SIS*
 DISTRICT:05 COUNTY:FLAGLER TYPE OF WORK:BRIDGE REPLACEMENT
 ROADWAY ID:73010000 PROJECT LENGTH: .231MI LANES EXIST/IMPROVED/ADDED: 2/ 2/ 0

FUND CODE	LESS THAN 2025	2025	2026	2027	2028	2029	GREATER THAN 2029	ALL YEARS
PHASE: PRELIMINARY ENGINEERING / RESPONSIBLE AGENCY: MANAGED BY FDOT								
ACBR	248,189	0	0	0	0	0	0	248,189
DIH	70,782	2,195	0	0	0	0	0	72,977
DS	9,231	0	0	0	0	0	0	9,231
NHBR	1,543,528	0	0	0	0	0	0	1,543,528
PHASE: RIGHT OF WAY / RESPONSIBLE AGENCY: MANAGED BY FDOT								
ACBR	0	183,375	0	0	0	0	0	183,375
PHASE: CONSTRUCTION / RESPONSIBLE AGENCY: MANAGED BY FDOT								
ACBR	0	0	13,412,509	0	0	0	0	13,412,509
DS	4,237	0	0	0	0	0	0	4,237
PHASE: ENVIRONMENTAL / RESPONSIBLE AGENCY: MANAGED BY FDOT								
DDR	0	50,000	0	0	0	0	0	50,000
TOTAL 447118 1	1,875,967	235,570	13,412,509	0	0	0	0	15,524,046
TOTAL PROJECT:	1,875,967	235,570	13,412,509	0	0	0	0	15,524,046

Social and Economic Appendix

Contents:

Land Use Map

NRCS Coordination Documentation



1400: Commercial and services

4120: Longleaf pine - xeric oak

7400: Disturbed land

8140: Roads and highways (divided 4-lanes with medians)

6170: Mixed wetland hardwoods

4340: Upland mixed coniferous/hardwood

4340: Upland mixed coniferous/hardwood

5100: Streams and waterways

6420: Saltwater marshes

6420: Saltwater marshes

8140: Roads and highways (divided 4-lanes with medians)

6300: Wetland forested mixed

6170: Mixed wetland hardwoods

6250: Hydric pine flatwoods



Source: Esri, Maxar, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



United States Department of Agriculture

November 12, 2024

Jeanette Maldonado-Ambler
Planning & Environmental Management
Stantec, FDOT D5 Consultant
719 S Woodland Boulevard
DeLand, FL 32720
Jeanette.Maldonado@dot.state.fl.us

Dear Jeanette Maldonado,

The following guidance is provided for your information.

The Natural Resources Conservation Service (NRCS) has reviewed the 447118-1, US 1 / SR 5 over Pellicer Creek Bridge Replacement Project.

The Agriculture and Food Act of 1981, (Public Law 97-98) containing the Farmland Protection Policy Act (FPPA) -Subtitle I of Title XV, Section 1539-1549, is intended to minimize the impact federal programs have on the unnecessary and irreversible conversion of farmland to nonagricultural uses. Projects are subject to FPPA requirements if they may irreversibly convert farmland (directly or indirectly) to nonagricultural use and are completed by a federal agency or with assistance from a federal agency.

“Farmland” means prime or unique farmlands as defined in section 1540(c)(1) of the FPPA or farmland that is determined by the appropriate state or unit of local government agency or agencies with concurrence of the Secretary of Agriculture to be farmland of statewide local importance. “Farmland” does not include land already in or committed to urban development or water storage. Farmland already in urban development or water storage includes all such land with a density of 30 structures per 40-acre area. Farmland already in urban development also includes lands identified as urbanized area (UA) on the Census Bureau Map, or as urban area mapped with a tint overprint on the United States Geological Survey (USGS) topographical maps, or as urban-built-up on the United States Department of Agriculture (USDA) Important Farmland Maps.

Section 1540(b) of the Act, 7 U.S.C. 4201(b), states that the purpose of the Act is to minimize the extent to which Federal programs contribute to the unnecessary and irreversible conversion of farmland to nonagricultural uses. Based on the information provided for the area of interest; the area in question meets criteria for an exemption since the location was previously disturbed, no new conversion of farmland would be taking place. The project is exempt from FPPA for said areas, according to the Code of Federal Regulation 7CFR 658, Farmland Protection Policy Act, Section 658.2; and the 2020 Census Bureau Maps. You are exempt from filling the CPA 106 for these areas, currently. Use this letter as proof of exemption.

If you have any questions concerning the soils or interpretations for this project please email me, josue.aceitunodiaz@usda.gov

NRCS - Farmland Protection Policy Act Website:
[Farmland Protection Policy Act | Natural Resources Conservation Service \(usda.gov\)](#)

Sincerely,

Josué Aceituno-Díaz
Resource Soil Scientist
Sebring Field Office
USDA-NRCS

Natural Resources Conservation Service, Florida
4500 NW 27th Avenue, Bldg.A, Gainesville, FL 32606
Voice 352-338-9500

Cultural Resources Appendix

Contents:

FDOT and SHPO MOA

SHPO Concurrence Letter

Section 106 Effects Case Study 2023-4979B SHPO Concurrence Letter

Section 4(f) Report

**MEMORANDUM OF AGREEMENT
BETWEEN THE FLORIDA DEPARTMENT OF TRANSPORTATION AND
THE FLORIDA STATE HISTORIC PRESERVATION OFFICER
REGARDING THE STATE ROAD 5 (SR 5)/US 1 OVER PELLICER
CREEK BRIDGE (No. 730008) REPLACEMENT PROJECT IN FLAGLER
AND ST. JOHNS COUNTIES, FLORIDA**

WHEREAS, Pursuant to 23 United States Code (U.S.C.) § 327 and the implementing Memorandum of Understanding (MOU) executed on May 26, 2022, the Florida Department of Transportation (FDOT) has assumed Federal Highway Administration's (FHWA) responsibilities under the National Environmental Policy Act (NEPA) for highway projects on the State Highway System (SHS) and Local Agency Program (LAP) projects off the SHS; and

WHEREAS, in accordance with the MOU, FDOT's assumption of FHWA's responsibilities under NEPA for highway projects includes assumption of responsibilities for compliance with 36 CFR Part 800, regulations implementing Section 106 of the National Historic Preservation Act (54 U.S.C. § 306108); and

WHEREAS, FDOT will provide federal financial assistance for the SR 5/US 1 over Pellicer Creek Bridge (No. 730008) Replacement, Financial ID No. 447118-1-32-01 (**Project**); and

WHEREAS, FDOT has determined that the **Project** represents an undertaking in accordance with 36 CFR § 800.3(a); and

WHEREAS, FDOT has defined the **Project's** area of potential effects (APE) as the existing SR 5/US 1 right-of-way (ROW) from approximately 335 m (1,100 ft) south of the Flagler and St. Johns County line to 188 m (617 ft) north of the line extended to the back or side property lines of parcels adjacent to the ROW or no more than 100 m (330 ft) from the ROW line, as depicted in Attachment A; and

WHEREAS, FDOT has consulted with the Florida State Historic Preservation Officer (SHPO) pursuant to the requirements of 36 CFR Part 800 and has determined that the **Project** will have an adverse effect on FDOT Bridge No. 730008 (Florida Master Site File [FMSF] Nos. 8FL01008/8SJ08262), which is eligible for listing in the National Register of Historic Places (NRHP); and

WHEREAS, FDOT has consulted with the Muscogee (Creek) Nation of Florida regarding the effects of the **Project** on historic properties; and

WHEREAS, FDOT has consulted with the Florida SHPO and the Certified Local Government representative for St. Johns County regarding the effects of the **Project** on historic properties; and

WHEREAS, FDOT has provided opportunities for public review and comment regarding the effects of the **Project** on historic properties, as appropriate; and

WHEREAS, in accordance with 36 CFR § 800.6(a)(1) FDOT has notified the Advisory Council on Historic Preservation (ACHP) of the adverse effect determination with specified documentation and has invited the ACHP to comment and participate in consultation, and the ACHP has chosen not to participate pursuant to 36 CFR § 800.6(a)(1)(iii); and

NOW, THEREFORE, FDOT and the SHPO agree that the **Project** shall be implemented in accordance with the following stipulations to take into account the effect of the **Project** on historic properties.

STIPULATIONS

FDOT shall ensure that the following measures are carried out:

I. HISTORIC AMERICAN ENGINEERING RECORD (HAER) LEVEL III EQUIVALENT DOCUMENTATION

Prior to authorizing any demolition or other activity that could damage FDOT Bridge No. 730008 (FMSF Nos. 8FL01008/8SJ08262), per guidance provided by the National Park Service (NPS), FDOT will ensure that the bridge is documented in accordance with the standards and guidelines of the Historic American Engineering Record (HAER). FDOT shall:

A. Complete Historic American Engineering Record (HAER) Level III equivalent documentation of 8FL01008/8SJ08262, including:

B. Large format photography.

1. A HAER equivalent report, inclusive of historic context, scans of large format negatives, and a sketch plan of 8FL01008/8SJ08262.

C. Submit the final HAER equivalent report and photographic documentation to the Florida Division of Historical Resources (FDHR), including an archival copy and an electronic copy for inclusion in the FMSF. Unless otherwise agreed to by the NPS, FDOT will ensure that all documentation is completed and accepted in writing by the NPS prior to demolition.

II. PROFESSIONAL STANDARDS

All archaeological and historic preservation work carried out pursuant to this Agreement shall be conducted by, or under the direct supervision of, a person or persons meeting the Secretary of the Interior's Professional Qualification Standards for Archaeology and Historic Preservation as set forth at 62 FR 33708-33723 (June 20, 1997).

III. DURATION

This Memorandum of Agreement (MOA) will expire if its terms are not carried out within ten (10) years from the date of execution or upon project acceptance, whichever comes first if the FDOT has not completed all the terms and conditions within the MOA. Prior to expiration, the parties may agree to extend the timeframe for fulfillment of the terms by letter agreement.

IV. MOA DOCUMENTATION

A. The FDOT shall provide a summary of actions carried out pursuant to this MOA to the FDOT Office of Environmental Management (OEM) annually. The reporting period shall be the fiscal year from July 1st to June 30th and the summary shall describe the status of mitigation activities and, as applicable, any issues that may affect the ability of the FDOT to continue to meet the terms of this MOA, any disputes and objections received, and how they were resolved.

B. A Notice of Fulfillment will be prepared to summarize the implementation of the MOA after all stipulations have been fulfilled. This document will be submitted to OEM and SHPO for their files within six (6) months after completion of all MOA stipulations.

V. POST-REVIEW DISCOVERIES

A. If properties are discovered that may be historically significant, or if unanticipated effects on historic properties are found, FDOT shall implement the Post Review Discovery Plan established in Stipulation IX of the September 27, 2023 Programmatic Agreement among the ACHP, SHPO, and FDOT (2023 PA).

B. In the unlikely event that human skeletal remains or associated burial artifacts are uncovered within the project area during construction, all work in that area must stop. The individual in charge of the activity that leads to the discovery must notify the Project Engineer and the FDOT District 5 Cultural Resources Coordinator per Stipulation X of the 2023 PA. The discovery must be reported to local law enforcement and the appropriate medical examiner. The medical examiner will determine whether the State Archaeologist should be contacted per the requirements of Section 872.05, Florida Statutes, and Rule 1A-44.004, Florida Administrative Code (FAC).

VI. REVIEW STIPULATION

FDOT shall afford the SHPO and other consulting parties, including the federally recognized Tribes affiliated with Florida, a thirty (30) day period for review and comment following the receipt of delivery of those submittals and reviews described above. If no comments are received by FDOT at the end of these thirty (30) days, FDOT will presume there are no objections. Any objections to the findings or plans proposed in these submittals will be addressed in accordance with Stipulation VII, below.

VII. DISPUTE RESOLUTION

Should any signatory to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FDOT shall consult with such party to resolve the objection. If FDOT determines that such objection cannot be resolved, FDOT will:

A. Forward all documentation relevant to the dispute, including FDOT's proposed resolution, to the ACHP. The ACHP shall provide FDOT with its advice on the resolution of the objection within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FDOT shall prepare a written response that considers any timely advice or comments regarding the dispute from the ACHP, signatories, and concurring parties, and provide them with a copy of this written response. FDOT will then proceed according to its final decision.

B. Make a final decision on the dispute and proceed accordingly if the ACHP does not provide its advice regarding the dispute within thirty (30) days. Prior to reaching such a final decision, FDOT shall prepare a written response that considers any timely comments regarding the dispute from the signatories to the MOA, and provide them and the ACHP with a copy of the written response.

C. Fulfill its responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute.

VIII. AMENDMENTS

This MOA may be amended when such an amendment is agreed to in writing by all signatories. All signatories must signify their acceptance of the proposed changes to the MOA in writing within thirty (30) days of their receipt. The amendment will be effective on the date a copy signed by all signatories is filed with the ACHP. In accordance with 36 CFR § 800.6(b)(7), if the ACHP was not a signatory to the original agreement and the signatories execute an amended agreement, FDOT shall file the amended agreement with the ACHP.

IX. TERMINATION

If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other signatories in an effort to amend the MOA per Stipulation VII, above. If within thirty (30) days (or another time agreed to by all signatories) an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories.

Once the MOA is terminated, and prior to work continuing on the **Project**, FDOT must either (a) execute an MOA pursuant to 36 CFR § 800.6 or (b) request, take into account, and respond to the comments of the ACHP under 36 CFR § 800.7. FDOT shall notify the signatories as to the course of action it will pursue.

Execution of this MOA by FDOT and SHPO and implementation of its terms is evidence that FDOT has taken into account the effects of this undertaking on historic properties per the requirements of Section 106 (Public Law 113-287 [Title 54 U.S.C. 306108]), and 36 CFR Part 800 (Protection of Historic Properties).

SIGNATORIES:

FLORIDA STATE HISTORIC PRESERVATION OFFICER

Signed by:
Alissa Lotane
224D89ABEE884B3...

Alissa S. Lotane
Director, Division of Historical Resources
State Historic Preservation Officer

Date 10/29/2024 | 6:47 PM EDT

FLORIDA DEPARTMENT OF TRANSPORTATION

DocuSigned by:
Jennifer Marshall
D881D38487C64B2...

Jennifer Marshall, P.E.
Director, Office of Environmental Management

Date 10/30/2024 | 1:57 PM EDT

CONCURRING PARTIES:

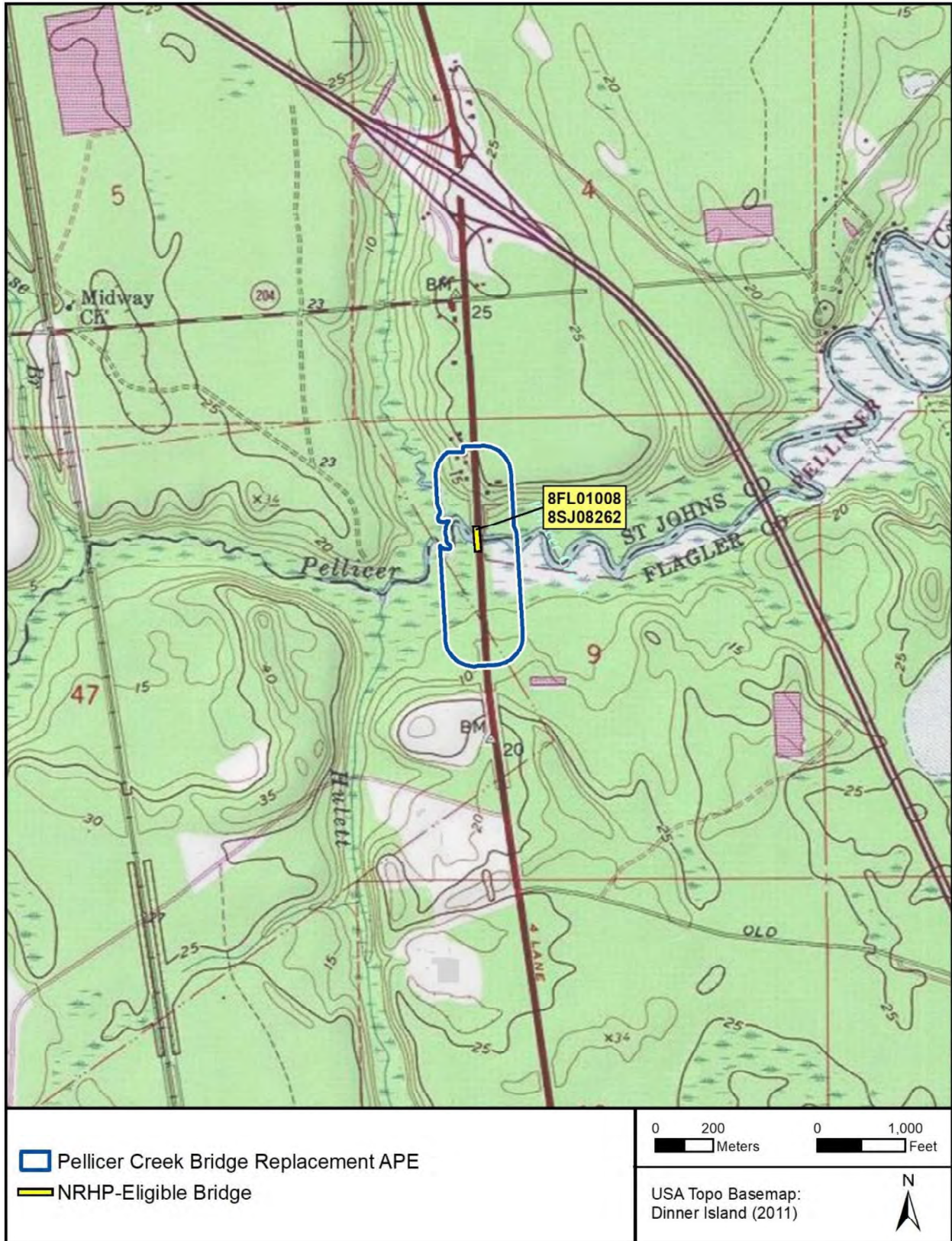
FLORIDA DEPARTMENT OF TRANSPORTATION, DISTRICT 5

DocuSigned by:
Adkins, Jack
1752524FC337485...

C. Jack Adkins
Director, Transportation Development

Date 09/25/2024 | 4:09 PM EDT

ATTACHMENT A



Location of NRHP-eligible bridge 8FL01008/8SJ08262.



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. Woodland Blvd.
DeLand, FL 32720

JARED W. PERDUE, P.E.
SECRETARY

June 23, 2022

Timothy A. Parsons, Ph.D.,
Director and State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attn: Ms. Alyssa McManus, Transportation Compliance Review Program

RE: Cultural Resource Assessment Survey
SR 5 Bridge Replacement over Pellicer Creek
Flagler and St. Johns Counties, Florida
Financial Management No.: 447118-1

Dear Dr. Parsons,

Enclosed please find one copy of the report titled *Cultural Resource Assessment Survey of the SR 5 Bridge Replacement over Pellicer Creek, Flagler and St. Johns Counties, Florida*. This report presents the findings of a CRAS conducted in support of proposed bridge replacements in Flagler and St. Johns Counties, Florida. The Florida Department of Transportation (FDOT), District 5, is proposing to replace both bridges carrying State Road (SR) 5 (US 1) over Pellicer Creek at the Flagler and St. Johns County line. The project also includes minor roadway work limited to that necessary to replace the bridges and modification of the existing drainage system as required to complete proposed roadway reconstruction work. The project includes widening of the existing shoulders from 1.2 meters (m) (4.0 feet [ft]) to 1.5 m (5.0 ft) along the roadway segment and 3.0 m (10.0 ft) outside the shoulder on the bridge. An easement across sovereign submerged lands is required to accommodate construction. This project is federally funded for construction in 2026.

The project Area of Potential Effect (APE) was defined as the existing right-of-way (ROW) from approximately 335 m (1,100 ft) south of the Flagler and St. Johns County line to 188 m (617 ft) north of the line. The APE extends to the back or side property lines of parcels adjacent to the ROW or no more than 100 m (330 ft) from the ROW line. SEARCH conducted the archaeological survey within the existing ROW and the architectural survey within the entire APE.

This CRAS was conducted in accordance with the requirements set forth in Section 106 of the National Historic Preservation Act of 1966, as amended, found in 36 CFR Part 800 (Protection of Historic Properties). The studies also comply with Chapter 267 of the Florida Statutes and Rule

Dr. Parsons, SHPO
FM #447118-1
June 23, 2022
Page 2

Chapter 1A-46, Florida Administrative Code and Section 267.12, Florida Statutes, Chapter 1A-32. All work was performed in accordance with Part 2, Chapter 8 of FDOT's PD&E Manual (revised July 2020), FDOT's Cultural Resources Management Handbook, and the standards stipulated in the Florida Division of Historical Resources' (FDHR) *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The Principal Investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 FR 44716-42). This study also complies with Public Law 113-287 (Title 54 U.S.C.), which incorporates the provisions of the National Historic Preservation Act of 1966, as amended, and the Archeological and Historic Preservation Act of 1974, as amended.

The archaeological survey included the excavation of three shovel tests within the SR 5 (US 1) ROW. SEARCH identified evidence of disturbance from bridge and road construction during subsurface testing. Archaeological testing was not possible along the eastern extent of the ROW due to the presence of multiple marked buried utilities. No archaeological sites, features, or artifact occurrences were encountered during the archaeological survey. No further archaeological work is therefore recommended according to current design. If design changes occur to include areas outside of the current study, additional archaeological consideration may be required.

The architectural survey resulted in identification and evaluation of five historic resources within the Pellicer Creek Bridge Replacement APE, one of which is previously recorded and four of which are newly recorded. The previously recorded resource is a historic roadway (8FL00291/8SJ05271) that was recorded elsewhere within both Flagler and St. Johns Counties but not surveyed within the current project APE. The newly recorded resources consist of one highway bridge (8FL01008/8SJ08262), two buildings (8SJ07379 and 8SJ07380), and one resource group (8SJ07359).

The SHPO evaluated previously recorded resource US 1 (8FL00291/8SJ05271) to be ineligible for listing in the NRHP approximately 3 mi south of the current project area (SEARCH 2018). Based on the results of the current survey, it is the opinion of SEARCH that the segment of US 1 (8FL00291/8SJ05271) within the APE is also ineligible for listing in the NRHP due to loss of historic integrity.

The newly recorded historic bridge, FDOT Bridge No. 730008 (8FL01008/8SJ08262), is one of the oldest remaining bridges along the US 1 corridor in Florida. Based on the current survey, Resource 8FL01008/8SJ08262 is recommended eligible for listing in the NRHP under Criterion A for its role in Florida's transportation history. The bridge is also recommended eligible for listing in the NRHP under Criterion C as a good example of an early tee-beam highway bridge. The three remaining resources within the APE (8SJ07359, 8SJ07379, and 8SJ07380) are recommended ineligible for listing in the NRHP due to a lack of significant historic associations and architectural or landscape distinction.

Contingent upon the SHPO's concurrence with the eligibility recommendations for historic resources presented in this CRAS, a separate Section 106 case study will be prepared to evaluate

Dr. Parsons, SHPO
FM #447118-1
June 23, 2022
Page 3

any project-related effects to the NRHP-eligible Bridge No. 730008 (8FL01008/8SJ08262). The resolution of project-related effects, if any, will be the subject of further agency consultation.

I respectfully request your concurrence with the findings of the enclosed report.

If you have any questions or need further assistance, please contact Catherine Owen, District Cultural Resource Coordinator, at (386) 943-5383 or me at (386) 943-5411.

Sincerely,

For: William G. Walsh
Environmental Manager
FDOT, District Five

The Florida State Historic Preservation Officer finds the attached Cultural Resource Assessment Survey Report complete and sufficient and concurs / does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 2022-4228. Or, the SHPO finds the attached document contains _____ insufficient information.

In accordance with the Programmatic Agreement among the ACHP, SHPO and FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida, if providing concurrence with a finding of No Historic Properties Affected for a project as a whole, or to No Adverse Effect on a specific historic property, SHPO shall presume that FDOT may approve the project as de minimis use under Section 4(f) under 23 CFR 774.

SHPO Comments:

Kelly L. Chase, Digitally signed by Kelly L. Chase, DSHPO
DN: cn=Kelly L. Chase, DSHPO, o, ou,
email=kelly.chase@dos.myflorida.com,
c=US
Date: 2022.07.06 10:38:32 -0400

7.6.2022

Timothy A. Parsons, PhD, Director
Florida Division of Historical Resources

Date



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. Woodland Blvd.
DeLand, FL 32720

JARED W. PERDUE, P.E.
SECRETARY

October 4, 2023

Alissa S. Lotane,
Director and State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attn: Mr. Benjamin Stewart, Transportation Compliance Review Program

RE: Section 106 Consultation Case Study Report
State Road 5/US 1 Bridge Replacement over Pellicer Creek
Flagler and St. Johns Counties, Florida
Financial Management No.: 447118-1-52-01

Dear Ms. Lotane,

Enclosed, please find one copy of the *Section 106 Consultation Case Study Report for the State Road 5/US 1 Bridge Replacement over Pellicer Creek, Flagler and St. Johns Counties, Florida*. The Florida Department of Transportation (FDOT), District 5, is proposing to replace both bridges carrying State Road (SR) 5/US 1 over Pellicer Creek at the Flagler and St. Johns County line. The project also includes minor roadway work limited to replacing the bridges and modification of the existing drainage system as required to complete proposed roadway reconstruction work. An easement across sovereign submerged lands is required to accommodate construction. The bridges are functionally obsolete, and because rehabilitation and reuse of the structures are not possible given their current condition, the project is proposing the replacement of both bridges to ensure safe travel and maintain connectivity along this portion of SR 5/US 1.

A Phase I cultural resource assessment survey for the preferred alternative was completed by SEARCH in June 2022. The cultural resource assessment survey and subsequent consultation with the State Historic Preservation Officer concluded that one National Register of Historic Places-eligible historic property is within the project area of potential effects. The effects assessment and alternatives analysis addresses project-related effects relative to this NRHP-eligible resource, FDOT Bridge No. 730008 (8FL01008/8SJ08262). Three alternatives in addition to a rehabilitation alternative and no-build alternative were considered prior to choosing the selected alternative.

www.fdot.gov

Ms. Lotane, SHPO
FM # 447118-1-52-01
October 4, 2023
Page 2

As a federally funded project, the effects evaluation was conducted to comply with Public Law 113-287 (Title 54 US Code), which incorporates the provisions of the National Historic Preservation Act of 1966, as amended, and the Archeological and Historic Preservation Act of 1974, as amended. The study also meets the regulations for implementing National Historic Preservation Act Section 106 found in 36 Code of Federal Regulations Part 800 (*Protection of Historic Properties*). This study also complies with Chapter 267 of the Florida Statutes and Rule Chapter 1A-46, Florida Administrative Code. SEARCH performed all work in accordance with Part 2, Chapter 8 of the FDOT's Project Development & Environment Manual (revised July 2023) and the Florida Division of Historical Resources' recommendations for such projects as stipulated in *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The principal investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 Federal Register 44716-42).

Based on a review of the project plans, it is the district's opinion that the project will result in an adverse effect to FDOT Bridge No. 730008 (8FL01008/8SJ08262). Because construction of the selected alternative will result in an adverse effect to this historic property, FDOT proposes to prepare state equivalent Level III Historic American Engineering Record documentation for 8FL01008/8SJ08262. If your office agrees with this proposed mitigation strategy, FDOT will prepare a draft Memorandum of Agreement to memorialize this commitment and circulate for your review.

I respectfully request your concurrence with the findings of the enclosed report and the proposed mitigation strategy described above.

If you have any questions or need further assistance, please contact Catherine Owen, District Cultural Resource Coordinator, at (386) 943-5383 or me at (386) 943-5436.

Sincerely,



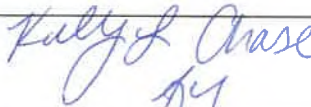
Casey Lyon, MS
Environmental Manager
FDOT, District Five

Ms. Lotane, SHPO
FM # 447118-1-52-01
October 4, 2023
Page 3

The Florida State Historic Preservation Officer finds the attached Section 106 Consultation Case Study Report complete and sufficient and concurs / does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 2023-4979B. Or, the SHPO finds the attached document contains _____ insufficient information.

In accordance with the Programmatic Agreement among the ACHP, SHPO and FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida, if providing concurrence with a finding of No Historic Properties Affected for a project as a whole, or to No Adverse Effect on a specific historic property, SHPO shall presume that FDOT may approve the project as de minimis use under Section 4(f) under 23 CFR 774.

SHPO Comments:



Alissa S. Lotane, Director
Florida Division of Historical Resources

10.25.2023
Date

Section 4(f) Resources

Florida Department of Transportation

SR 5/US 1 OVER PELLICER CREEK BRIDGE REPLACEMENT

District: FDOT District 5

County: Flagler County

ETDM Number: N/A

Financial Management Number: 447118-1-32-01

Federal-Aid Project Number: D521-060-B

Project Manager: Jeanette Maldonado-Ambler

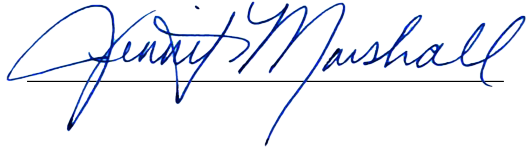
The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022 and executed by the Federal Highway Administration and FDOT. Submitted pursuant 49 U.S.C. § 303.

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Summary and Approval

Resource Name	Facility Type	Property Classification	Owner/Official with Jurisdiction	Recommended Outcome	OEM SME Action
Bridge #730008	Historic Bridge	Historic Site	State Historic Preservation Office	Programmatic	Concurrence 01-16-2025



February 18, 2025

Director of the Office of Environmental Management
Florida Department of Transportation

Bridge #730008

Facility Type: Historic Bridge

Property Classification: Historic Site

Address and Coordinates:

Address: carries southbound US 1 over Pellicer Creek at Flagler County and St. Johns County line

Latitude: 29.651369 Longitude: -81.286854

Description of Property:

Bridge #730008 is a 1927 tee beam bridge spanning Pellicer Creek at the Flagler County and St. Johns County line. The bridge carries two lanes of southbound traffic on State Route (SR) 5 / US Route 1 (SR 5/ US 1). It is approximately 68.2 m (225 ft) long with 10m (33 ft) deck width. Cantilevered post and beam concrete rails line each side of the bridge, tying into galvanized metal guardrail at each end. The seven span tee beam superstructure is comprised of continuous cast-in-place concrete and is supported by pile bent formations. Each formation features six concrete pile bents.

The bridge was altered in 1948 when it was widened by 4 m (13 ft) to its current configuration. There is no bridge plaque; however, the rail is stamped with "1948", documenting the widening. The bridge carried both north and south traffic until 1957 when bridge #7300045 was constructed parallel to bridge #730008. The construction of a second bridge was the result of US 1 becoming a divided highway. Since that time, bridge #730008 has carried southbound traffic only. Pile jackets were added to the bents ca. 1977, resulting in larger dimensions.

Bridge #730008 was evaluated as eligible for listing in the NRHP under Criteria A and C for its significance in Engineering and Transportation. The bridge is an early example of a tee beam bridge. The widening effort in 1948 did not obscure the girder and beam configurations that are distinctive with this type. Tee beam bridges were known to be cost effective and easy-to-construct bridges, providing state transportation agencies with easy solutions for the rapid emergence and expansion of the automobile age. The bridge is also representative of the early efforts of transportation engineering in Florida of which there are few examples remaining on US Route 1. Bridge #730008 is the oldest FDOT bridge in Flagler County and the second oldest in in St. Johns County. Its period of significance is 1927.

Owner/Official with Jurisdiction: State Historic Preservation Office

Recommended Outcome: Programmatic (Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges)

Describe in detail how the Section 4(f) property will be used.

Bridge #730008 was evaluated to be structurally deficient and functionally obsolete per the 2023 Bridge Development Report, located in the project file. FDOT proposes to replace the northbound and southbound bridges over Pellicer Creek. The northbound bridge, #730045, was built in 1957 and is not a Section 4(f) resource; however, the bridge's need for improvement influences the alternatives for Bridge #730008. FDOT determined that the action for the bridges would be considered as a crossing, meaning that regardless of alternative, the action at both bridges would be the same.

This section describes the Section 4(f) use of Bridge #730008 that would result from the construction of the Preferred Alternative as well as descriptions of additional alternatives considered. Some details about #730045 which are germane to the alternatives analysis are included.

Preferred Alternative (Bridge Replacement) - The structurally deficient and functionally obsolete Bridge #730008 would be demolished and replaced with one that meets current FDOT design standards. No element of Bridge #730008 would remain on this alignment and its materials will be disposed of. While the bridge is currently incorporated into a transportation facility, transportation projects that result in a finding of adverse effect to historic properties under Section 106 of the NHPA, are also considered to use the Section 4(f) resource. The Section 106 Case Study, SHPO concurrence, and the signed Memorandum of Agreement located in the project file, and attached herein, as appropriate. The new bridge is proposed as slightly wider to increase inside and outside shoulder width and would tie into the existing road geometry. Plan sheets are included in the Section 106 Case Study.

No Build, Build on a New Location, and Rehabilitation would not result in Section 4(f) use of the bridge; however, the purpose and need of the project would not be met. These alternatives are described below.

No Build - This alternative would take no action to correct the deficiencies at the crossing. Bridge #730008 would remain in situ and would continue to provide an insufficient, substandard crossing of Pellicer Creek. If the No Build alternative was selected, the integrity of the historic bridge would continue to convey the significance for which it is eligible for listing in the NHRP; however, this alternative would retain in place a "functionally obsolete" bridge beyond its service life. It would not improve the condition, including but not limited to, intolerable deck geometry of the important transportation corridor in a manner that allows safe and efficient crossing of Pellicer Creek and which meet current and predicted vehicular traffic volumes.

Build on New Location - Building new bridges in new locations would not address the deficiencies of bridge #730008. In this alternative, the bridge would likely be closed to traffic and remain in-situ. US 1/SR 5 traffic would be removed from the historic original alignment. This alternative would require FDOT to maintain a closed-to-traffic historic bridge according to the Secretary of Interior's Standards and Guidelines for the Treatment of Historic Properties (SOI Standards). Given the nature of the site and current bridge condition, it is unlikely the bridge could be used as a bicycle/pedestrian facility. The bridge would have no or limited maintenance which would not improve the condition of the bridge. Lack of maintenance would lead to eventual structural failure and collapse, resulting in demolition by neglect. The bridge site has a history of settlement affecting the ride quality and potentially impacting the existing bridges' load carrying capacity and stability. Constructing new bridges adjacent to the existing bridges could cause the existing bridges to have excessive long-term settlement and stability issues.

Additionally, the site has insufficient right of way for building new bridges in new locations. At the bridge site, both sides of the right of way are bordered by environmentally sensitive and protected lands including St. Johns River Water Management District's (SJRWMD) Pellicer Creek Conservation Area and the Guana Tolomato Matanzas National Estuarine Research Reserve. The Pellicer Creek Conservation Area provides protection over acres of regionally significant and protected lands and is actively managed by SJRWMD. The project is also located within the research reserve and is within the Pellicer Creek Aquatic Preserve. This outstanding Florida water is designated by the Florida Department of Environmental Protection (FDEP) as warranting protection due to its natural or rare characteristics. The habitats provided within these areas are essential to many federally and state listed species. Pellicer Creek adjacent to the bridge is also considered Essential Fish Habitat (EFH) which provides important protections to fisheries.

Bridge Rehabilitation - This alternative considers rehabilitation efforts that would not result in adverse effects to the bridge. The distinctive character of the tee beam bridge, including the girders and beams, would remain recognizable. The bridge rehabilitation would focus on the substructure. It would not address efflorescence and water intrusion at the standard deck. Crutch bents would be installed as a second foundation, using transverse beams to support the superstructure.

While crutch bents have been successfully used in District Five, this location presents unique increased challenges due to space limitations. There are previously abandoned timber piles within the channel that would remain in situ and the installation of additional substructure elements within the channel may constrict the flow of Pellicer Creek. These conditions would be exacerbated by the need for crutch bents at the northbound bridge. Additionally, it would retain sub-standard concrete rail in place. It would not correct the functional and geometric deficiencies. Rehabilitation would not meaningfully extend the bridge's design life and would retain a nearly 100-year-old bridge carrying traffic on one of the busiest highways in the US Highway System.

Applicability

Yes No

Does the project meet all of the following criteria?

1. The bridge is to be replaced or rehabilitated with Federal funds.
2. The project will require the use of a historic bridge structure which is on or is eligible for listing on the National Register of Historic Places.
3. The bridge is not a National Historic Landmark.
4. FDOT has determined that the facts of the project match those set forth in the sections below labeled Alternatives, Findings, and Measures to Minimize Harm.
5. Agreement among FDOT, the State Historic Preservation Officer (SHPO), and the Advisory Council on Historic Preservation (ACHP), if participating, has been reached through procedures pursuant to Section 106 of the NHPA.

Alternatives and Findings

1. No Build: The No Build Alternative has been studied and does not meet the Section 4(f) prudent and feasible standard. The No Build Alternative is not recommended based on the following:

- **Structural Deficiencies:** The No Build Alternative does not correct the situation that causes the bridge to be considered structurally deficient or significantly deteriorated. These deficiencies can lead to eventual structural failure/collapse. Normal maintenance is not considered adequate to address these deficiencies.
- **Functional/Geometric Deficiencies:** The No Build Alternative does not correct the situation that causes the bridge to be considered functionally/geometrically deficient. These deficiencies can lead to safety hazards to the traveling public or place unacceptable restrictions on transport and travel.

2. Build on New Location Without Using the Old Bridge: This alternative has been studied and does not meet the Section 4(f) prudent and feasible standard. The New Location Alternative is not recommended based on the following:

- **Structural Deficiencies:** The New Location Alternative does not correct the situation that causes the bridge to be considered structurally deficient or significantly deteriorated. These deficiencies can lead to eventual structural failure/collapse. Normal maintenance is not considered adequate to address these deficiencies.
- **Functional/Geometric Deficiencies:** The New Location Alternative does not correct the situation that causes the bridge to be considered functionally/geometrically deficient. These deficiencies can lead to safety hazards to the traveling public or place unacceptable restrictions on transport and travel.

3. Rehabilitation Without Affecting the Historic Integrity of the Bridge: This alternative has been studied and does not meet the Section 4(f) prudent and feasible standard. The Rehabilitation Alternative is not recommended based on the following:

- **Structural Deficiencies:** The Rehabilitation Alternative does not correct the situation that causes the bridge to be considered structurally deficient or significantly deteriorated. These deficiencies can lead to eventual structural failure/collapse. Normal maintenance is not considered adequate to address these deficiencies.
- **Functional/Geometric Deficiencies:** The Rehabilitation Alternative does not correct the situation that causes the bridge to be considered functionally/geometrically deficient. These deficiencies can lead to safety hazards to the traveling public or place unacceptable restrictions on transport and travel.

4. Replacement: The Replacement Alternative has been studied and is determined to meet the Section 4(f) prudent and feasible standard. The Replacement Alternative is recommended based on the following:

- **Structural Deficiencies:** The Replacement Alternative corrects the situation that causes the bridge to be considered structurally deficient or significantly deteriorated.
- **Functional/Geometric Deficiencies:** The Replacement Alternative corrects the situation that causes the bridge to be considered functionally/geometrically deficient.

Measures to Minimize Harm

- For bridges that are to be rehabilitated, the historic integrity of the bridge is preserved, to the greatest extent possible, consistent with unavoidable transportation needs, safety, and load requirements;
- For bridges that are to be rehabilitated to the point that the historic integrity is affected or that are to be moved or demolished, FDOT ensures that, in accordance with the Historic American Engineering Record (HAER) standards, or other suitable means developed through consultation, fully adequate records are made of the bridge;
- For bridges that are to be replaced, the existing bridge is made available for an alternative use, provided a responsible party agrees to maintain and preserve the bridge; and
- For bridges that are adversely affected, agreement among the SHPO, FDOT, and ACHP (if participating in consultation) is reached through the Section 106 process of the NHPA on measures to minimize harm and those measures are incorporated into the project. This programmatic Section 4(f) evaluation does not apply to projects where such an agreement cannot be reached.

The proposed project meets all the applicable criteria set forth by the Federal Highway Administration's (FHWA) Guidance on Programmatic Section 4(f) Evaluation and Approval for FHWA Projects Which Necessitate the Use of Historic Bridges (23 CFR Part 774). All alternatives set forth in the subject programmatic evaluation were fully analyzed and the findings made are clearly applicable to this project. There are no feasible and prudent alternatives to the use of the historic bridge, and the project includes all possible planning to minimize harm.

Public Involvement Activities:

A public engagement plan/community action awareness plan was formalized on April 27, 2022.

- Stakeholder meetings were held on June 28, 2022, and March 21, 2023.
- Individual public meetings with adjacent properties were invited to participate in the March 21, 2023 stakeholders meeting.
- The public was invited to the March 21, 2023 meeting.

The 30% design plans were provided to local government stakeholders by the Design Consultant, Florida Bridge and Transportation, on June 6, 2023.

- The St. Johns County Certified Local Government (CLG) representative commented on the historic nature of the bridge. The CLG asked if it was feasible to save a bridge rail post with a date stamp. However, during the Section 106 process it was determined that the date is stamped on an element of the bridge constructed in 1948, which is outside the bridge's period of significance. Thus, the date stamp is not a significant feature of the bridge. FDOT provided this

clarification to the CLG, and they subsequently stated they have no further concerns on January 19, 2024.

- As of October 2023, the public outreach conducted by FDOT's design team in support of the design is complete.
- Coordination regarding the public engagement strategy in support of the PD&E Study was discussed between District 5 and OEM and agreed to on October 3, 2023.
- The approved PD&E engagement approach is to publish a notice of opportunity that provides the opportunity for the public to request a public hearing. The following apply to the notice of opportunity:

The notice of opportunity was published on Friday, August 16, 2024. The notice of opportunity provided 15 days following the publication of the notice for the public to submit a written request to the FDOT Project Manager to request a public hearing for this project. The notice of opportunity period closed on Saturday, August 31, 2024.

The notice was published in the Daytona Beach News Journal (the local newspaper), in the Florida Administrative Record, and on FDOT's public meeting notices website. It was also included on CFLRoads on the project website at

<https://www.cflroads.com/project/447118-1>.

At the same time that the notice was published, the Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation were made available for public review on the project website. FDOT did not receive any requests for a public hearing nor inquiries about the project during the notice of opportunity period. Therefore, no public hearing is required. Supporting documentation of the notice of opportunity publications is included in the Attachment.

FDOT D5 presented initial results of the project to the SHPO and Muscogee (Creek) Nation (MCN) in a CRAS. The SHPO concurred with the CRAS on July 6, 2022, and no response was received from the MCN. FDOT D5 presented the Case Study Report to SHPO and MCN in October 2023. The Case Study Report concluded with the Adverse Effects to the bridge and proposed mitigation measures. SHPO issued concurrence on October 25, 2023 (attached). No response was received from the MCN. The process concluded with the execution of a MOA between FDOT and SHPO documenting the stipulations for mitigation resulting from the adverse effects to NR-eligible bridge #730008 on October 30, 2024.

OEM SME Concurrence Date: 01-16-2025

Project-Level Attachments

None

Resource Attachments

Bridge #730008

T22083_APE_Topo

Project Information Handout

Project Information Session presentation

SHPO Section 106 Case Study Concurrence Letter

447118-1_SR5_US1_Pellicer Creek Bridge_Effects_Transmittal Letter_Muscogee Nation

447118-1 US1_Pellicer Creek Notices 8-16-2024 (1)

447118-1_FDOT MOA_PellicerCreekBridge_FINAL_signed

447118-1 Section 4(f) July 2022

447118-1_SR5_US1_Pellicer Creek Bridge_SEARCH_Effects Eval_RAI_FINAL_rev_no attachments

Bridge #730008

Contents:

T22083_APE_Topo

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Project Information Session presentation

SHPO Section 106 Case Study Concurrence Letter

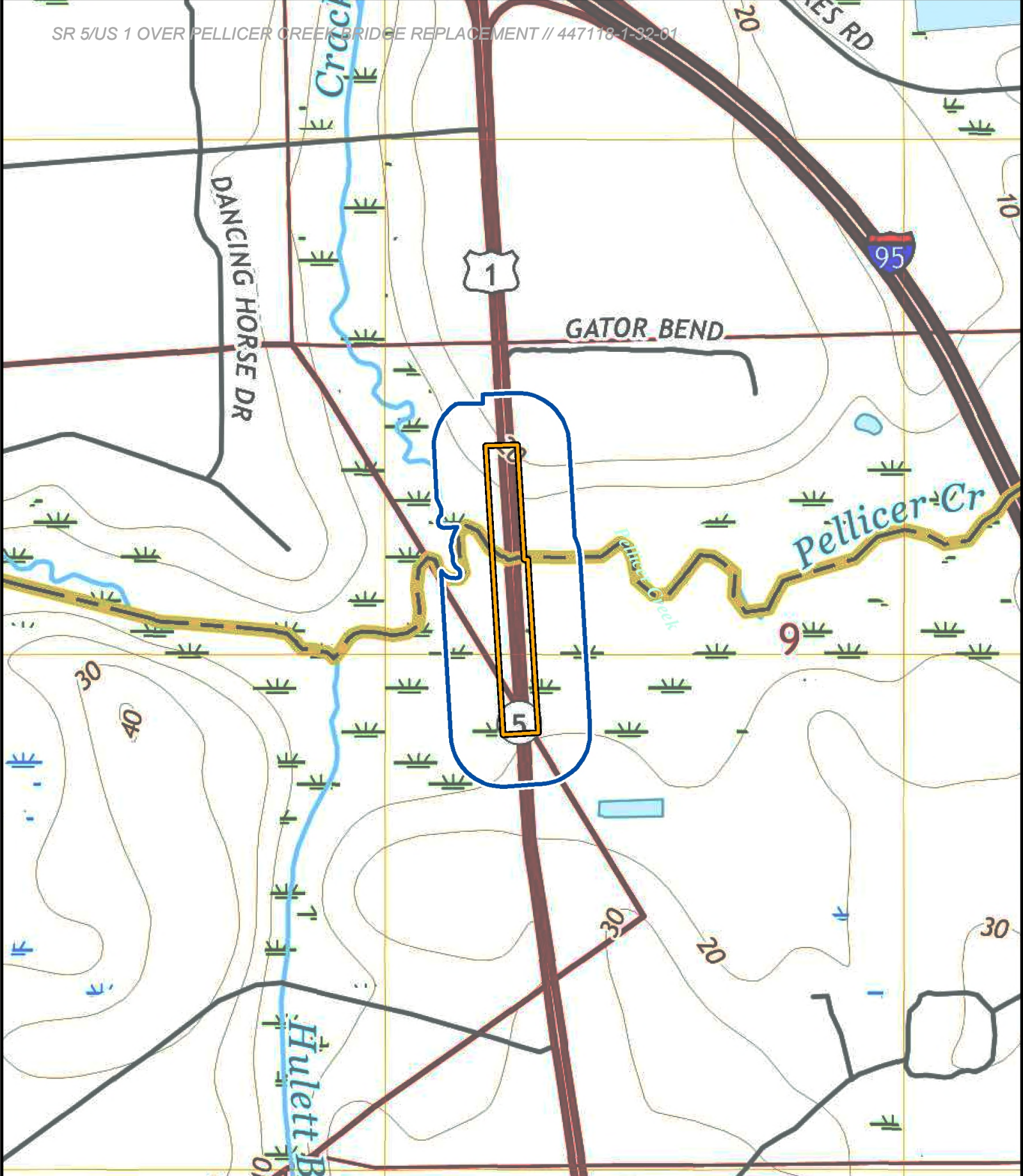
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447118-1_FDOT MOA_PellicerCreekBridge_FINAL_signed

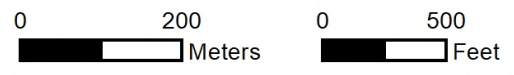
447118-1 Section 4(f) July 2022

447118-1_SR5_US1_Pellicer Creek Bridge_SEARCH_Effects Eval_RAI_FINAL_rev_no attachments



- Pellicer Creek Bridge Replacement APE
- Pellicer Creek Bridge Replacement ROW

Special Use Easement



USGS 7.5' Quadrangle Map -
 Dinner Island (2018) Page 60 of 746

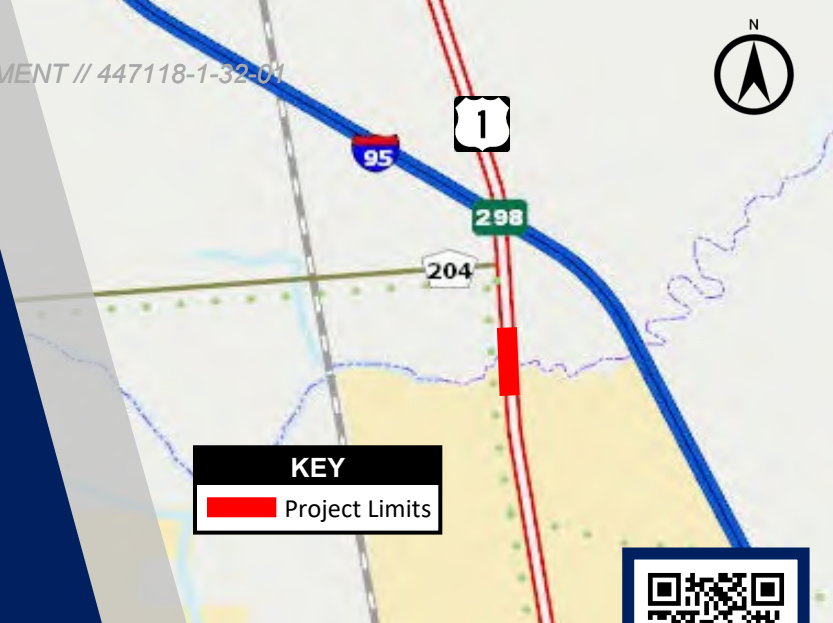




U.S. 1 Bridge Replacement Over Pellicer Creek

Flagler County / St. Johns County

Financial Project Identification (FPID) No.: 447118-1



KEY
 Project Limits



Project Description

The Florida Department of Transportation (FDOT) is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County/St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area.

The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements.

The bridges will be constructed in phases so that at least one travel lane in each direction of U.S. 1 remains open at all times.

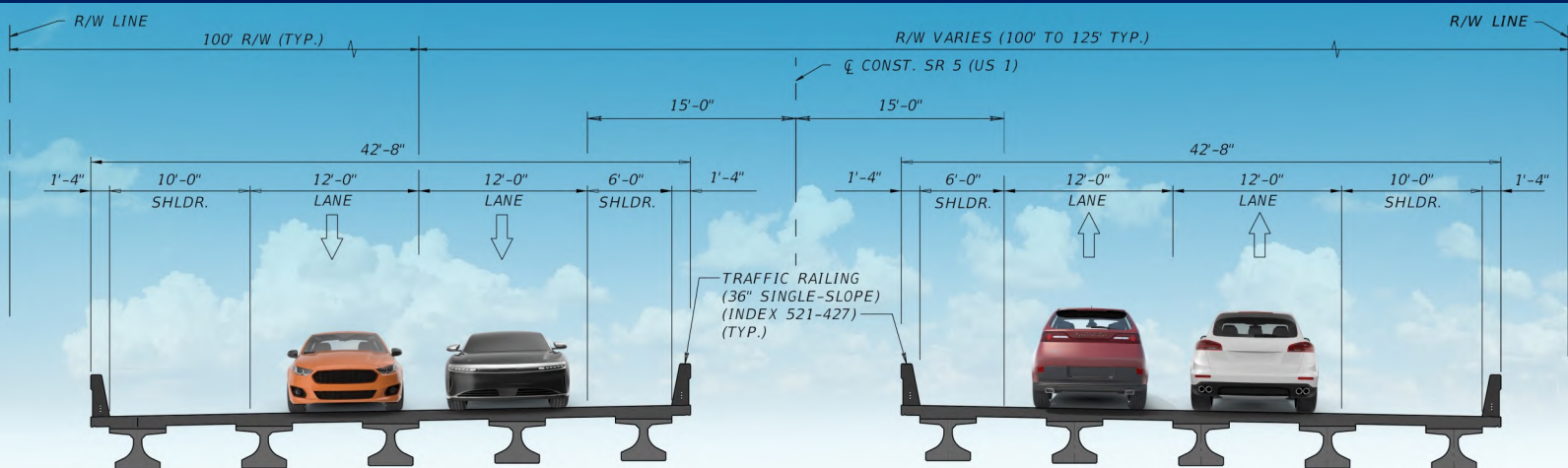
Project Status and Estimated Costs*

Design:	Ongoing - fall 2023	\$ 1.7 Million
Right of Way:	Within existing	--
Construction:	Funded - summer 2026	\$ 9.3 Million

*subject to change

Contact:

Shelley ChinQuee
 FDOT Project Manager
 386-943-5439
 Shelley.ChinQuee@dot.state.fl.us





U.S. 1 Bridge Replacement over Pellicer Creek

Stakeholder Meeting

Financial Project Identification (FPID) No.: 447118-1

Flagler and St. Johns County
March 21, 2023





Project Location

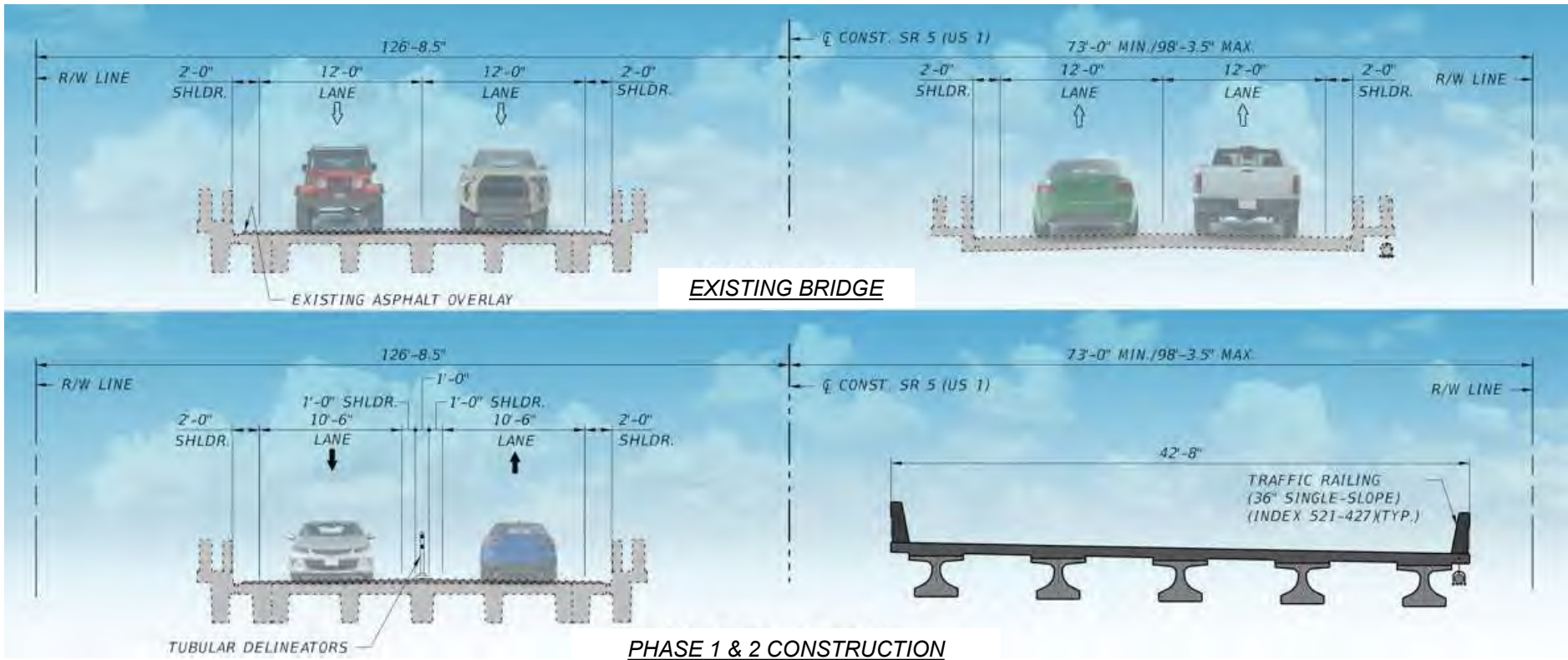


Proposed Improvements

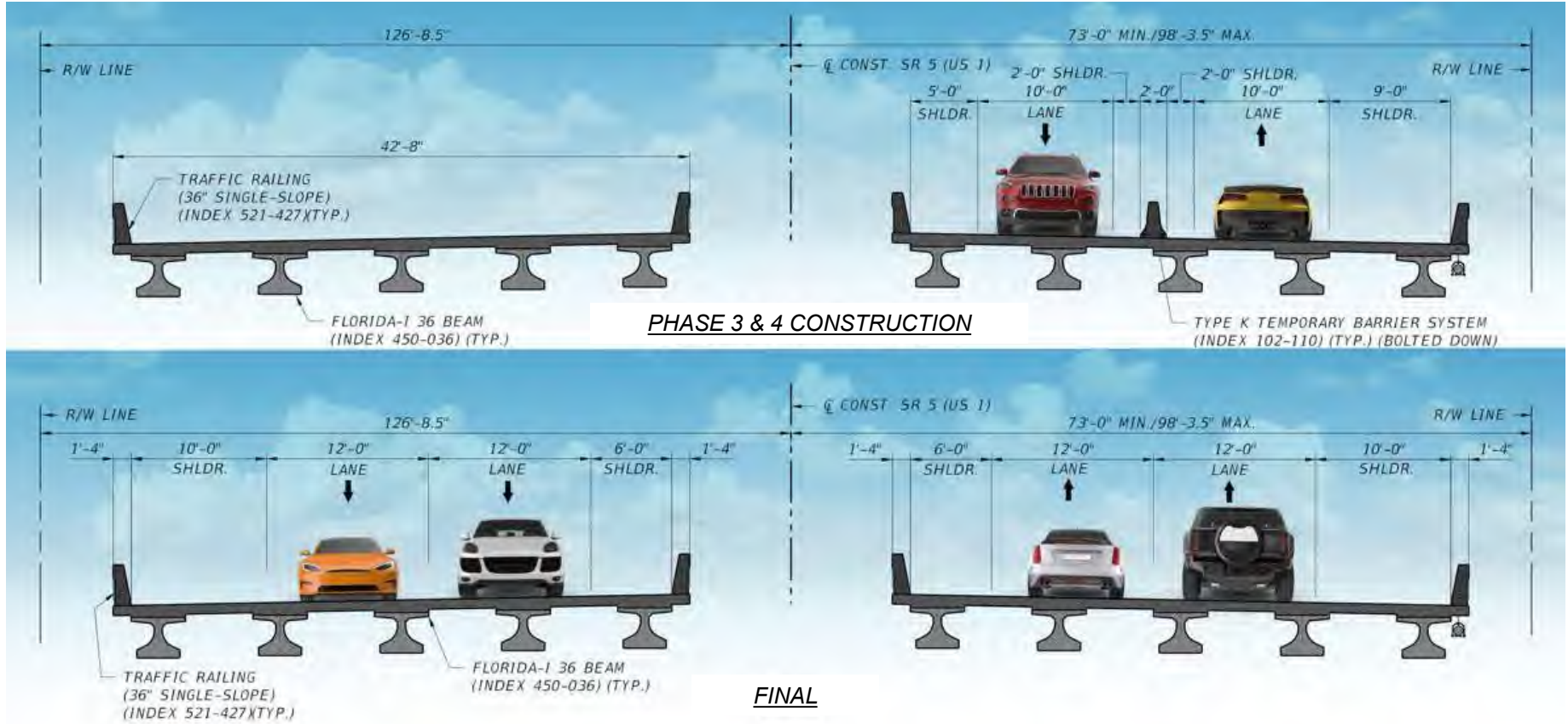


- Replacement of SB bridge (Bridge Number 730008)
 - Built in 1927/ Widened 1948
 - 96 years old
- Replacement of NB bridge (Bridge Number 730045)
 - Built in 1957
 - 66 years old
- Roadway reconstruction at bridge approaches
- Minor drainage improvements

Construction Phasing

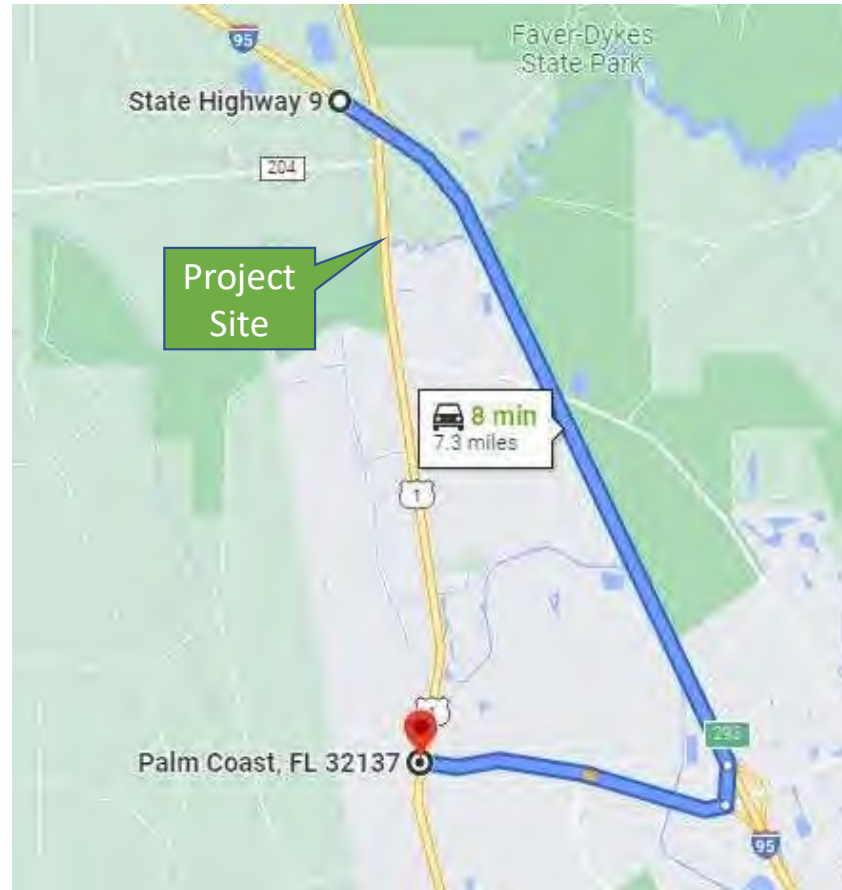
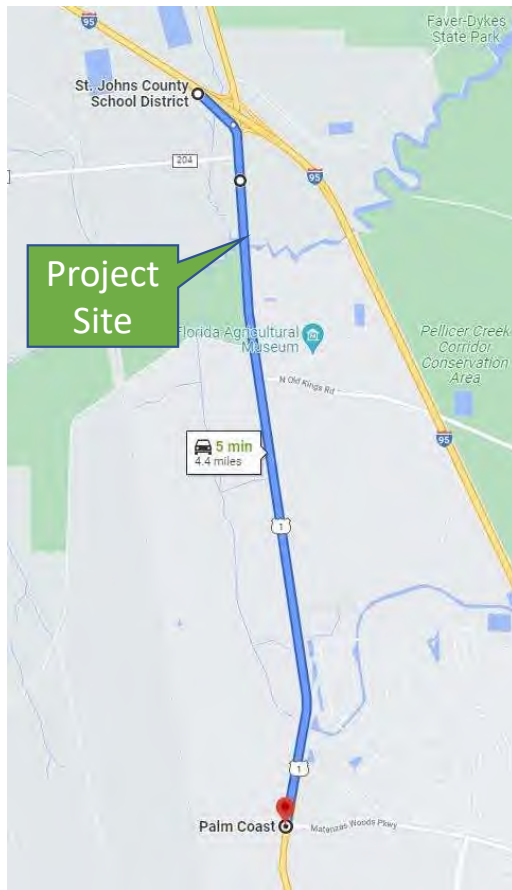


Construction Phasing



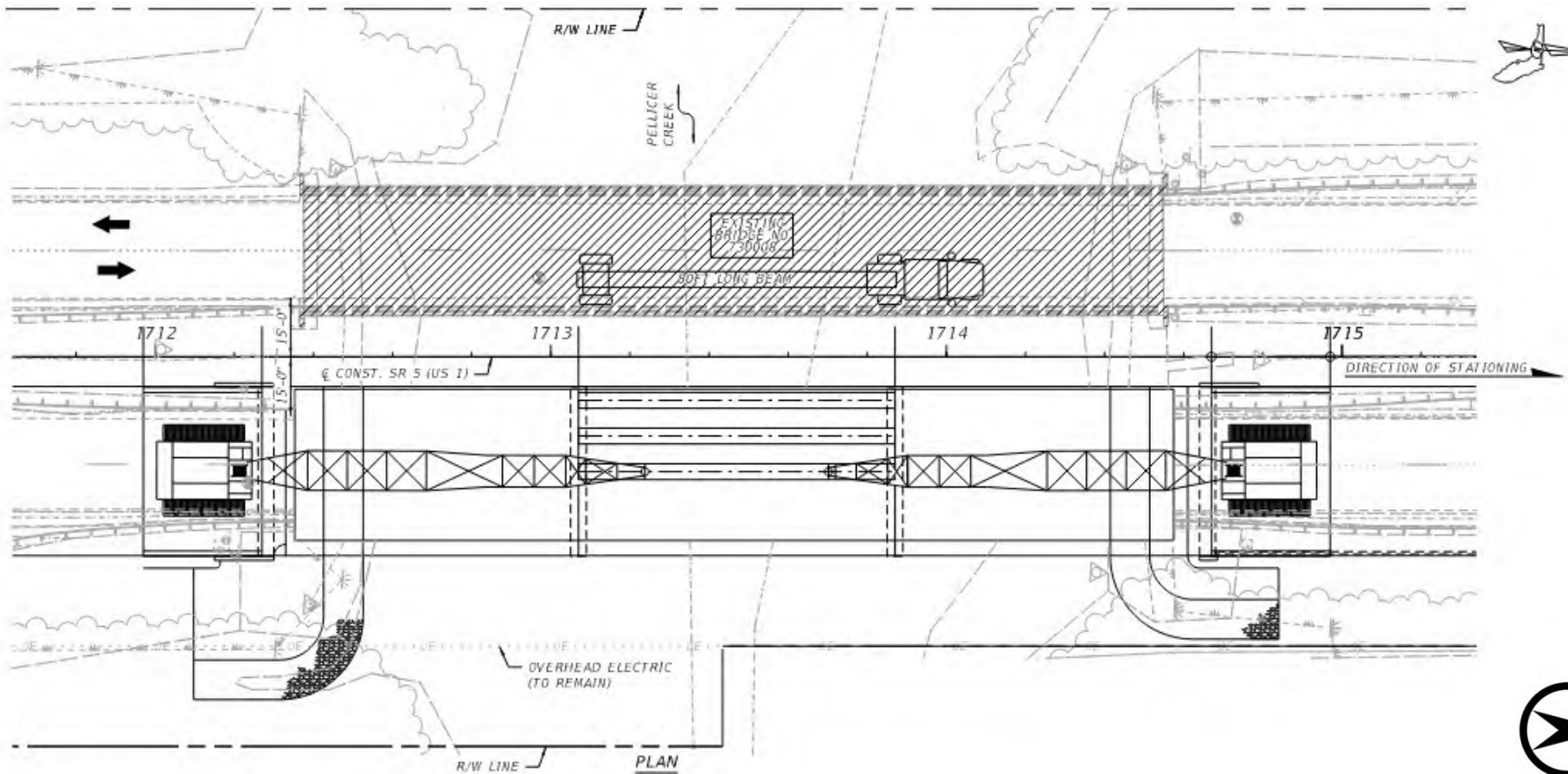


Detour Exists if Needed



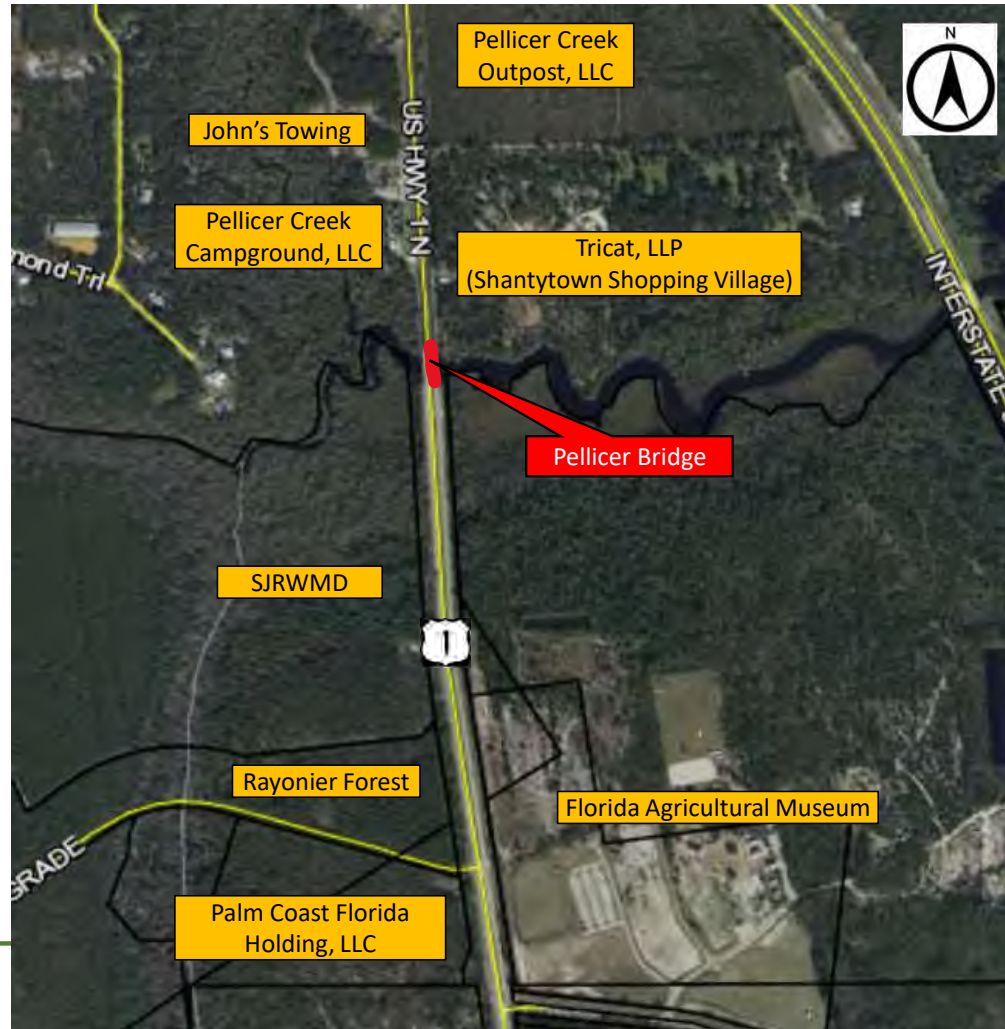
- Detour route can accommodate oversized vehicles
- Normal travel time – 5 min
- Detour travel time – 8 min
- Brief bridge closure for beam setting
- Utilizing I-95 and Matanzas Woods Parkway

Beam Setting Operations

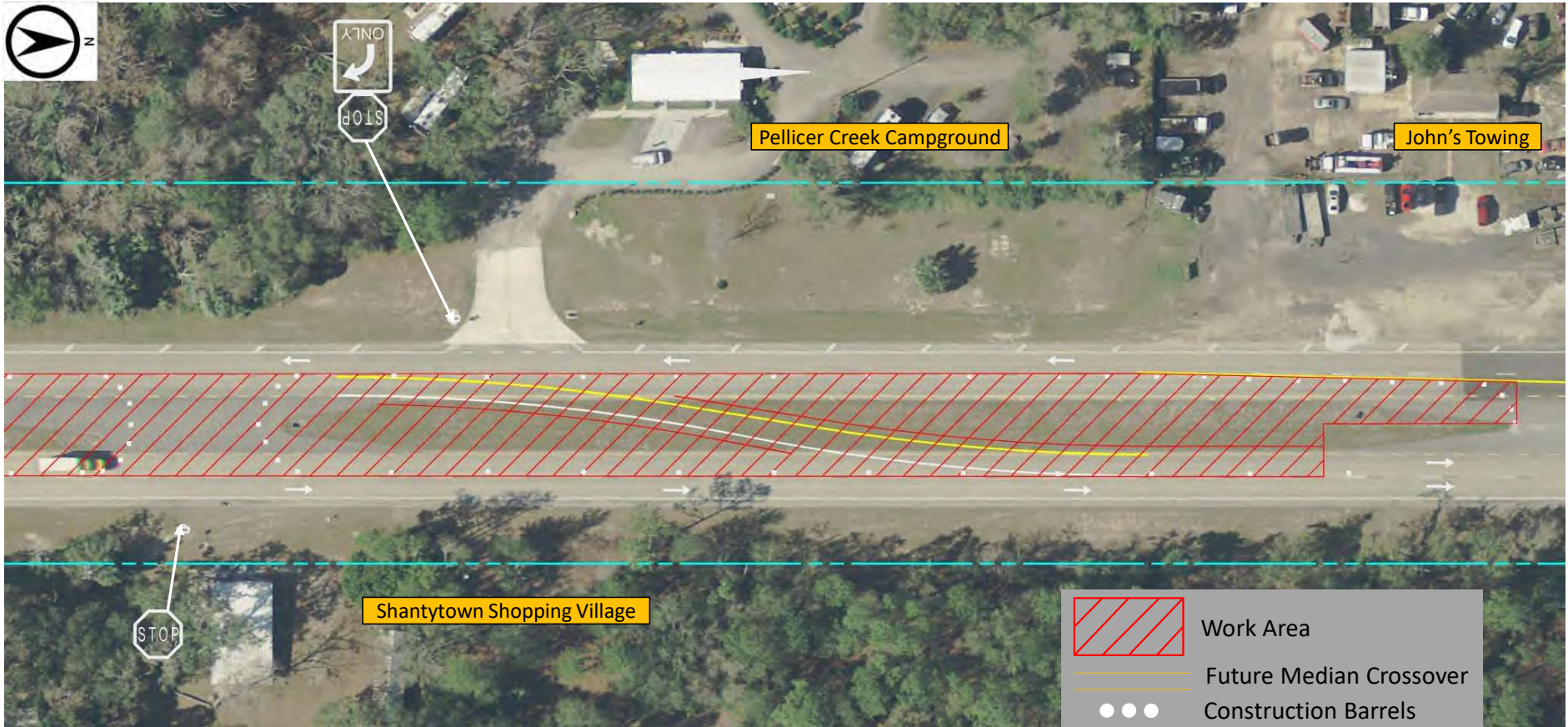




Stakeholder Coordination



Property Access Phase I - NB Bridge Construction



- John's Towing – Access remains unchanged
- Pellicer Creek Campground – Right turn only SB, then utilize the median opening at Shantytown for U-turns NB
- Shantytown – Access remains unchanged

Property Access Phase 2 - NB Bridge Construction



- John's Towing – Access remains unchanged
- Pellicer Creek Campground – Right turn only SB, then utilize U-turn at opening just before Old Kings Road to head NB
- Shantytown– Right turn only NB via median crossover, then U-turn at median opening by John's Towing to head SB

Property Access Phase 3 - SB Bridge Construction



- John's Towing – Access remains unchanged
- Pellicer Creek Campground – Right turn only SB, then utilize U-turn at opening just before Old Kings Road to head NB
- Shantytown – Right turn only NB, then U-turn at median opening by John's Towing to head SB

Property Access Phase 4 - SB Bridge Construction



- John's Towing – Access remains unchanged
- Pellicer Creek Campground – Right turn only SB via median opening, then utilize U-turn at opening just before Old Kings Road to head NB
- Shantytown – Right turn only NB, then U-turn at median opening by John's Towing to head SB via median crossover



Schedule & Estimated Costs





Ways to Comment

- Submit a written comment form today
- Submit a comment on the project website www.CFLRoads.com/project/447118-1
- Contact the project manager directly:



Shelley.ChinQuee@dot.state.fl.us



**719 S. Woodland Blvd., MS 542
DeLand, FL 32720**



386-943-5439

Questions





Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. Woodland Blvd.
DeLand, FL 32720

JARED W. PERDUE, P.E.
SECRETARY

October 4, 2023

Alissa S. Lotane,
Director and State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attn: Mr. Benjamin Stewart, Transportation Compliance Review Program

RE: Section 106 Consultation Case Study Report
State Road 5/US 1 Bridge Replacement over Pellicer Creek
Flagler and St. Johns Counties, Florida
Financial Management No.: 447118-1-52-01

Dear Ms. Lotane,

Enclosed, please find one copy of the *Section 106 Consultation Case Study Report for the State Road 5/US 1 Bridge Replacement over Pellicer Creek, Flagler and St. Johns Counties, Florida*. The Florida Department of Transportation (FDOT), District 5, is proposing to replace both bridges carrying State Road (SR) 5/US 1 over Pellicer Creek at the Flagler and St. Johns County line. The project also includes minor roadway work limited to replacing the bridges and modification of the existing drainage system as required to complete proposed roadway reconstruction work. An easement across sovereign submerged lands is required to accommodate construction. The bridges are functionally obsolete, and because rehabilitation and reuse of the structures are not possible given their current condition, the project is proposing the replacement of both bridges to ensure safe travel and maintain connectivity along this portion of SR 5/US 1.

A Phase I cultural resource assessment survey for the preferred alternative was completed by SEARCH in June 2022. The cultural resource assessment survey and subsequent consultation with the State Historic Preservation Officer concluded that one National Register of Historic Places-eligible historic property is within the project area of potential effects. The effects assessment and alternatives analysis addresses project-related effects relative to this NRHP-eligible resource, FDOT Bridge No. 730008 (8FL01008/8SJ08262). Three alternatives in addition to a rehabilitation alternative and no-build alternative were considered prior to choosing the selected alternative.

www.fdot.gov

Ms. Lotane, SHPO
FM # 447118-1-52-01
October 4, 2023
Page 2

As a federally funded project, the effects evaluation was conducted to comply with Public Law 113-287 (Title 54 US Code), which incorporates the provisions of the National Historic Preservation Act of 1966, as amended, and the Archeological and Historic Preservation Act of 1974, as amended. The study also meets the regulations for implementing National Historic Preservation Act Section 106 found in 36 Code of Federal Regulations Part 800 (*Protection of Historic Properties*). This study also complies with Chapter 267 of the Florida Statutes and Rule Chapter 1A-46, Florida Administrative Code. SEARCH performed all work in accordance with Part 2, Chapter 8 of the FDOT's Project Development & Environment Manual (revised July 2023) and the Florida Division of Historical Resources' recommendations for such projects as stipulated in *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The principal investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 Federal Register 44716-42).

Based on a review of the project plans, it is the district's opinion that the project will result in an adverse effect to FDOT Bridge No. 730008 (8FL01008/8SJ08262). Because construction of the selected alternative will result in an adverse effect to this historic property, FDOT proposes to prepare state equivalent Level III Historic American Engineering Record documentation for 8FL01008/8SJ08262. If your office agrees with this proposed mitigation strategy, FDOT will prepare a draft Memorandum of Agreement to memorialize this commitment and circulate for your review.

I respectfully request your concurrence with the findings of the enclosed report and the proposed mitigation strategy described above.

If you have any questions or need further assistance, please contact Catherine Owen, District Cultural Resource Coordinator, at (386) 943-5383 or me at (386) 943-5436.

Sincerely,



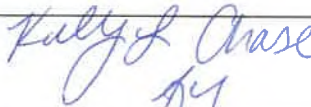
Casey Lyon, MS
Environmental Manager
FDOT, District Five

Ms. Lotane, SHPO
FM # 447118-1-52-01
October 4, 2023
Page 3

The Florida State Historic Preservation Officer finds the attached Section 106 Consultation Case Study Report complete and sufficient and concurs / does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 2023-4979B. Or, the SHPO finds the attached document contains _____ insufficient information.

In accordance with the Programmatic Agreement among the ACHP, SHPO and FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida, if providing concurrence with a finding of No Historic Properties Affected for a project as a whole, or to No Adverse Effect on a specific historic property, SHPO shall presume that FDOT may approve the project as de minimis use under Section 4(f) under 23 CFR 774.

SHPO Comments:



Alissa S. Lotane, Director
Florida Division of Historical Resources

10.25.2023
Date



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. Woodland Blvd.
DeLand, FL 32720

JARED W. PERDUE, P.E.
SECRETARY

October 18, 2023

Historic and Cultural Preservation Department
Muscogee (Creek) Nation Cultural Preservation
P.O. Box 580
Okmulgee, OK 74447
section106@mcn-nsn.gov

RE: Section 106 Consultation Case Study Report
State Road 5/US 1 Bridge Replacement over Pellicer Creek
Flagler and St. Johns Counties, Florida
Financial Management No.: 447118-1-52-01

Dear Madam or Sir,

Enclosed, please find one copy of the cultural resource assessment survey (CRAS) and one copy of the *Section 106 Consultation Case Study Report for the State Road 5/US 1 Bridge Replacement over Pellicer Creek, Flagler and St. Johns Counties, Florida*. The Florida Department of Transportation (FDOT), District 5, is proposing to replace both bridges carrying State Road (SR) 5/US 1 over Pellicer Creek at the Flagler and St. Johns County line. The project also includes minor roadway work limited to replacing the bridges and modification of the existing drainage system as required to complete the proposed roadway reconstruction work. An easement across sovereign submerged lands is required to accommodate construction. The bridges are functionally obsolete, and because rehabilitation and reuse of the structures are not possible given their current condition, the project is proposing the replacement of both bridges to ensure safe travel and maintain connectivity along this portion of SR 5/US 1.

A Phase I CRAS included an archaeological survey, but no artifacts were recovered, and no archaeological sites or occurrences were identified within the project Area of Potential Effect (APE). However, the CRAS and subsequent consultation with the State Historic Preservation Officer (SHPO) concluded that one National Register of Historic Places-eligible historic property is within the project area of potential effects. The Case Study provides an effects assessment and alternatives analysis addressing project-related effects relative to this NRHP-eligible resource, FDOT Bridge No. 730008 (8FL01008/8SJ08262). Three alternatives in addition to a rehabilitation alternative and no-build alternative were considered prior to choosing the selected alternative.

As a federally funded project, the effects evaluation was conducted to comply with Public Law 113-287 (Title 54 US Code), which incorporates the provisions of the National Historic Preservation Act of 1966, as amended, and the Archeological and Historic Preservation Act of

www.fdot.gov

Muscogee (Creek) Nation Cultural Preservation

FM # 447118-1-52-01

October 18, 2023

Page 2

1974, as amended. The study also meets the regulations for implementing the National Historic Preservation Act Section 106 found in 36 Code of Federal Regulations Part 800 (*Protection of Historic Properties*). This study also complies with Chapter 267 of the Florida Statutes and Rule Chapter 1A-46, Florida Administrative Code. SEARCH performed all work in accordance with Part 2, Chapter 8 of the FDOT's Project Development & Environment Manual (revised July 2023) and the Florida Division of Historical Resources' recommendations for such projects as stipulated in *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The principal investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 Federal Register 44716-42).

Based on all provided information, it is the district's opinion that the project will result in an adverse effect to FDOT Bridge No. 730008 (8FL01008/8SJ08262). Because construction of the selected alternative will result in an adverse effect to this historic property, FDOT proposes to prepare state equivalent Level III Historic American Engineering Record documentation for 8FL01008/8SJ08262. We are currently awaiting concurrence from the SHPO for this proposed mitigation strategy. Once approved, FDOT will prepare a draft Memorandum of Agreement to memorialize this commitment and circulate it for review.

We are respectfully requesting your review and opinion regarding the findings of the enclosed reports and the proposed mitigation strategy described above.

If you have any questions or need further assistance, please contact Catherine Owen, District Cultural Resource Coordinator, at (386) 943-5383 or me at (386) 943-5436.

Sincerely,



For: Casey Lyon, MS
Environmental Manager
FDOT, District Five

THE DAYTONA BEACH
NEWS-JOURNAL PO Box 631244 Cincinnati, OH 45263-1244

AFFIDAVIT OF PUBLICATION

Mary Bierwagen
Mary Bierwagen PLEMO- cc:501
Florida Dept Of Transportation
719 S Woodland BLVD # 5
Deland FL 32720-6834

STATE OF WISCONSIN, COUNTY OF BROWN

Before the undersigned authority personally appeared, who on oath says that he or she is the Legal Coordinator of The News-Journal, published in Volusia and Flagler Counties, Florida; that the attached copy of advertisement, being a Govt Public Notices, was published on the publicly accessible website of Volusia and Flagler Counties, Florida, or in a newspaper by print in the issues of, on:

08/16/2024

Affiant further says that the website or newspaper complies with all legal requirements for publication in chapter 50, Florida Statutes.

Subscribed and sworn to before me, by the legal clerk, who is personally known to me, on 08/16/2024

Keegan Loren

Legal Clerk
Will Feltz

Notary, State of WI, County of Brown
3-7-27

My commission expires

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U.S. 1/State Road (S.R.) 5 Over
Pellicer Creek Bridge Replacement
Financial Project Number 447118-1

The FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces a notice of opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried-out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

Project Name: U.S. 1/State Road (S.R.) 5 Over Pellicer Creek Bridge Replacement
Type of Project: Project Development & Environment (PD&E) Study
Project Limits: Flagler County / St. Johns County Line
City/County: City of Palm Coast in Flagler County and St. Johns County, Florida
Financial Project Identification (FPID) Number: 447118-1
Federal-Aid Project Number: N/A

FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek, and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements.

The PD&E study is evaluating and documenting potential engineering and natural, physical, socioeconomic, and cultural environmental effects of the proposed improvements. FDOT is conducting the PD&E study in accordance with the requirements of the National Environmental Policy Act and other applicable federal and state laws and regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.flroads.com/project/447118-1 through August 31, 2024.

Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to:

Florida Department
of Transportation
Project Manager
Shelley ChinQuee, P.E.
719 South Woodland Boulevard
DeLand, FL 32720
386-943-5439
Shelley.ChinQuee@dot.state.fl.us

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.

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FAR Issue Vol. 50/No. 161

Section XII

Issue Date: August 16, 2024

Total number of notices in Section XII: 3

Notice / Adopted	Section	Description	ID	Publish Date
	Misc Agency 1	Index of Administrative Rules Filed with the Secretary of State	28657891	8/16/2024 Vol. 50/161
	Misc Agency 62	State Revolving Fund (SRF) Program	28642759	8/16/2024 Vol. 50/161
	Misc Agency 1000T46	Notice of Opportunity	28636551	8/16/2024 Vol. 50/161

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Under Florida law, E-mail addresses are public records. If you do not want your E-mail address released in response to a public records request, do not send electronic mail to this entity. Instead, contact this office by phone or in writing.

Miscellaneous

OTHER AGENCIES AND ORGANIZATIONS**Quest Corporation of America, Inc.**

Notice of Opportunity

THE FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces an opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

Project Name: U.S. 1/State Road (S.R.) 5 Over Pellicer Creek Bridge Replacement
 Type of Project: Project Development & Environment (PD&E) Study
 Project Limits: Flagler County / St. Johns County Line
 City/County: City of Palm Coast in Flagler County and St. Johns County, Florida
 Financial Project Identification (FPID) Number: 447118-1
 Federal-Aid Project Number: N/A

Project Description:

FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements. The PD&E study is evaluating and documenting potential engineering and natural, physical, socioeconomic, and cultural environmental effects of the proposed improvements. FDOT is conducting the PD&E study in accordance with the requirements of the National Environmental Policy Act and other applicable federal and state laws and regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.cflroads.com/project/447118-1 through August 31, 2024. Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to: Florida Department of Transportation, Shelley ChinQuee, P.E., Project Manager, 719 South Woodland Boulevard, DeLand, FL 32720, or by email at Shelley.ChinQuee@dot.state.fl.us.

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.



Florida Department of

TRANSPORTATION

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District Five

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. For additional information please contact the [Equal Opportunity Office](#).

If you need special accommodations under the American with Disabilities Act or require translation services (free of charge) please contact the person listed in the notice at least seven days before the meeting.

TECHNICAL REVIEW AND AWARD COMMITTEE MEETINGS

District: Five
Meeting Type: Meeting
Location Name: Florida Department of Transportation, District Five Office
Street Address: 719 S. Woodland Blvd.
City: Deland
Website: [District 5 Technical Review and Award Committee Meeting Notices](#)

OPPORTUNITY FOR PUBLIC HEARING: U.S. 1/S.R. 5 OVER PELLICER CREEK BRIDGE REPLACEMENT

District: Five
Meeting Type: Hearing
Date: Friday, August 16, 2024
Time: 2:00 pm to N/A
Purpose: The FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces a notice of opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried-out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

Project Name: U.S. 1/State Road (S.R.) 5 Over Pellicer Creek Bridge Replacement

Type of Project: Project Development & Environment (PD&E) Study

Project Limits: Flagler County / St. Johns County Line

City/County: City of Palm Coast in Flagler County and St. Johns County, Florida

Financial Project Identification (FPID) Number: 447118-1

Federal-Aid Project Number: N/A

Project Description: FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek, and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements.

The PD&E study is evaluating and documenting potential engineering and natural, physical, socioeconomic, and cultural environmental effects of the proposed improvements. FDOT is conducting the PD&E study in accordance with the requirements of the National Environmental Policy Act and other applicable federal and state laws and regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.cflroads.com/project/447118-1 through August 31, 2024.

Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to:

Florida Department of Transportation Project Manager Shelley ChinQueue, P.E. 719 South Woodland Boulevard DeLand, FL 32720 386-943-5439 Shelley.ChinQueue@dot.state.fl.us

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.

Project Web Site: <https://www.cflroads.com/project/447118-1>

Primary Contact: Shelley ChinQueue

Primary Phone: 386-943-5439

Primary E-Mail: Shelley.ChinQueue@dot.state.fl.us

Expires: 9/1/2024

PUBLIC INFORMATION MEETING: GARDEN STREET (S.R. 406) RESURFACING

District: Five

Meeting Type: Open House

Date: Thursday, August 22, 2024

Time: 5:30 pm to 7:00 pm

Location Name: Good Shepherd Lutheran Church

Street Address: 2073 Garden St.

City: Titusville

Purpose: The Florida Department of Transportation (FDOT) will hold a public meeting regarding project plans on Garden Street (State Road (S.R.) 406) from west of Interstate 95 (I-95) to Forrell Avenue in Titusville on Thursday, August 22, at 5:30 p.m.

The purpose of this project to resurface the existing pavement to extend the service life of the roadway. The public meeting is being held to present information and receive community feedback.

FDOT encourages community involvement and is offering this public meeting in person and virtually to provide more ways to

participate. All attendees, regardless of which platform they choose, will receive the same information.

- **In-Person Open House Option:** Participants may attend in person by going to Good Shepherd Lutheran Church, 2073 Garden St., Titusville, FL 32796 anytime between 5:30 p.m. and 7 p.m. to view a looping presentation and project displays, speak with project team members, and submit comments or questions. If attending in person, please remember to follow all safety and sanitation guidelines. If you are feeling unwell, please consider attending the meeting virtually.
- **Virtual Option:** Interested persons may join over the Internet from a computer, tablet or mobile device at 5:30 p.m. For this option, advance registration is required by visiting fdot.cc/Garden_Street_Improvements. Once registered, participants will receive a confirmation email containing information about joining the meeting online. If using a mobile device, use the GoToWebinar app to be able to view the presentation and submit comments. If joining online, please allow adequate time to log in to view the presentation in its entirety.

If you are unable to attend the meeting, you can view all meeting materials, including the presentation, and provide comment through the project website at www.cflroads.com/project/450626-1. You may also contact FDOT Project Manager Gregory Prytyka, P.E., directly by phone at 386-943-5394 or email at Gregory.Prytyka@dot.state.fl.us.

FDOT is sending notices to nearby property owners, business owners, interested persons, and organizations to provide the opportunity to offer comments and express their views regarding this project and the proposed improvements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons wishing to express their concerns relative to FDOT compliance with Title VI may do so by contacting Melissa McKinney, FDOT District Five Title VI Coordinator, at Melissa.McKinney@dot.state.fl.us.

Persons who require accommodations under the Americans with Disabilities Act (ADA) or persons who require translation services (free of charge) should contact Carolyn Fitzwilliam at 386-943-5215 or Carolyn.Fitzwilliam@dot.state.fl.us at least seven (7) days prior to the meeting. If you are hearing or speech impaired, please contact us by using the Florida Relay Service, 1-800-955-8771 (TDD) or 1-800-955-8770 (Voice).

If you have any questions or comments about the meeting or the project, please contact Mr. Prytyka by phone at 386-943-5394, by email at Gregory.Prytyka@dot.state.fl.us, or U.S. mail at Florida Department of Transportation, 719 S. Woodland Blvd., MS 542, DeLand, FL 32720.

Project Web Site: <https://www.cflroads.com/project/450626-1>

Primary Contact: Gregory Prytyka

Primary Phone: 386-943-5394

Primary E-Mail: Gregory.Prytyka@dot.state.fl.us

Expires: 8/23/2024

SUNRAIL CENTRAL FLORIDA COMMUTER RAIL COMMISSION BOARD MEETING AUGUST 22, 2024

District: Five

Meeting Type: Meeting

Date: Thursday, August 22, 2024

Time: 2:00 pm to 3:00 pm

Location Name: LYNX Central Station Administration Building Board Room

Street Address: 455 N. Garland Avenue

City: Orlando

Purpose: ORLANDO, Fla. – The Central Florida Commuter Rail Commission (CFCRC), the SunRail advisory board of elected leaders from the five Local Funding Partners, will hold a board meeting on Thursday, August 22, 2024.

The meeting takes place at 2 p.m. in the LYNX Central Station Administration Building. Attendees have the option of joining in person, virtually, or over the phone. Please note that there is limited seating in the room where the commission will hold its meeting.

Those who wish to attend by phone may dial 213-929-4221 and enter access code 244-900-372 (muted) when prompted.

Attendees may also join from a computer, tablet, or cell phone by registering at attendee.gotowebinar.com/register/4066244029641842263. The meeting will start promptly at 2 p.m. The meeting agenda and meeting materials are made available at least 72 hours prior to the meeting at [Corporate.Sunrail.com](https://www.corporate.sunrail.com).

Those wishing to provide comments in person must come the day and time of the meeting to the LYNX main office at:

LYNX Central Station Administration Building Board Room 455 N. Garland Avenue, 2nd Floor Orlando, FL 32801

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons wishing to express their concerns relative to FDOT compliance with Title VI may do so by contacting Roger Masten, SunRail Title VI Coordinator at Roger.Masten@dot.state.fl.us.

Persons who require accommodations under the Americans with Disabilities Act or persons who require translation services

(free of charge) should contact Roger Masten, at least seven days in advance of the meeting, c/o SunRail, 801 SunRail Drive, Sanford, FL 32771; or by phone at (321) 257-7161; or by email at Roger.Masten@dot.state.fl.us.

Questions? Please contact the FDOT Public Information Office at FDOT-D5COMM@dot.state.fl.us or (386) 943-5593.

Please be careful around trains, railroad crossings, and while at station platforms. Be smart. Be safe. For more information about SunRail, including fares, schedule, and station locations, please visit www.SunRail.com, or download the SunRail app.

Project Web Site: <https://corporate.sunrail.com/>

Primary Contact: Public Information Office

Primary Phone: (386) 943-5593

Primary E-Mail: FDOT-D5COMM@dot.state.fl.us

Expires: 8/23/2024

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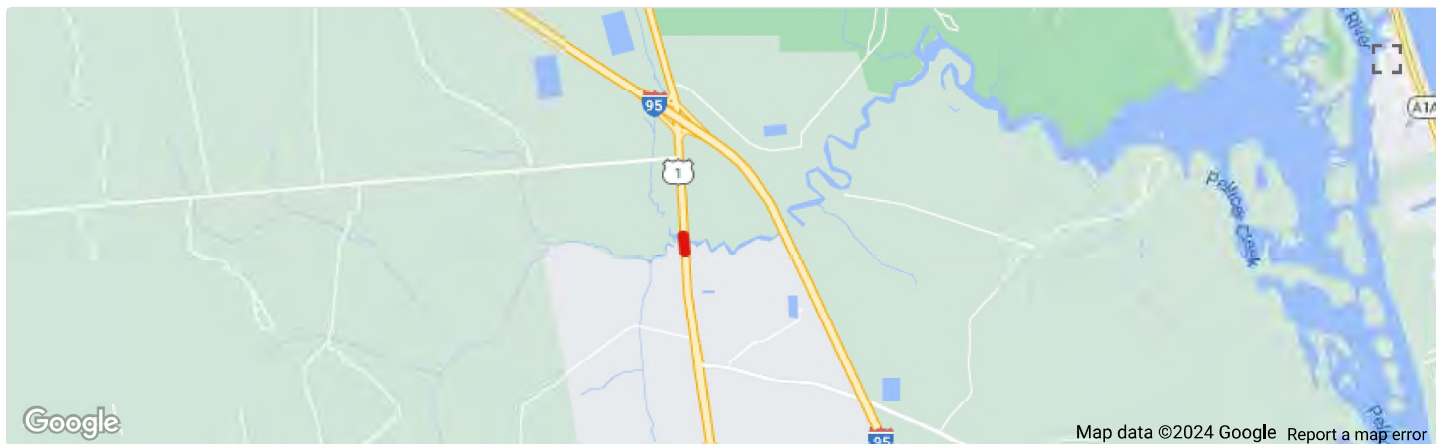
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Florida Department of TRANSPORTATION

447118-1 US 1 (SR 5) over Pellicer Creek Bridge Replacement



Project Details

Phase: Design

Length: 0.231 miles

County: Flagler

Road: SR 5
US 1

Design Cost: \$1.9 Million

Letting: 01/28/2026

Estimated Costs

Construction Cost: \$11.6 Million

About

The purpose of the project is to replace the northbound and southbound U.S. 1 (State Road 5) bridges over Pellicer Creek at the Flagler/St Johns County line.

Get Involved

As part of the Project Development & Environment (PD&E) Study, the Florida Department of Transportation (FDOT) announces a **notice of opportunity to request a public hearing.**

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review in the project files below. Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to: FDOT Project Manager Shelley ChinQuee, P.E., by mail at 719 South Woodland Boulevard, DeLand, FL 32720, by phone at 386-943-5439 or by email at Shelley.ChinQuee@dot.state.fl.us. The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.

Send a comment or ask a question about this project

Contact Information
Design Project Manager Shelley ChinQuee (386) 943-5439 Shelley.ChinQuee@dot.state.fl.us
Design Firm Florida Bridge and Transportation, Inc.
Last Updated 08/15/2024



Project Files
Documents 2024-08-16 Draft Section 106 MOA 2024-08-16 Draft Programmatic Section 4(f) Evaluation 2023-10-25 Approved Section 106 Case Study Report 2023-03-21 Stakeholder Presentation 2023-02-28 Project Information Handout



Welcome to FDOT's District Five Projects Website



Grab Your Running Shoes: Learn about upcoming trail plans and projects

Looking for the nearest trail to explore? Check here for SUN Trail funded construction projects – both complete and on the horizon! **More...**

Project Finder

You can find FDOT projects quickly by selecting a road, city or county below, or by clicking a county on the service area map.

Search Road Search County Search City

Current News

08/16/2024 Opportunity for Public Hearing: U.S. 1/S.R. 5 Over Pellicer Creek Bridge Replacement

08/14/2024 Upcoming Public Meeting: SunRail Central Florida Commuter Rail Commission Board Meeting August 22, 2024

08/14/2024 Upcoming Public Meeting: Garden Street (S.R. 406) Resurfacing

08/13/2024 Traffic Advisory: Westbound I-4 Entrance Ramp from Sand Lake Road scheduled for Overnight Closures August 14, 15, and 18

07/03/2024 Traffic Advisory: Lane Closures at I-4 and Conroy Road Interchange to Begin July 8

Social Media

FDOT Central Florida
Follow Page 7.9K followers

FDOT Central Florida
on Thursday

Attention Flagler County motorists!
Flagler County Sheriff's Office Flagler Beach
Police Department

U.S. Army Corps of Engineers, Jacksonville District

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Opportunity for Public Hearing: U.S. 1/S.R. 5 Over Pellicer Creek Bridge Replacement

Friday, August 16, 2024

The FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces a notice of opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried-out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

Project Name: U.S. 1/State Road (S.R.) 5 Over Pellicer Creek Bridge Replacement

Type of Project: Project Development & Environment (PD&E) Study

Project Limits: Flagler County / St. Johns County Line

City/County: City of Palm Coast in Flagler County and St. Johns County, Florida

Financial Project Identification (FPID) Number: 447118-1

Federal-Aid Project Number: N/A

Project Description: FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek, and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements.

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regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.cflroads.com/project/447118-1 through August 31, 2024.

Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to:

Florida Department of Transportation Project Manager Shelley ChinQueue, P.E. 719 South Woodland Boulevard DeLand, FL 32720 386-943-5439 Shelley.ChinQueue@dot.state.fl.us

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.

[Back to Current News](#)



**MEMORANDUM OF AGREEMENT
BETWEEN THE FLORIDA DEPARTMENT OF TRANSPORTATION AND
THE FLORIDA STATE HISTORIC PRESERVATION OFFICER
REGARDING THE STATE ROAD 5 (SR 5)/US 1 OVER PELLICER
CREEK BRIDGE (No. 730008) REPLACEMENT PROJECT IN FLAGLER
AND ST. JOHNS COUNTIES, FLORIDA**

WHEREAS, Pursuant to 23 United States Code (U.S.C.) § 327 and the implementing Memorandum of Understanding (MOU) executed on May 26, 2022, the Florida Department of Transportation (FDOT) has assumed Federal Highway Administration’s (FHWA) responsibilities under the National Environmental Policy Act (NEPA) for highway projects on the State Highway System (SHS) and Local Agency Program (LAP) projects off the SHS; and

WHEREAS, in accordance with the MOU, FDOT’s assumption of FHWA’s responsibilities under NEPA for highway projects includes assumption of responsibilities for compliance with 36 CFR Part 800, regulations implementing Section 106 of the National Historic Preservation Act (54 U.S.C. § 306108); and

WHEREAS, FDOT will provide federal financial assistance for the SR 5/US 1 over Pellicer Creek Bridge (No. 730008) Replacement, Financial ID No. 447118-1-32-01 (**Project**); and

WHEREAS, FDOT has determined that the **Project** represents an undertaking in accordance with 36 CFR § 800.3(a); and

WHEREAS, FDOT has defined the **Project’s** area of potential effects (APE) as the existing SR 5/US 1 right-of-way (ROW) from approximately 335 m (1,100 ft) south of the Flagler and St. Johns County line to 188 m (617 ft) north of the line extended to the back or side property lines of parcels adjacent to the ROW or no more than 100 m (330 ft) from the ROW line, as depicted in Attachment A; and

WHEREAS, FDOT has consulted with the Florida State Historic Preservation Officer (SHPO) pursuant to the requirements of 36 CFR Part 800 and has determined that the **Project** will have an adverse effect on FDOT Bridge No. 730008 (Florida Master Site File [FMSF] Nos. 8FL01008/8SJ08262), which is eligible for listing in the National Register of Historic Places (NRHP); and

WHEREAS, FDOT has consulted with the Muscogee (Creek) Nation of Florida regarding the effects of the **Project** on historic properties; and

WHEREAS, FDOT has consulted with the Florida SHPO and the Certified Local Government representative for St. Johns County regarding the effects of the **Project** on historic properties; and

WHEREAS, FDOT has provided opportunities for public review and comment regarding the effects of the **Project** on historic properties, as appropriate; and

WHEREAS, in accordance with 36 CFR § 800.6(a)(1) FDOT has notified the Advisory Council on Historic Preservation (ACHP) of the adverse effect determination with specified documentation and has invited the ACHP to comment and participate in consultation, and the ACHP has chosen not to participate pursuant to 36 CFR § 800.6(a)(1)(iii); and

NOW, THEREFORE, FDOT and the SHPO agree that the **Project** shall be implemented in accordance with the following stipulations to take into account the effect of the **Project** on historic properties.

STIPULATIONS

FDOT shall ensure that the following measures are carried out:

I. HISTORIC AMERICAN ENGINEERING RECORD (HAER) LEVEL III EQUIVALENT DOCUMENTATION

Prior to authorizing any demolition or other activity that could damage FDOT Bridge No. 730008 (FMSF Nos. 8FL01008/8SJ08262), per guidance provided by the National Park Service (NPS), FDOT will ensure that the bridge is documented in accordance with the standards and guidelines of the Historic American Engineering Record (HAER). FDOT shall:

A. Complete Historic American Engineering Record (HAER) Level III equivalent documentation of 8FL01008/8SJ08262, including:

B. Large format photography.

1. A HAER equivalent report, inclusive of historic context, scans of large format negatives, and a sketch plan of 8FL01008/8SJ08262.

C. Submit the final HAER equivalent report and photographic documentation to the Florida Division of Historical Resources (FDHR), including an archival copy and an electronic copy for inclusion in the FMSF. Unless otherwise agreed to by the NPS, FDOT will ensure that all documentation is completed and accepted in writing by the NPS prior to demolition.

II. PROFESSIONAL STANDARDS

All archaeological and historic preservation work carried out pursuant to this Agreement shall be conducted by, or under the direct supervision of, a person or persons meeting the Secretary of the Interior's Professional Qualification Standards for Archaeology and Historic Preservation as set forth at 62 FR 33708-33723 (June 20, 1997).

III. DURATION

This Memorandum of Agreement (MOA) will expire if its terms are not carried out within ten (10) years from the date of execution or upon project acceptance, whichever comes first if the FDOT has not completed all the terms and conditions within the MOA. Prior to expiration, the parties may agree to extend the timeframe for fulfillment of the terms by letter agreement.

IV. MOA DOCUMENTATION

A. The FDOT shall provide a summary of actions carried out pursuant to this MOA to the FDOT Office of Environmental Management (OEM) annually. The reporting period shall be the fiscal year from July 1st to June 30th and the summary shall describe the status of mitigation activities and, as applicable, any issues that may affect the ability of the FDOT to continue to meet the terms of this MOA, any disputes and objections received, and how they were resolved.

B. A Notice of Fulfillment will be prepared to summarize the implementation of the MOA after all stipulations have been fulfilled. This document will be submitted to OEM and SHPO for their files within six (6) months after completion of all MOA stipulations.

V. POST-REVIEW DISCOVERIES

A. If properties are discovered that may be historically significant, or if unanticipated effects on historic properties are found, FDOT shall implement the Post Review Discovery Plan established in Stipulation IX of the September 27, 2023 Programmatic Agreement among the ACHP, SHPO, and FDOT (2023 PA).

B. In the unlikely event that human skeletal remains or associated burial artifacts are uncovered within the project area during construction, all work in that area must stop. The individual in charge of the activity that leads to the discovery must notify the Project Engineer and the FDOT District 5 Cultural Resources Coordinator per Stipulation X of the 2023 PA. The discovery must be reported to local law enforcement and the appropriate medical examiner. The medical examiner will determine whether the State Archaeologist should be contacted per the requirements of Section 872.05, Florida Statutes, and Rule 1A-44.004, Florida Administrative Code (FAC).

VI. REVIEW STIPULATION

FDOT shall afford the SHPO and other consulting parties, including the federally recognized Tribes affiliated with Florida, a thirty (30) day period for review and comment following the receipt of delivery of those submittals and reviews described above. If no comments are received by FDOT at the end of these thirty (30) days, FDOT will presume there are no objections. Any objections to the findings or plans proposed in these submittals will be addressed in accordance with Stipulation VII, below.

VII. DISPUTE RESOLUTION

Should any signatory to this MOA object at any time to any actions proposed or the manner in which the terms of this MOA are implemented, FDOT shall consult with such party to resolve the objection. If FDOT determines that such objection cannot be resolved, FDOT will:

A. Forward all documentation relevant to the dispute, including FDOT’s proposed resolution, to the ACHP. The ACHP shall provide FDOT with its advice on the resolution of the objection within thirty (30) days of receiving adequate documentation. Prior to reaching a final decision on the dispute, FDOT shall prepare a written response that considers any timely advice or comments regarding the dispute from the ACHP, signatories, and concurring parties, and provide them with a copy of this written response. FDOT will then proceed according to its final decision.

B. Make a final decision on the dispute and proceed accordingly if the ACHP does not provide its advice regarding the dispute within thirty (30) days. Prior to reaching such a final decision, FDOT shall prepare a written response that considers any timely comments regarding the dispute from the signatories to the MOA, and provide them and the ACHP with a copy of the written response.

C. Fulfill its responsibility to carry out all other actions subject to the terms of this MOA that are not the subject of the dispute.

VIII. AMENDMENTS

This MOA may be amended when such an amendment is agreed to in writing by all signatories. All signatories must signify their acceptance of the proposed changes to the MOA in writing within thirty (30) days of their receipt. The amendment will be effective on the date a copy signed by all signatories is filed with the ACHP. In accordance with 36 CFR § 800.6(b)(7), if the ACHP was not a signatory to the original agreement and the signatories execute an amended agreement, FDOT shall file the amended agreement with the ACHP.

IX. TERMINATION

If any signatory to this MOA determines that its terms will not or cannot be carried out, that party shall immediately consult with the other signatories in an effort to amend the MOA per Stipulation VII, above. If within thirty (30) days (or another time agreed to by all signatories) an amendment cannot be reached, any signatory may terminate the MOA upon written notification to the other signatories.

Once the MOA is terminated, and prior to work continuing on the **Project**, FDOT must either (a) execute an MOA pursuant to 36 CFR § 800.6 or (b) request, take into account, and respond to the comments of the ACHP under 36 CFR § 800.7. FDOT shall notify the signatories as to the course of action it will pursue.

Execution of this MOA by FDOT and SHPO and implementation of its terms is evidence that FDOT has taken into account the effects of this undertaking on historic properties per the requirements of Section 106 (Public Law 113-287 [Title 54 U.S.C. 306108]), and 36 CFR Part 800 (Protection of Historic Properties).

SIGNATORIES:

FLORIDA STATE HISTORIC PRESERVATION OFFICER

Signed by:
Alissa Lotane
224D89ABEE884B3...

Alissa S. Lotane
Director, Division of Historical Resources
State Historic Preservation Officer

Date 10/29/2024 | 6:47 PM EDT

FLORIDA DEPARTMENT OF TRANSPORTATION

DocuSigned by:
Jennifer Marshall
D881D38487C64B2...

Jennifer Marshall, P.E.
Director, Office of Environmental Management

Date 10/30/2024 | 1:57 PM EDT

CONCURRING PARTIES:

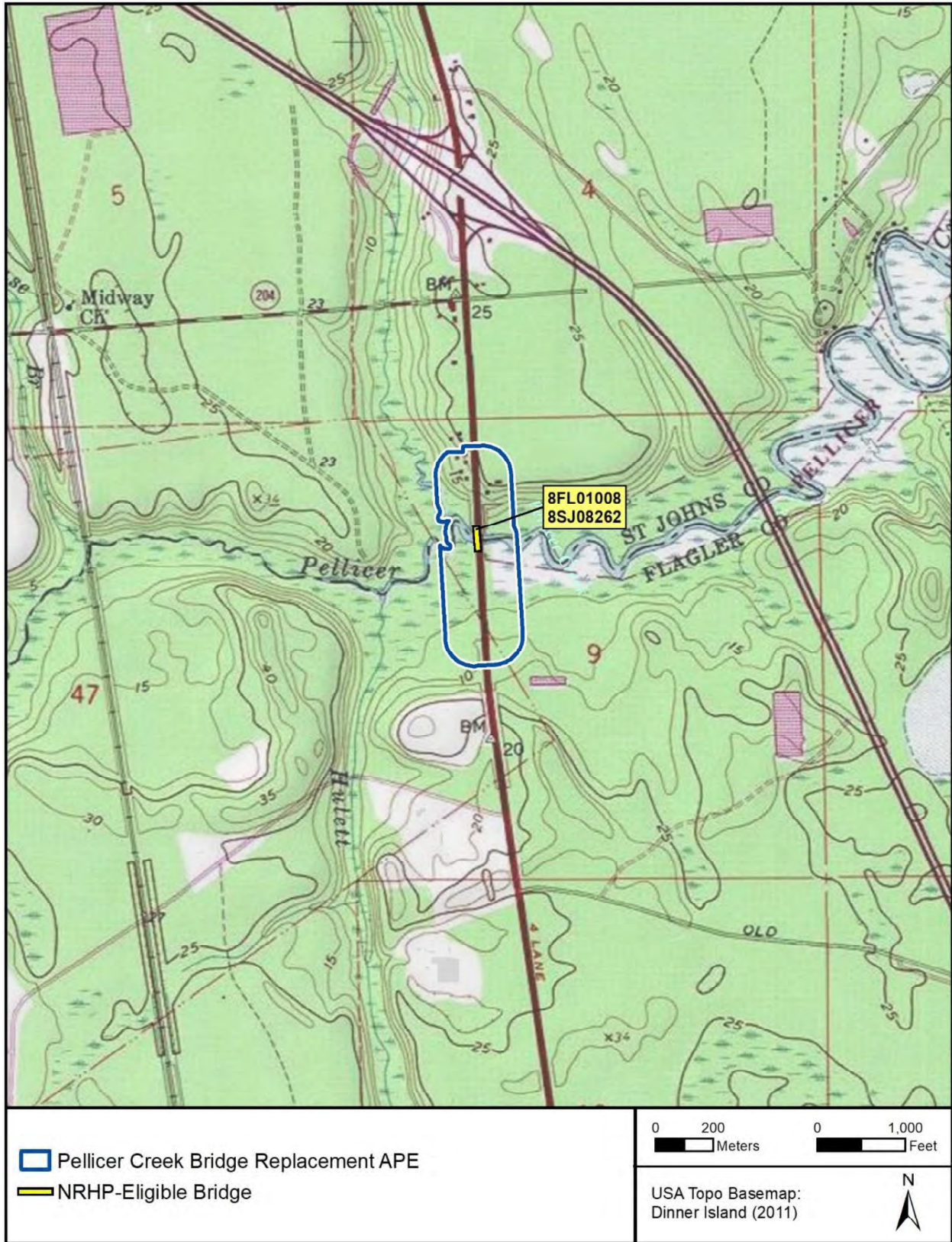
FLORIDA DEPARTMENT OF TRANSPORTATION, DISTRICT 5

DocuSigned by:
Adkins, Jack
1752524FC337485...

C. Jack Adkins
Director, Transportation Development

Date 09/25/2024 | 4:09 PM EDT

ATTACHMENT A



Location of NRHP-eligible bridge 8FL01008/8SJ08262.



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. Woodland Blvd.
DeLand, FL 32720

JARED W. PERDUE, P.E.
SECRETARY

June 23, 2022

Timothy A. Parsons, Ph.D.,
Director and State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attn: Ms. Alyssa McManus, Transportation Compliance Review Program

RE: Cultural Resource Assessment Survey
SR 5 Bridge Replacement over Pellicer Creek
Flagler and St. Johns Counties, Florida
Financial Management No.: 447118-1

Dear Dr. Parsons,

Enclosed please find one copy of the report titled *Cultural Resource Assessment Survey of the SR 5 Bridge Replacement over Pellicer Creek, Flagler and St. Johns Counties, Florida*. This report presents the findings of a CRAS conducted in support of proposed bridge replacements in Flagler and St. Johns Counties, Florida. The Florida Department of Transportation (FDOT), District 5, is proposing to replace both bridges carrying State Road (SR) 5 (US 1) over Pellicer Creek at the Flagler and St. Johns County line. The project also includes minor roadway work limited to that necessary to replace the bridges and modification of the existing drainage system as required to complete proposed roadway reconstruction work. The project includes widening of the existing shoulders from 1.2 meters (m) (4.0 feet [ft]) to 1.5 m (5.0 ft) along the roadway segment and 3.0 m (10.0 ft) outside the shoulder on the bridge. An easement across sovereign submerged lands is required to accommodate construction. This project is federally funded for construction in 2026.

The project Area of Potential Effect (APE) was defined as the existing right-of-way (ROW) from approximately 335 m (1,100 ft) south of the Flagler and St. Johns County line to 188 m (617 ft) north of the line. The APE extends to the back or side property lines of parcels adjacent to the ROW or no more than 100 m (330 ft) from the ROW line. SEARCH conducted the archaeological survey within the existing ROW and the architectural survey within the entire APE.

This CRAS was conducted in accordance with the requirements set forth in Section 106 of the National Historic Preservation Act of 1966, as amended, found in 36 CFR Part 800 (Protection of Historic Properties). The studies also comply with Chapter 267 of the Florida Statutes and Rule

www.fdot.gov

Dr. Parsons, SHPO

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Chapter 1A-46, Florida Administrative Code and Section 267.12, Florida Statutes, Chapter 1A-32. All work was performed in accordance with Part 2, Chapter 8 of FDOT's PD&E Manual (revised July 2020), FDOT's Cultural Resources Management Handbook, and the standards stipulated in the Florida Division of Historical Resources' (FDHR) *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The Principal Investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 FR 44716-42). This study also complies with Public Law 113-287 (Title 54 U.S.C.), which incorporates the provisions of the National Historic Preservation Act of 1966, as amended, and the Archeological and Historic Preservation Act of 1974, as amended.

The archaeological survey included the excavation of three shovel tests within the SR 5 (US 1) ROW. SEARCH identified evidence of disturbance from bridge and road construction during subsurface testing. Archaeological testing was not possible along the eastern extent of the ROW due to the presence of multiple marked buried utilities. No archaeological sites, features, or artifact occurrences were encountered during the archaeological survey. No further archaeological work is therefore recommended according to current design. If design changes occur to include areas outside of the current study, additional archaeological consideration may be required.

The architectural survey resulted in identification and evaluation of five historic resources within the Pellicer Creek Bridge Replacement APE, one of which is previously recorded and four of which are newly recorded. The previously recorded resource is a historic roadway (8FL00291/8SJ05271) that was recorded elsewhere within both Flagler and St. Johns Counties but not surveyed within the current project APE. The newly recorded resources consist of one highway bridge (8FL01008/8SJ08262), two buildings (8SJ07379 and 8SJ07380), and one resource group (8SJ07359).

The SHPO evaluated previously recorded resource US 1 (8FL00291/8SJ05271) to be ineligible for listing in the NRHP approximately 3 mi south of the current project area (SEARCH 2018). Based on the results of the current survey, it is the opinion of SEARCH that the segment of US 1 (8FL00291/8SJ05271) within the APE is also ineligible for listing in the NRHP due to loss of historic integrity.

The newly recorded historic bridge, FDOT Bridge No. 730008 (8FL01008/8SJ08262), is one of the oldest remaining bridges along the US 1 corridor in Florida. Based on the current survey, Resource 8FL01008/8SJ08262 is recommended eligible for listing in the NRHP under Criterion A for its role in Florida's transportation history. The bridge is also recommended eligible for listing in the NRHP under Criterion C as a good example of an early tee-beam highway bridge. The three remaining resources within the APE (8SJ07359, 8SJ07379, and 8SJ07380) are recommended ineligible for listing in the NRHP due to a lack of significant historic associations and architectural or landscape distinction.

Contingent upon the SHPO's concurrence with the eligibility recommendations for historic resources presented in this CRAS, a separate Section 106 case study will be prepared to evaluate

Dr. Parsons, SHPO
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any project-related effects to the NRHP-eligible Bridge No. 730008 (8FL01008/8SJ08262). The resolution of project-related effects, if any, will be the subject of further agency consultation.

I respectfully request your concurrence with the findings of the enclosed report.

If you have any questions or need further assistance, please contact Catherine Owen, District Cultural Resource Coordinator, at (386) 943-5383 or me at (386) 943-5411.

Sincerely,



For: William G. Walsh
Environmental Manager
FDOT, District Five

The Florida State Historic Preservation Officer finds the attached Cultural Resource Assessment Survey Report complete and sufficient and concurs / does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 2022-4228. Or, the SHPO finds the attached document contains _____ insufficient information.

In accordance with the Programmatic Agreement among the ACHP, SHPO and FDOT Regarding Implementation of the Federal-Aid Highway Program in Florida, if providing concurrence with a finding of No Historic Properties Affected for a project as a whole, or to No Adverse Effect on a specific historic property, SHPO shall presume that FDOT may approve the project as de minimis use under Section 4(f) under 23 CFR 774.

SHPO Comments:

Kelly L. Chase, DSHPO	<small>Digitally signed by Kelly L. Chase, DSHPO DN: cn=Kelly L. Chase, DSHPO, o, ou, email=kelly.chase@dohs.myflorida.com, c=US Date: 2022.07.06 10:38:32 -0400</small>	7.6.2022
Timothy A. Parsons, PhD, Director Florida Division of Historical Resources	Date	

SECTION 106 CONSULTATION CASE STUDY REPORT FOR THE STATE ROAD 5/US 1 BRIDGE REPLACEMENT OVER PELLICER CREEK, FLAGLER AND ST. JOHNS COUNTIES, FLORIDA

CONSULTANT:	SEARCH 3117 Edgewater Drive, Orlando, FL 32804
ARCHITECTURAL HISTORIAN AND PRINCIPAL INVESTIGATOR:	Mikel Travisano, MS
CLIENT:	Florida Bridge and Transportation, Inc. and Florida Department of Transportation, District 5
DATE:	October 2023
FINANCIAL MANAGEMENT #:	447118-1-52-01

This Section 106 case study report provides an alternatives analysis and effects assessment for the State Road (SR) 5/US 1 bridge replacement over Pellicer Creek in Flagler and St. Johns Counties, Florida (**Figures 1–2**). The Florida Department of Transportation (FDOT), District 5, is proposing to replace both bridges (FDOT Bridge Nos. 730008 and 730045) carrying SR 5/US 1 over Pellicer Creek at the Flagler and St. Johns County line. The project also includes minor roadway work limited to replacing the bridges, as well as modification of the existing drainage system as required to complete proposed roadway reconstruction work. This project will update the bridge typical section, increasing the inside and outside shoulders 2.0 feet (ft) (0.7 meters [m]) from the existing condition to 6.0 ft (1.8 m) inside shoulders and 10.0 ft (3.0 m) outside shoulders. An easement across sovereign submerged lands is required for construction. The purpose of this project is to replace these two functionally obsolete bridges (FDOT Bridge Nos. 730008 and 730045). Because rehabilitation and reuse of the structures is not possible given their current condition, their replacement will ensure safe travel and maintain connectivity along this portion of SR 5/US 1. This project is federally funded for construction in 2026.

SEARCH completed a cultural resource assessment survey (CRAS) for the preferred alternative in June 2022 (Matusik and Newton 2022). The CRAS and subsequent consultation with the State Historic Preservation Officer (SHPO) concluded that one historic property (i.e., a cultural resource listed in or eligible for listing in the National Register of Historic Places [NRHP]) is located within the project area of potential effects (APE). The SHPO concurrence letter for the 2022 CRAS is included as **Attachment A**. This effects assessment will address project-related effects relative to this NRHP-eligible resource, FDOT Bridge No. 730008 (8FL01008/8SJ08262). The second (northbound) US 1 bridge over Pellicer Creek (FDOT Bridge No. 730045) was excluded from Section 106 consideration based on the provisions of the Program Comment (Federal Register 2012:68793) regarding common post-World War II bridge types, and so was not recorded or evaluated as part of the CRAS for this project.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated December 14, 2016, and executed by the Federal Highway Administration and FDOT.

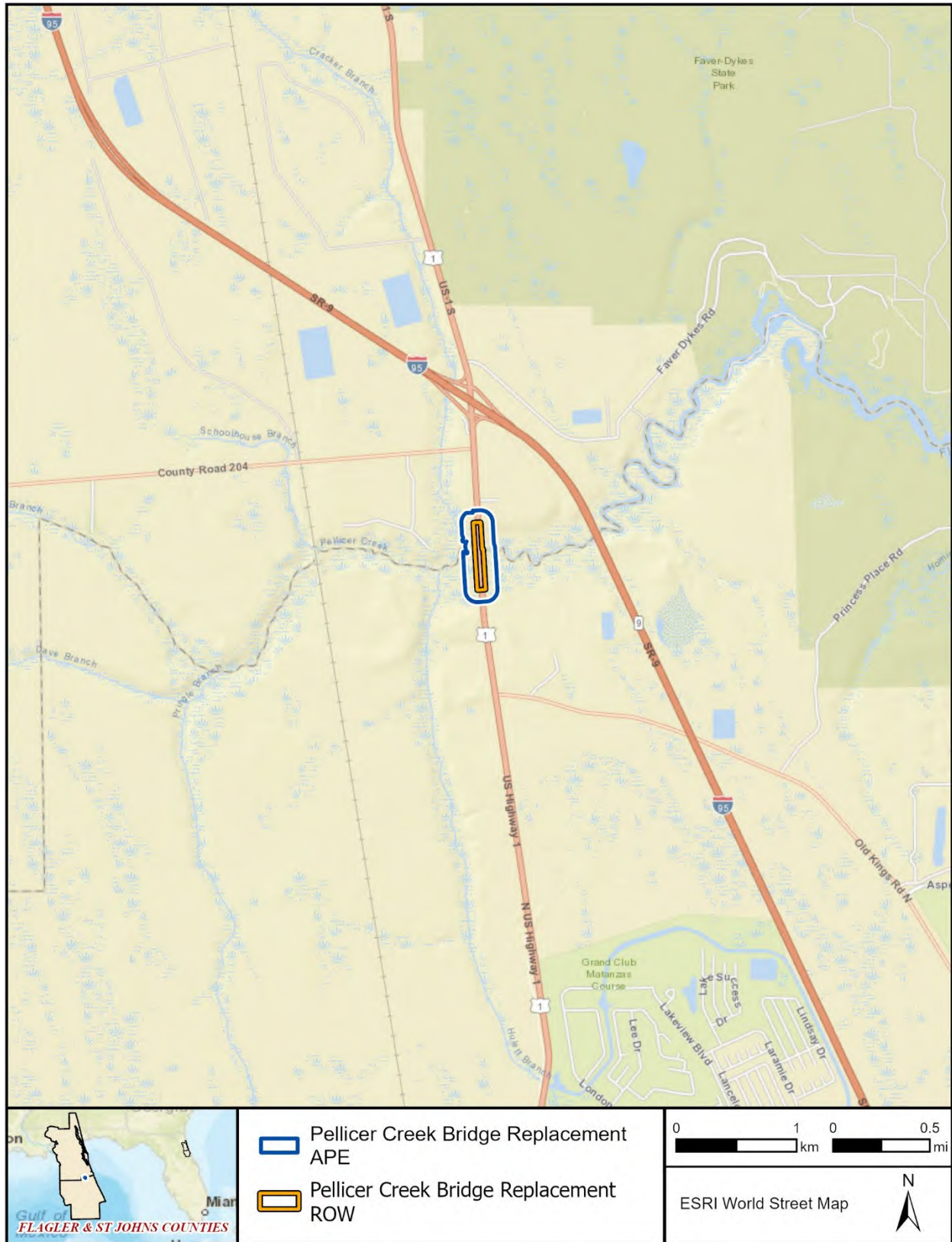


Figure 1. The Pellicer Creek bridge replacement project location in Flagler and St. Johns Counties, Florida.



Figure 2. The Pellicer Creek bridge replacement APE shown on an aerial map of Flagler and St. Johns Counties, Florida.

As a federally funded project, the CRAS and the present effects evaluation were conducted to comply with Public Law 113-287 (Title 54 US Code), which incorporates the provisions of the National Historic Preservation Act (NHPA) of 1966, as amended, and the Archeological and Historic Preservation Act of 1974, as amended. The study also meets the regulations for implementing NHPA Section 106 found in 36 Code of Federal Regulations (CFR) Part 800 (*Protection of Historic Properties*). This study also complies with Chapter 267 of the Florida Statutes and Rule Chapter 1A-46, Florida Administrative Code. SEARCH performed all work in accordance with Part 2, Chapter 8 of the FDOT's Project Development & Environment Manual (revised July 2023) and the Florida Division of Historical Resources' recommendations for such projects as stipulated in *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The principal investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 Federal Register 44716-42).

PROPOSED UNDERTAKING

Project Description

The project consists of replacing the parallel bridges (FDOT Bridge Nos. 730008 and 730045) at SR 5/US 1 over Pellicer Creek in Flagler and St. Johns Counties. This project will update the bridge typical section, increasing the inside and outside shoulders 2.0 ft (0.7 m) from the existing condition to 6.0 ft (1.8 m) inside shoulders and 10.0 ft (3.0 m) outside shoulders. The project also includes roadway work limited to replacing the bridges, as well as modification of the existing drainage system as required to complete proposed roadway reconstruction work. The recommended alternative is Alternative 1B, which was selected based on overall cost, constructability and construction time, and maintenance and right-of-way (ROW) impacts. A discussion of all alternatives considered by the project is included below.

Purpose and Need

The purpose of this project is to replace two functionally obsolete bridges that do not meet current road design standards (FDOT Bridge Nos. 730008 and 730045; Florida Bridge and Transportation, Inc. 2023). Their replacement will ensure safe travel and maintain connectivity along this portion of SR 5/US 1.

Alternatives

The Bridge Development Report (BDR) provides four alternatives that were considered, inclusive of the recommended alternative (1B) (**Attachment B**). The project engineering team also provided SEARCH with information regarding Rehabilitation and No-Build alternatives for the purposes of the present Section 106 Case Study, although this is not included in the BDR. All six alternatives are included in **Table 1**.

Table 1. Matrix Analysis of Bridge Alternatives.

Alternatives	Description of Superstructure	Bridge Length	Span Length	Number of Spans	Description of Substructure	Bridge Cost
1A	Florida I Beams	240.0 ft (73.2 m)	80.0 ft (24.4 m)	3	18.0-in PPC Piles	\$6,769,679
1B	Florida I Beams	240.0 ft (73.2 m)	80.0 ft (24.4 m)	3	24.0-in PPC Piles	\$7,046,555
2A	Florida Slab Beams	240.0 ft (73.2 m)	60.0 ft (18.3 m)	4	18.0-in PPC Piles	\$8,303,265
2B	Florida Slab Beams	240.0 ft (73.2 m)	60.0 ft (18.3 m)	4	24.0-in PPC Piles	\$8,603,803
*Rehabilitation	In-kind replacement	No Change	No Change	No Change	Rehabilitation (crutch bent installation)	\$8,712,896
*No Build	No Change	No Change	No Change	No Change	No Change	NA

Source: (Florida Bridge and Transportation, Inc. 2023: 3, 20)

*Not included in the BDR.

The BDR also provides an evaluation matrix for rating four bridge alternatives, (1A, 1B, 2A, and 2B). The matrix ranks each alternative from 1-10 using a weighted score and multiplier, with 100 being the highest possible score (Florida Bridge and Transportation, Inc. 2023:23). The scores for each alternative discussed in the BDR are provided in the descriptions below.

Alternative 1A

Alternative 1A uses Florida I Beams and has three spans of 80.0 ft (73.2 m) each. Alternative 1A uses 18.0 in (457.2 mm) Precast Prestressed Concrete (PPC) Piles, while Alternative 1B (discussed below) uses 24.0 in (609.6 mm) PPC piles. While the smaller-diameter piles are easier to drive, they are not as efficient for the required length and will require preplanned pile splices that are more expensive and need a high degree of quality control during installation. The smaller-diameter piles would also likely require pile jackets in the future, while the larger-diameter piles would not. The use of Florida I Beams for Alternative 1A is preferable for construction time as the Florida Slab Beam Alternatives 2A and 2B would require an additional intermediate bent to support the bridge structure (Florida Bridge and Transportation, Inc. 2023).

Bridge maintenance for all alternatives is similar because concrete bridges are durable with a low-maintenance record. ROW impacts are anticipated for all alternatives. The Florida Slab Beam Alternatives 2A and 2B are preferable to Alternative 1A for having the profile of the new road and bridge meet the existing road profile; this can be completed faster because of the lower beam depth on Alternatives 2A and 2B.

The cost of Alternative 1A is estimated at \$6,769,679, making it the least expensive build alternative. Given the drawbacks regarding maintenance and ROW impacts, Alternative 1A received an 87.0 score and was ranked second among the four build alternatives (Florida Bridge and Transportation, Inc. 2023: 23).

Advantages of Alternative 1A:

- Florida I Beams do not require an additional intermediate bent to support the bridge structure; and
- Least expensive of the four alternatives.

Disadvantages of Alternative 1A:

- 18.0 in (457.2 mm) PPC piles require preplanned pile splices that are more expensive and need a high degree of quality control during installation, and these smaller-diameter piles may require future maintenance (pile jackets) to extend bridge life; and
- Higher depth of Florida I Beams when compared to the Florida Slab Beam may complicate the profile of the new road and bridge meeting the existing road profile.

Alternative 1B

The constructability of Alternative 1B is similar to 1A: it uses Florida I Beams and has three spans of 80.0 ft (73.2 m). The main difference is that Alternative 1B uses 24.0 in (609.6 mm) PPC piles, while Alternative 1A uses 18.0 in (457.2 mm) PPC piles, which are smaller in diameter and easier to drive. However, they are not as efficient for the required length and will require preplanned pile splices that are more expensive and need a high degree of quality control during installation. The use of Florida I Beams for Alternative 1B is preferable for construction time because the Florida Slab Beam Alternatives 2A and 2B would require an additional intermediate bent to support the bridge structure (Florida Bridge and Transportation, Inc. 2023).

Bridge maintenance for all alternatives is similar because concrete bridges are durable with a low-maintenance record. ROW impacts are anticipated for all alternatives. The Florida Slab Beam Alternatives 2A and 2B are preferable to Alternative 1B for having the profile of the new road and bridge meet the existing road profile; this can be completed faster because of the lower beam depth on Alternatives 2A and 2B. However, the primary consideration for maintenance and ROW is using 24.0 in (609.6 mm) PPC piles instead of 18.0 in (457.2 mm) PPC piles because the smaller-diameter piles will require preplanned pile splices, and on the existing bridges, they already require pile jackets. Furthermore, it's anticipated that using 24.0 in (609.6 mm) PPC piles will lessen the need for future maintenance (pile jackets) and extend bridge life. Therefore, Alternative 1B has advantages relative to maintenance and ROW impacts.

The cost of Alternative 1B is estimated at \$7,046,555, making it the second least expensive option. Alternative 1B received a 91.6 score and was ranked first among the four building alternatives, making it the recommended alternative (Florida Bridge and Transportation, Inc. 2023: 23).

Advantages of Alternative 1B:

- Florida I Beams do not require an additional intermediate bent to support the bridge structure;
- 24.0 in (609.6 mm) PPC piles do not need preplanned pile splices and are more durable than 18.0 in (457.2 mm) PPC piles, lessening the need for future maintenance and extending bridge life; and
- Second least expensive of the four alternatives.

Disadvantages of Alternative 1B:

- Higher depth of Florida I Beams when compared to the Florida Slab Beam may complicate the profile of the new road and bridge meeting the existing road profile.

Alternative 2A

Alternative 2A uses Florida Slab Beams and has three spans of 80.0 ft (73.2 m). Alternative 2A uses 18.0 in (457.2 mm) PPC Piles, while Alternative 2B uses 24.0 in (609.6 mm) PPC piles. While the smaller-diameter piles are easier to drive, they are not as efficient for the required length and will require preplanned pile splices that are more expensive and need a high degree of quality control. In addition, the Florida Slab Beam Alternatives 2A and 2B require an additional intermediate bent to support the bridge structure (Florida Bridge and Transportation, Inc. 2023), which would likely increase construction time when compared to Alternatives 1A and 1B.

Bridge maintenance for all alternatives is similar because concrete bridges are durable with a low-maintenance record. ROW impacts are anticipated for all alternatives. The Florida Slab Beam Alternatives 2A and 2B are preferable to Alternatives 1A and 1B for having the profile of new road and bridge meet the existing road profile; this can be completed faster because of the lower beam depth on Alternatives 2A and 2B. However, the primary consideration for maintenance and ROW is using 24.0 in (609.6 mm) PPC piles instead of 18.0 in (457.2 mm) PPC piles because the smaller-diameter piles will require preplanned pile splices and will require pile jackets at some point in the future, while the larger diameter piles would not. Therefore, Alternative 2A is at a disadvantage with regard to maintenance and ROW impacts.

The cost of Alternative 2A is estimated at \$8,303,265, making it the second most expensive option. Alternative 2A received a 72.4 score and was ranked fourth, the lowest score of all the build alternatives (Florida Bridge and Transportation, Inc. 2023: 23).

Advantages of Alternative 2A:

- Lower depth of Florida Slab Beam when compared to the Florida I Beams is preferable to Alternatives 1A and 1B for having the profile of new road and bridge meet the existing road profile; this can be completed faster because of the lower beam depth.

Disadvantages of Alternative 2A:

- 18.0 in (457.2 mm) PPC piles require preplanned pile splices that are more expensive and need a high degree of quality control during installation, and these smaller-diameter piles may require future maintenance (pile jackets) to extend bridge life;
- Requires an additional intermediate bent to support the bridge structure; and
- Second most expensive of the four alternatives.

Alternative 2B

The constructability of Alternative 2B is similar to 2A: it uses Florida Slab Beams and has three spans of 80.0 ft (73.2 m). The main difference is that Alternative 2B uses 24.0 in (609.6 mm) PPC piles, while Alternative 2A uses 18.0 in (457.2 mm) PPC piles. While the smaller-diameter piles are easier to drive, they are not as efficient for the required length and will require preplanned pile splices that are more expensive and need a high degree of quality control.

In addition, the Florida Slab Beam Alternatives 2A and 2B require an additional intermediate bent to support the bridge structure (Florida Bridge and Transportation, Inc. 2023), which would likely increase construction time when compared to Alternatives 1A and 1B.

Bridge maintenance for all alternatives is similar because concrete bridges are durable with a low-maintenance record. ROW impacts are anticipated for all alternatives. The Florida Slab Beam Alternatives 2A and 2B are preferable to Alternatives 1A and 1B for having the profile of new road and bridge meet the existing road profile; this can be completed faster because of the lower beam depth on Alternatives 2A and 2B. Further, it is anticipated that using 24.0 in (609.6 mm) PPC piles will lessen the need for future maintenance (pile jackets) and extend bridge life. Therefore, Alternative 2B has advantages relative to maintenance and ROW impacts.

Finally, the cost of Alternative 2B is estimated at \$8,603,803, making it the most expensive option. Alternative 2B received a 78.2 score and was ranked third among the four build alternatives (Florida Bridge and Transportation, Inc. 2023: 23).

Advantages of Alternative 2B:

- Lower depth of Florida Slab Beam when compared to the Florida I Beams is preferable to Alternatives 1A and 1B for having the profile of new road and bridge meet the existing road profile; this can be completed faster because of the lower beam depth; and
- 24.0 in (609.6 mm) PPC piles do not need preplanned pile splices and are more durable than 18.0 in (457.2 mm) PPC piles, lessening the need for future maintenance to extend bridge life.

Disadvantages of Alternative 2B:

- Requires an additional intermediate bent to support the bridge structure; and
- Most expensive of the four alternatives.

Rehabilitation Alternative

A rehabilitation alternative was not specifically included in the BDR; however, the inspection report and BDR state that FDOT Bridge No. 730008 (8FL01008/8SJ08262) over Pellicer Creek is “functionally obsolete” (**Attachment C**). This designation indicates that the bridge does not meet current road design standards due to narrow shoulder width (the current shoulder width is 2.0 ft [0.7 m], while the standard is 6.0 ft [1.8 m] for the inside shoulder and 10.0 ft [3.0 m] for the outside shoulders). At 96 years old, the bridge is already past its design life of 75 years. Furthermore, FDOT Bridge No. 730008 already has pile jackets installed to extend its service life. Replacing the pile jackets would require extensive work while trying to avoid damage to the original piles, while adding more width to the existing pile jackets would affect the hydraulic opening of the bridge over the creek. Even if repairs were to occur, the substandard shoulders would remain, which would pose a safety hazard per current FDOT standards. As such, the rehabilitation alternative is not a viable option because it does not meet the purpose and need of the project (Florida Bridge and Transportation, Inc. 2023: 23).

No-Build Alternative

A no-build alternative was not specifically included in the BDR because it would require maintaining in service a “functionally obsolete” bridge that is 21 years past its design life. A no-build option would entail no changes or construction to the existing bridge, and no structural elements would be removed or added. The no-build alternative would retain the bridge’s substandard 2.0 ft (0.7 m) shoulders and piles that have already been repaired with the installation of pile jackets. As such, the no-build alternative is not a viable option and does not meet the purpose and need of the project (Florida Bridge and Transportation, Inc. 2023: 23).

CRITERIA OF ADVERSE EFFECTS

To evaluate the project-related effects posed by the preferred alternative on the NRHP-eligible historic resource, SEARCH applied the criteria of adverse effects, as described by 36 CFR 800:

(a) Assessment of Adverse Effects

(1) *Criteria of adverse effects.* An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association. Consideration shall be given to all qualifying characteristics of a historic property, including those that may have been identified subsequent to the original evaluation of the property's eligibility for the National Register. Adverse effects may include reasonably foreseeable effects caused by the undertaking that may occur later in time, be farther removed in distance, or be cumulative.

(2) *Examples of adverse effects.* Adverse effects include, but are not limited to:

- (i) Physical destruction of or damage to all or part of the property;
- (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary of the Interior's *Standards for the Treatment of Historic Properties* (36 CFR part 68) and applicable guidelines;
- (iii) Removal of the property from its historic location;
- (iv) Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;
- (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features;
- (vi) Neglect of a property that causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and
- (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

(b) *Finding of no adverse effect.* The agency official, in consultation with the SHPO/THPO, may propose a finding of no adverse effect when the undertaking's effects do not meet the criteria of paragraph (a)(1) of this section or the undertaking is modified or conditions are imposed, such as the subsequent review of plans for rehabilitation by the SHPO/THPO to ensure consistency with the Secretary's standards for the treatment of historic properties (36 CFR part 68) and applicable guidelines, to avoid adverse effects.

NATIONAL REGISTER OF HISTORIC PLACES CONTEXT

Brief Historic Background

Bridge No. 730008 was built in 1927 and exhibits tee-beam construction. The 2012 edition of *Historic Highway Bridges of Florida* discusses historic tee-beam bridges, noting that they were commonly constructed in the 1930s and 1940s as integral cast-in-place reinforced concrete decks and beam stems (Deming et al. 2012). These types of bridges became prevalent during this period because they were relatively easy and cost-effective to build and provided long-lasting durability. While many historic tee-beam bridges were identified and evaluated during the 2012 study, Bridge No. 730008 was not included. However, with a construction date of 1927, Bridge No. 730008 is considered an early example of the type. Furthermore, few bridges from this early date along US 1 are still extant in Florida. Bridge No. 730008 underwent reconstruction in 1948, according to FDOT records. This involved widening the superstructure by 13.3 ft (4.1 m).

The existing outer railing on the bridge was also constructed at this time and is marked with the date “1948.” This widening was the last major alteration to the bridge, with some additional minor work in the 1990s and 2000s, including the addition of a vertical face retrofit and the installation of pile jackets on all 18.0 in (457.2 mm) piles on the interior bents. In 1957, US 1 became a divided highway when the eastern lane and the northbound bridge (Bridge No. 730045) were constructed; afterwards, Bridge No. 730008 was utilized solely for southbound traffic.

Historic Resources

8FL01008/8SJ08262, US 1 Southbound over Pellicer Creek Bridge (FDOT Bridge No. 730008)

FDOT Bridge No. 730008 (8FL01008/8SJ08262) (**Figure 3**) is 223.8 ft (68.2 m) long and features seven main spans with no approach spans. The roadway width is 27.9 ft (8.5 m), and the complete width from edge to edge is 32.8 ft (10 m). The bridge carries two lanes of southbound traffic, and the deck and abutment are composed of cast-in-place concrete. The bridge supports feature a standard six-pile bent design. No plaques are present on the bridge, but “H-15” and “1948” are visible on the rail, the latter of which is the year of the bridge’s reconstruction.

During the 2022 CRAS, Bridge No. 730008 was determined eligible for the NRHP under Criterion A for its role in Florida’s transportation history. Although Bridge No. 730008 is a common bridge type, it remains an early and relatively intact example of a tee-beam bridge from the Florida boom period of the 1920s, an era from which remaining bridges are becoming increasingly rare. Due to its engineering significance as an early representation of a bridge type that was constructed across Florida due to its affordability and constructability, FDOT Bridge No. 730008 (8FL01008/8SJ08262) was also determined eligible for listing in the NRHP under Criterion C.

ASSESSMENT OF EFFECTS

The project proposes replacing FDOT Bridge No. 730008 (8FL01008/8SJ08262) over Pellicer Creek and the construction of a new bridge in its place. The BDR prepared by the project engineering consultant concluded that the only reasonable alternative would be replacement of the bridge (Florida Bridge and Transportation, Inc. 2023; see **Attachment B**). As discussed in the BDR, the bridge must be replaced due to multiple factors related to the age of the structure, the structural capacity and condition of the bridge, and the substandard roadway geometry compared to current FDOT standards.

At present, FDOT Bridge No. 730008 is 96 years old, which is well beyond the 75-year design life for the bridge. Structurally, the bridge has had pile jackets installed as a repair procedure and as a preventative measure to keep the existing piles from deteriorating further. The pile jackets themselves have spalls, voids, and exposed rebar per the most recent inspection reports. Further, the bridge has been labeled as “functionally obsolete,” which means it does not meet current



Figure 3. Representative views of Resource 8FL01008/8SJ08262. Deck and roadway, facing southeast (top left); underside with girders and piers, facing southeast (top right); west side of structure, facing south (middle left); railing along east side, facing south (middle right); railing and east side, facing north (bottom left); east side and piers, facing northwest (bottom right).

road design standards due to narrow shoulder width (the current shoulder width is 2.0 ft [0.7 m], and the standard is 6.0 ft [1.8 m] for the inside shoulder and 10.0 ft [3.0 m] for the outside shoulders).

Repairing the bridge is not recommended since the structure is beyond its design life. Replacing the pile jackets would require extensive work while trying to avoid damage to the original piles, while adding more width to the existing pile jackets would affect the hydraulic opening of the bridge over the creek. Even if repairs were to occur, the substandard shoulders would remain, which would pose a safety hazard per today's standards. For these reasons, the rehabilitation and no-build alternatives were dismissed from consideration for not meeting the purpose and need for the undertaking, and the bridge is recommended for replacement.

Because rehabilitation and continued use of this historic property are not feasible given its current condition, SEARCH evaluated project-related effects posed by the four build alternatives. All four build alternatives involve removal and replacement of 8FL01008/8SJ08262, resulting in a total loss of historic fabric. As such, SEARCH recommends that implementation of any of the four build alternatives will result in an adverse effect to NRHP-eligible FDOT Bridge No. 730008 (8FL01008/8SJ08262).

CONCLUSION

This Section 106 case study report provides an alternatives analysis and effects discussion regarding the SR 5/US 1 bridge replacement over Pellicer Creek. Specifically, this document discusses project-related effects relative to NRHP-eligible FDOT Bridge No. 730008 (8FL01008/8SJ08262). Based on a review of the project plans and viable alternatives, it is SEARCH's opinion that the project will result in an adverse effect to the historic bridge. As such, SEARCH recommends consultation with the Florida SHPO to develop appropriate mitigation measures to resolve the adverse effect under Section 106 of the NHPA. Once mitigation methods are determined, these commitments should be presented in a Memorandum of Agreement between FDOT and the SHPO.

REFERENCES CITED

Deming, Joan, Kisa Hooks, and Elaine Lund

2012 *The Historic Highway Bridges of Florida. Florida Master Site File Survey No. 20057.* On file, Florida Division of Historical Resources, Tallahassee, and Florida Department of Transportation Environmental Management Office. Electronic document, <https://www.fdot.gov/docs/default-source/environment/pubs/Historic-Highway-Bridges-of-Florida-2010-Update.pdf>, accessed May, 2022.

Federal Register

2012 Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges. US Government Printing Office, Washington, DC.

Florida Bridge and Transportation, Inc.

2023 *Bridge Development Report: SR 5/US 1 over Pellicer Creek Bridge Replacement.* On file, Florida Bridge and Transportation Office, Orlando, Florida.

Matusik, Angela and Jason Newton

2022 *Cultural Resource Assessment Survey of the SR 5 Bridge Replacement over Pellicer Creek, Flagler and St. Johns Counties, Florida.* FMSF No. 28226. On file, Division of Historical Resources, Tallahassee, Florida.

Natural Resources Appendix

Contents:

Recreation and Conservation Areas Map

USFWS Species Concurrence Letter

Wetlands Map

Floodplains Map

Essential Fish Habitat Concurrence

Conserved Areas Map



Conserved Areas Map




Florida Department of Transportation

RON DESANTIS
GOVERNOR

Ta

June 17, 2024

U.S. Fish and Wildlife Service
7915 Baymeadows Way, Suite 200
Jacksonville, FL 32256
c/o Zakia Williams

	Florida Ecological Services Field Office
	Service Project Code No. 24-I-0116837
The U.S. Fish and Wildlife Service has reviewed the information provided and finds that the proposed action is not likely to adversely affect any federally listed species or designated critical habitat protected by the Endangered Species Act of 1973 (Act), as amended (16 U.S.C. 1531 et. seq.). A record of this consultation is on file at the Florida Ecological Services Field Office.	
This fulfills the requirements of section 7 of the Act and further action is not required. If modifications are made to the project, if additional information involving potential effects to listed species becomes available, or if a new species is listed, reinitiation of consultation may be necessary.	
CATRINA MARTIN	Digitally signed by CATRINA MARTIN Date: 2024.07.31 08:33:48 -05'00'
Supervisor	

Subject: Request for Concurrence under Section 7 of the ESA
SR 5 over Pellicer Creek
Flagler and St. Johns Counties, Florida
FPID 447118-1

Dear Ms. Williams:

The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. 327 and a Memorandum of Understanding dated December 14, 2016, and executed by the Federal Highway Administration and FDOT. FDOT, acting as the lead federal agency, has determined that the SR 5 bridges over Pellicer Creek are structurally deficient, functionally obsolete, and must be replaced. The project includes replacing the northbound and southbound bridges to current design standards.

FDOT considered the direct, indirect, and cumulative effects of the project on threatened, endangered, and candidate species for listing under the Endangered Species Act (ESA). Three species were evaluated due to their suitable habitat near the project area. The species include:

- Eastern indigo snake
- Florida scrub-jay
- Wood stork

The project is located entirely within the U.S. Fish and Wildlife Service's (USFWS) Consultation Area for the Florida scrub-jay and core foraging area for one wood stork nesting colony (606109). Additionally, the project area contains freshwater wetland

habitats that may be suitable for the eastern indigo snake. Applying the 2017 USFWS Consultation Key for the Eastern Indigo Snake, FDOT arrived at the sequential effect determination of $A > B > C$ = “not likely to adversely affect” the eastern indigo snake due to a lack of gopher tortoise habitat. Per the 2008 USFWS Wood Stork Effect Determination Key for Central and North Peninsular Florida, FDOT applied the sequential effect determination of $A > B > C$ = “not likely to adversely affect” the wood stork since the project will impact < 0.50 acres of suitable foraging habitat. The project will have “no effect” on the Florida scrub-jay due to a lack of natural upland habitat in the project area.

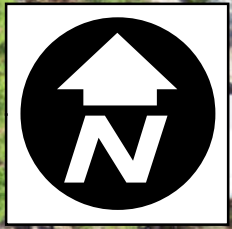
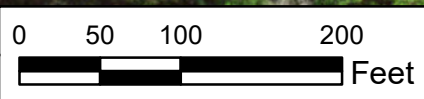
With the submittal of this letter, the project narrative, and the highlighted effect determination keys, FDOT requests informal consultation and concurrence per 50 CFR § 402.13(c) for the referenced bridge replacement project. If you have any questions, please contact Jennifer Cappelletti, FDOT Environmental Permits Supervisor, via the information provided below. Thank you for your continued assistance.

Sincerely,

DocuSigned by:

E130FC5F7AD2193...

Jennifer Cappelletti
FDOT Environmental Permits Supervisor
District Five
(386) 943-5437
Jennifer.Ferngren@dot.state.fl.us



Legend

- WL Points
- WL Lines
- NWI

National Flood Hazard Layer FIRMette

SR 50 US COVER PELICER CREEK BRIDGE REPLACEMENT / 447176-1-02-01



81°17'31"W 29°39'21"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

- | | | |
|------------------------------------|--|---|
| SPECIAL FLOOD HAZARD AREAS | | Without Base Flood Elevation (BFE)
Zone A, V, A50 |
| | | With BFE or Depth Zone AE, AO, AH, VE, AR |
| | | Regulatory Floodway |
| OTHER AREAS OF FLOOD HAZARD | | 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X |
| | | Future Conditions 1% Annual Chance Flood Hazard Zone X |
| | | Area with Reduced Flood Risk due to Levee. See Notes. Zone X |
| | | Area with Flood Risk due to Levee Zone D |
| OTHER AREAS | | NO SCREEN Area of Minimal Flood Hazard Zone X |
| | | Effective LOMRs |
| | | Area of Undetermined Flood Hazard Zone B |
| GENERAL STRUCTURES | | Channel, Culvert, or Storm Sewer |
| | | Levee, Dike, or Floodwall |
| OTHER FEATURES | | 20.2 Cross Sections with 1% Annual Chance Water Surface Elevation |
| | | 17.5 Coastal Transect |
| | | Base Flood Elevation Line (BFE) |
| | | Limit of Study |
| | | Jurisdiction Boundary |
| | | Coastal Transect Baseline |
| | | Profile Baseline |
| | | Hydrographic Feature |
| MAP PANELS | | Digital Data Available |
| | | No Digital Data Available |
| | | Unmapped |

The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards.

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 4/6/2022 at 10:18 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmoderated areas cannot be used for regulatory purposes.



UNITED STATES DEPARTMENT OF COMMERCE
National Oceanic and Atmospheric Administration
NATIONAL MARINE FISHERIES SERVICE
Southeast Regional Office
263 13th Avenue South
St. Petersburg, Florida 33701-5505
<https://www.fisheries.noaa.gov/region/southeast>

8/16/2024

F/SER31:KG
SERO-2024-01135

Jennifer Ferngren Cappelletti,
Environmental Permits Supervisor
Florida Department of Transportation, District 5
719 South Woodland Boulevard
Deland, Florida 32720

Ref.: SERO-2024-01135, Florida Department of Transportation SR5 US1 Bridge Replacement over Pellicer Creek, in Flagler and St. Johns Counties, Florida – EXPEDITED TRACK

Dear Jennifer Cappelletti,

This letter responds to your May 23, 2024 request pursuant to Section 7 of the Endangered Species Act (ESA) for initiation of consultation with the National Marine Fisheries Service (NMFS) on the subject action. Five ESA-listed species under NMFS' purview were determined to not likely to be adversely affected.

We reviewed the action agency's consultation request document and related materials. Based on our knowledge, expertise, and the action agency's materials, we concur with the action agency's conclusions that the proposed action is not likely to adversely affect the NMFS ESA-listed species or designated critical habitat.

Updates to the regulations governing interagency consultation (50 CFR part 402) were effective on May 6, 2024 (89 Fed. Reg. 24268). We are applying the updated regulations to this consultation. The 2024 regulatory changes, like those from 2019, were intended to improve and clarify the consultation process, and, with one exception from 2024 (offsetting reasonable and prudent measures), were not intended to result in changes to the Services' existing practice in implementing section 7(a)(2) of the Act. 84 Fed. Reg. at 45015; 89 Fed. Reg. at 24268. We have considered the prior rules and affirm that the substantive analysis and conclusions articulated in this letter of concurrence would not have been any different under the 2019 regulations or pre-2019 regulations.

This concludes your consultation responsibilities under the ESA for species and/or designated critical habitat under NMFS's purview. Reinitiation of consultation is required and shall be requested by the action agency where discretionary Federal action agency involvement or control over the action has been retained or is authorized by law and: (a) take occurs; (b) new information reveals effects of the action that may affect listed species or critical habitat in a manner or to an extent not previously considered in this consultation; (c) the action is subsequently modified in a manner that causes an effect to the listed species or critical habitat not previously considered in this consultation; or (d) if a new species is listed or critical habitat designated that may be affected by the action.

We look forward to further cooperation with you on other projects to ensure the conservation of our threatened and endangered marine species and designated critical habitat. If you have any questions on this consultation, please contact Kurtis Gregg, Consultation Biologist, at (561) 291-9843 or by email at Kurtis.Gregg@noaa.gov.

Sincerely,

REECE.KARLA.MICHELLE.1365
HELLE.1365885962
Digitally signed by
REECE.KARLA.MICHELLE.1365
885962
Date: 2024.08.16 15:33:06 -04'00'

for

David Bernhart
Assistant Regional Administrator
for Protected Resources

File: 1514-22.1.4

Public Involvement Appendix

Contents:

Notice of Opportunity

THE DAYTONA BEACH
NEWS-JOURNAL PO Box 631244 Cincinnati, OH 45263-1244

AFFIDAVIT OF PUBLICATION

Mary Bierwagen
Mary Bierwagen PLEMO- cc:501
Florida Dept Of Transportation
719 S Woodland BLVD # 5
Deland FL 32720-6834

STATE OF WISCONSIN, COUNTY OF BROWN

Before the undersigned authority personally appeared, who on oath says that he or she is the Legal Coordinator of The News-Journal, published in Volusia and Flagler Counties, Florida; that the attached copy of advertisement, being a Govt Public Notices, was published on the publicly accessible website of Volusia and Flagler Counties, Florida, or in a newspaper by print in the issues of, on:

08/16/2024

Affiant further says that the website or newspaper complies with all legal requirements for publication in chapter 50, Florida Statutes.

Subscribed and sworn to before me, by the legal clerk, who is personally known to me, on 08/16/2024

Keegan Loren

Legal Clerk

Will Feltz

Notary, State of WI, County of Brown

3.7.27

My commission expires

Publication Cost: \$562.48
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THIS IS NOT AN INVOICE!

Please do not use this form for payment remittance.

KAITLYN FELTY
Notary Public
State of Wisconsin



U.S. 1/State Road (S.R.) 5 Over
Pellicer Creek Bridge Replacement
Financial Project Number 447118-1

The FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces a notice of opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried-out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

Project Name: U.S. 1/State Road (S.R.) 5 Over Pellicer Creek Bridge Replacement
Type of Project: Project Development & Environment (PD&E) Study
Project Limits: Flagler County / St. Johns County Line
City/County: City of Palm Coast in Flagler County and St. Johns County, Florida
Financial Project Identification (FPID) Number: 447118-1
Federal-Aid Project Number: N/A

FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek, and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements.

The PD&E study is evaluating and documenting potential engineering and natural, physical, socioeconomic, and cultural environmental effects of the proposed improvements. FDOT is conducting the PD&E study in accordance with the requirements of the National Environmental Policy Act and other applicable federal and state laws and regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.flroads.com/project/447118-1 through August 31, 2024.

Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to:

Florida Department
of Transportation
Project Manager
Shelley ChinQuee, P.E.
719 South Woodland Boulevard
DeLand, FL 32720
386-943-5439
Shelley.ChinQuee@dot.state.fl.us

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.



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FAR Issue Vol. 50/No. 161

Section XII

Issue Date: August 16, 2024

Total number of notices in Section XII: 3

Notice / Adopted	Section	Description	ID	Publish Date
	Misc Agency 1	Index of Administrative Rules Filed with the Secretary of State	28657891	8/16/2024 Vol. 50/161
	Misc Agency 62	State Revolving Fund (SRF) Program	28642759	8/16/2024 Vol. 50/161
	Misc Agency 1000T46	Notice of Opportunity	28636551	8/16/2024 Vol. 50/161

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Miscellaneous

OTHER AGENCIES AND ORGANIZATIONS**Quest Corporation of America, Inc.**

Notice of Opportunity

THE FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces an opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

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 Type of Project: Project Development & Environment (PD&E) Study
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 City/County: City of Palm Coast in Flagler County and St. Johns County, Florida
 Financial Project Identification (FPID) Number: 447118-1
 Federal-Aid Project Number: N/A

Project Description:

FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements. The PD&E study is evaluating and documenting potential engineering and natural, physical, socioeconomic, and cultural environmental effects of the proposed improvements. FDOT is conducting the PD&E study in accordance with the requirements of the National Environmental Policy Act and other applicable federal and state laws and regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.cflroads.com/project/447118-1 through August 31, 2024. Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to: Florida Department of Transportation, Shelley ChinQuee, P.E., Project Manager, 719 South Woodland Boulevard, DeLand, FL 32720, or by email at Shelley.ChinQuee@dot.state.fl.us.

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.



Florida Department of

TRANSPORTATION

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District Five

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. For additional information please contact the [Equal Opportunity Office](#).

If you need special accommodations under the American with Disabilities Act or require translation services (free of charge) please contact the person listed in the notice at least seven days before the meeting.

TECHNICAL REVIEW AND AWARD COMMITTEE MEETINGS

District: Five
Meeting Type: Meeting
Location Name: Florida Department of Transportation, District Five Office
Street Address: 719 S. Woodland Blvd.
City: Deland
Website: [District 5 Technical Review and Award Committee Meeting Notices](#)

OPPORTUNITY FOR PUBLIC HEARING: U.S. 1/S.R. 5 OVER PELLICER CREEK BRIDGE REPLACEMENT

District: Five
Meeting Type: Hearing
Date: Friday, August 16, 2024
Time: 2:00 pm to N/A
Purpose: The FLORIDA DEPARTMENT OF TRANSPORTATION (FDOT) announces a notice of opportunity to request a public hearing. The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried-out by FDOT pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

PURPOSE: Notice is hereby given that interested persons may request a location and design concept public hearing for:

Project Name: U.S. 1/State Road (S.R.) 5 Over Pellicer Creek Bridge Replacement

Type of Project: Project Development & Environment (PD&E) Study

Project Limits: Flagler County / St. Johns County Line

City/County: City of Palm Coast in Flagler County and St. Johns County, Florida

Financial Project Identification (FPID) Number: 447118-1

Federal-Aid Project Number: N/A

Project Description: FDOT is proposing to replace the northbound and southbound U.S. 1 bridges over Pellicer Creek at the Flagler County / St. Johns County line. The existing southbound bridge was constructed in 1927, and the northbound bridge was constructed in 1957. The purpose of the project is to provide modern bridges to ensure continued safety and mobility along U.S. 1 in this area. The proposed bridges will have two travel lanes in each direction and wider shoulders to accommodate bicyclists. The bridges will maintain similar clearance over Pellicer Creek, and the bridge opening will be wider than the existing opening. Additional improvements include reconstructing the roadway at the bridge approaches and providing minor drainage improvements.

The PD&E study is evaluating and documenting potential engineering and natural, physical, socioeconomic, and cultural environmental effects of the proposed improvements. FDOT is conducting the PD&E study in accordance with the requirements of the National Environmental Policy Act and other applicable federal and state laws and regulations.

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

The Approved Section 106 Case Study Report, the Draft Section 106 MOA, and the Draft Programmatic Section 4(f) Evaluation are available for public review on the project webpage at www.cflroads.com/project/447118-1 through August 31, 2024.

Persons who have questions or comments or those interested in requesting a location and design concept public hearing should submit a request in writing to:

Florida Department of Transportation Project Manager Shelley ChinQueue, P.E. 719 South Woodland Boulevard DeLand, FL 32720 386-943-5439 Shelley.ChinQueue@dot.state.fl.us

The request should be postmarked no later than 15 calendar days following the publication of this notice or by August 31, 2024.

Project Web Site: <https://www.cflroads.com/project/447118-1>

Primary Contact: Shelley ChinQueue

Primary Phone: 386-943-5439

Primary E-Mail: Shelley.ChinQueue@dot.state.fl.us

Expires: 9/1/2024

PUBLIC INFORMATION MEETING: GARDEN STREET (S.R. 406) RESURFACING

District: Five

Meeting Type: Open House

Date: Thursday, August 22, 2024

Time: 5:30 pm to 7:00 pm

Location Name: Good Shepherd Lutheran Church

Street Address: 2073 Garden St.

City: Titusville

Purpose: The Florida Department of Transportation (FDOT) will hold a public meeting regarding project plans on Garden Street (State Road (S.R.) 406) from west of Interstate 95 (I-95) to Forrell Avenue in Titusville on Thursday, August 22, at 5:30 p.m.

The purpose of this project to resurface the existing pavement to extend the service life of the roadway. The public meeting is being held to present information and receive community feedback.

FDOT encourages community involvement and is offering this public meeting in person and virtually to provide more ways to

participate. All attendees, regardless of which platform they choose, will receive the same information.

- **In-Person Open House Option:** Participants may attend in person by going to Good Shepherd Lutheran Church, 2073 Garden St., Titusville, FL 32796 anytime between 5:30 p.m. and 7 p.m. to view a looping presentation and project displays, speak with project team members, and submit comments or questions. If attending in person, please remember to follow all safety and sanitation guidelines. If you are feeling unwell, please consider attending the meeting virtually.
- **Virtual Option:** Interested persons may join over the Internet from a computer, tablet or mobile device at 5:30 p.m. For this option, advance registration is required by visiting fdot.cc/Garden_Street_Improvements. Once registered, participants will receive a confirmation email containing information about joining the meeting online. If using a mobile device, use the GoToWebinar app to be able to view the presentation and submit comments. If joining online, please allow adequate time to log in to view the presentation in its entirety.

If you are unable to attend the meeting, you can view all meeting materials, including the presentation, and provide comment through the project website at www.cflroads.com/project/450626-1. You may also contact FDOT Project Manager Gregory Prytyka, P.E., directly by phone at 386-943-5394 or email at Gregory.Prytyka@dot.state.fl.us.

FDOT is sending notices to nearby property owners, business owners, interested persons, and organizations to provide the opportunity to offer comments and express their views regarding this project and the proposed improvements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons wishing to express their concerns relative to FDOT compliance with Title VI may do so by contacting Melissa McKinney, FDOT District Five Title VI Coordinator, at Melissa.McKinney@dot.state.fl.us.

Persons who require accommodations under the Americans with Disabilities Act (ADA) or persons who require translation services (free of charge) should contact Carolyn Fitzwilliam at 386-943-5215 or Carolyn.Fitzwilliam@dot.state.fl.us at least seven (7) days prior to the meeting. If you are hearing or speech impaired, please contact us by using the Florida Relay Service, 1-800-955-8771 (TDD) or 1-800-955-8770 (Voice).

If you have any questions or comments about the meeting or the project, please contact Mr. Prytyka by phone at 386-943-5394, by email at Gregory.Prytyka@dot.state.fl.us, or U.S. mail at Florida Department of Transportation, 719 S. Woodland Blvd., MS 542, DeLand, FL 32720.

Project Web Site: <https://www.cflroads.com/project/450626-1>

Primary Contact: Gregory Prytyka

Primary Phone: 386-943-5394

Primary E-Mail: Gregory.Prytyka@dot.state.fl.us

Expires: 8/23/2024

SUNRAIL CENTRAL FLORIDA COMMUTER RAIL COMMISSION BOARD MEETING AUGUST 22, 2024

District: Five

Meeting Type: Meeting

Date: Thursday, August 22, 2024

Time: 2:00 pm to 3:00 pm

Location Name: LYNX Central Station Administration Building Board Room

Street Address: 455 N. Garland Avenue

City: Orlando

Purpose: ORLANDO, Fla. – The Central Florida Commuter Rail Commission (CFCRC), the SunRail advisory board of elected leaders from the five Local Funding Partners, will hold a board meeting on Thursday, August 22, 2024.

The meeting takes place at 2 p.m. in the LYNX Central Station Administration Building. Attendees have the option of joining in person, virtually, or over the phone. Please note that there is limited seating in the room where the commission will hold its meeting.

Those who wish to attend by phone may dial 213-929-4221 and enter access code 244-900-372 (muted) when prompted.

Attendees may also join from a computer, tablet, or cell phone by registering at attendee.gotowebinar.com/register/4066244029641842263. The meeting will start promptly at 2 p.m. The meeting agenda and meeting materials are made available at least 72 hours prior to the meeting at [Corporate.Sunrail.com](https://www.corporate.sunrail.com).

Those wishing to provide comments in person must come the day and time of the meeting to the LYNX main office at:

LYNX Central Station Administration Building Board Room 455 N. Garland Avenue, 2nd Floor Orlando, FL 32801

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons wishing to express their concerns relative to FDOT compliance with Title VI may do so by contacting Roger Masten, SunRail Title VI Coordinator at Roger.Masten@dot.state.fl.us.

Persons who require accommodations under the Americans with Disabilities Act or persons who require translation services

(free of charge) should contact Roger Masten, at least seven days in advance of the meeting, c/o SunRail, 801 SunRail Drive, Sanford, FL 32771; or by phone at (321) 257-7161; or by email at Roger.Masten@dot.state.fl.us.

Questions? Please contact the FDOT Public Information Office at FDOT-D5COMM@dot.state.fl.us or (386) 943-5593.

Please be careful around trains, railroad crossings, and while at station platforms. Be smart. Be safe. For more information about SunRail, including fares, schedule, and station locations, please visit www.SunRail.com, or download the SunRail app.

Project Web Site: <https://corporate.sunrail.com/>

Primary Contact: Public Information Office

Primary Phone: (386) 943-5593

Primary E-Mail: FDOT-D5COMM@dot.state.fl.us

Expires: 8/23/2024

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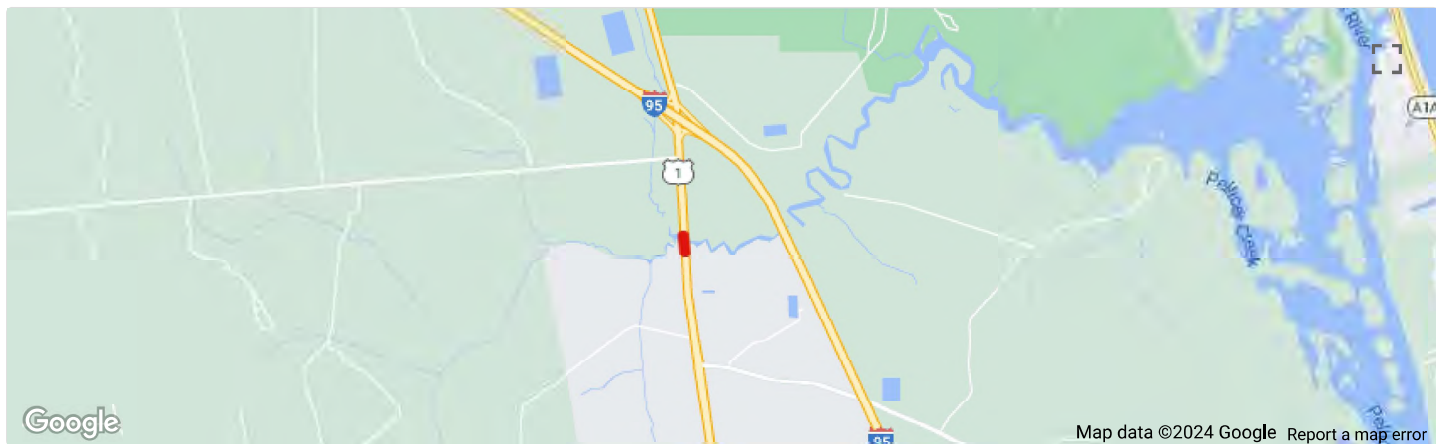
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Florida Department of TRANSPORTATION

447118-1 US 1 (SR 5) over Pellicer Creek Bridge Replacement



Project Details

Phase: Design

Length: 0.231 miles

County: Flagler

Road: SR 5
US 1

Design Cost: \$1.9 Million

Letting: 01/28/2026

Estimated Costs

Construction Cost: \$11.6 Million

About

The purpose of the project is to replace the northbound and southbound U.S. 1 (State Road 5) bridges over Pellicer Creek at the Flagler/St Johns County line.

Get Involved

As part of the Project Development & Environment (PD&E) Study, the Florida Department of Transportation (FDOT) announces a **notice of opportunity to request a public hearing.**

This proposed project involves the use of properties protected under the National Historic Preservation Act of 1966. The southbound bridge (#730008) was evaluated as eligible for listing in the National Register of Historic Places (NRHP) for its significance in Engineering and Transportation. The proposed undertaking will result in an 'adverse effect' to the NRHP-eligible bridge. In consultation with the State Historic Preservation Officer, proposed mitigation strategies for the southbound bridge include preparation of a Historic American Engineering Record document, as outlined in the Section 106 Case Study Report and the Draft Section 106 Memorandum of Agreement (MOA). A Programmatic Section 4(f) Evaluation was also prepared.

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Send a comment or ask a question about this project

Contact Information
Design Project Manager Shelley ChinQuee (386) 943-5439 Shelley.ChinQuee@dot.state.fl.us
Design Firm Florida Bridge and Transportation, Inc.
Last Updated 08/15/2024



Project Files
Documents 2024-08-16 Draft Section 106 MOA 2024-08-16 Draft Programmatic Section 4(f) Evaluation 2023-10-25 Approved Section 106 Case Study Report 2023-03-21 Stakeholder Presentation 2023-02-28 Project Information Handout



Florida Department of TRANSPORTATION

Welcome to FDOT's District Five Projects Website



Grab Your Running Shoes: Learn about upcoming trail plans and projects

Looking for the nearest trail to explore? Check here for SUN Trail funded construction projects – both complete and on the horizon! **More...**

Project Finder

You can find FDOT projects quickly by selecting a road, city or county below, or by clicking a county on the service area map.

Search Road Search County Search City

Current News

08/16/2024 Opportunity for Public Hearing: U.S. 1/S.R. 5 Over Pellicer Creek Bridge Replacement

08/14/2024 Upcoming Public Meeting: SunRail Central Florida Commuter Rail Commission Board Meeting August 22, 2024

08/14/2024 Upcoming Public Meeting: Garden Street (S.R. 406) Resurfacing

08/13/2024 Traffic Advisory: Westbound I-4 Entrance Ramp from Sand Lake Road scheduled for Overnight Closures August 14, 15, and 18

07/03/2024 Traffic Advisory: Lane Closures at I-4 and Conroy Road Interchange to Begin July 8

Social Media

FDOT Central Florida
Follow Page 7.9K followers

FDOT Central Florida
on Thursday

Attention Flagler County motorists!
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Police Department

U.S. Army Corps of Engineers, Jacksonville District

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Diverging Diamond Interchange (DDI)

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Federal DOT Roundabouts





Florida Department of TRANSPORTATION

Opportunity for Public Hearing: U.S. 1/S.R. 5 Over Pellicer Creek Bridge Replacement

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